

APPENDIX G

Historic, Architectural, Archaeological, and Cultural Resources

Section 106 Initiation Letters

From: Alexander, Timothy L (FAA) <timothy.l.alexander@faa.gov>

Sent: Friday, February 19, 2021 3:49 PM

To: president@oldemeck.org

Cc: Leathers, Amber <amber.leathers@cltairport.com>; Gledhill-earley, Renee <renee.gledhill-earley@ncdcr.gov>; Sarah Potter <Sarah.Potter@landrumbrown.com>; Dupree, Tommy (FAA) <Tommy.Dupree@faa.gov>; Johnson, Duane (FAA) <Duane.Johnson@faa.gov>

Subject: Section 106 Consultation CLT

CAUTION: This email attachment originated from a third party. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Mr. Houser,

The FAA has identified your organization as a potentially interested party that may wish to participate as a consulting party in the Section 106 process for the Charlotte-Douglas International Airport (CLT). The purpose of this correspondence is to determine if you wish to participate. The attached letter and the attached figure provide additional information. If you wish to participate in the process as a consulting party, please respond by March 22, 2021. If you have additional questions, please contact me.

Regards,

Tim Alexander

Environmental Protection Specialist
Federal Aviation Administration
Memphis Airports District Office
2600 Thousand Oaks Boulevard
Suite 2250
Memphis, Tennessee 38118

timothy.l.alexander@faa.gov

901-322-8188



U.S. Department
of Transportation
**Federal Aviation
Administration**

Memphis Airports District Office
2600 Thousand Oaks Blvd., Suite 2250
Memphis, TN 38118

Phone (901) 322-8180

February 19, 2021

Jeffrey Houser, President
Olde Mecklenburg Genealogical Society
P.O. Box 32453
Charlotte, NC 28232

**RE: Section 106 Consultation for Charlotte Douglas International Airport
Request for Participation**

Dear Mr. Houser:

This letter is notification that the Federal Aviation Administration (FAA) has initiated Section 106 consultation through the NEPA process as stipulated in 36 CFR 800.8. The City of Charlotte is preparing an Environmental Assessment (EA) to review the potential impacts from proposed capacity enhancement projects at the Charlotte Douglas International Airport (CLT).

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If you have any questions regarding the EA or this request, please contact Tim Alexander at (901) 322-8188 or by email at timothy.l.alexander@faa.gov.

Sincerely,

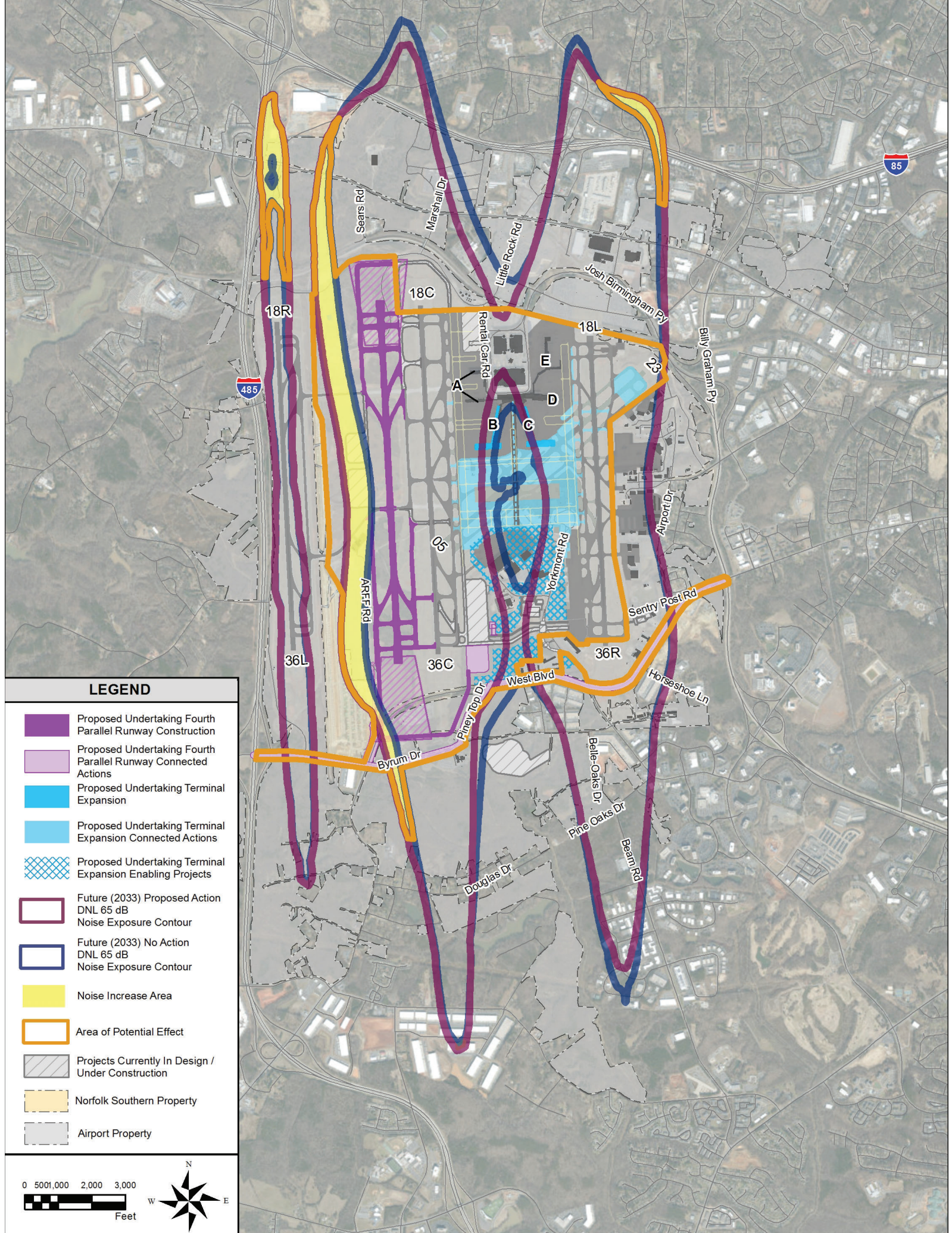
**Duane Leland
Johnson**

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Leland Johnson
Date: 2021.02.19 09:17:12
-06'00'

Duane L. Johnson, PE
Assistant Manager, Memphis Airports District Office

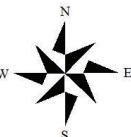
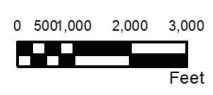
ENCLOSURES

cc: Amber Leathers, City of Charlotte Aviation Department
Renee Gledhill-Earley, NCDNCR



LEGEND

- Proposed Undertaking Fourth Parallel Runway Construction
- Proposed Undertaking Fourth Parallel Runway Connected Actions
- Proposed Undertaking Terminal Expansion
- Proposed Undertaking Terminal Expansion Connected Actions
- Proposed Undertaking Terminal Expansion Enabling Projects
- Future (2033) Proposed Action DNL 65 dB Noise Exposure Contour
- Future (2033) No Action DNL 65 dB Noise Exposure Contour
- Noise Increase Area
- Area of Potential Effect
- Projects Currently In Design / Under Construction
- Norfolk Southern Property
- Airport Property



From: Alexander, Timothy L (FAA) <timothy.l.alexander@faa.gov>

Sent: Thursday, March 11, 2021 1:26 PM

To: Jeffrey Houser <jeffrey.j.houser@gmail.com>

Cc: Dupree, Tommy (FAA) <Tommy.Dupree@faa.gov>; Johnson, Duane (FAA) <Duane.Johnson@faa.gov>; Leathers, Amber <amber.leathers@cltairport.com>; Sarah Potter <Sarah.Potter@landrumbrown.com>; Gledhill-earley, Renee <renee.gledhill-earley@ncdcr.gov>

Subject: RE: Section 106 Consultation CLT

Mr. Houser,

Thank you for your response regarding the CLT Major Capacity project. We will make sure you are notified and/or copied on documents provided for public review. Please contact me if you have additional comments or questions.

Thank you,

Tim Alexander

Environmental Protection Specialist

Federal Aviation Administration

Memphis Airports District Office

2600 Thousand Oaks Boulevard

Suite 2250

Memphis, Tennessee 38118

timothy.l.alexander@faa.gov

901-322-8188

From: Jeffrey Houser <jeffrey.j.houser@gmail.com>

Sent: Thursday, March 11, 2021 12:06 PM

To: Alexander, Timothy L (FAA) <timothy.l.alexander@faa.gov>

Subject: Re: Section 106 Consultation CLT

Hello Mr. Alexander,

Please consider myself and the society I am president of, the Mecklenburg Genealogical Society of Charlotte, to be used as consultants for the proposed changes for the Charlotte-Douglas Airport.

Jeff Houser

President, Olde Meck

On Fri, Feb 19, 2021 at 3:50 PM Alexander, Timothy L (FAA) <timothy.l.alexander@faa.gov> wrote:

Mr. Houser,

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this correspondence is to determine if you wish to participate. The attached letter and the attached figure provide additional information. If you wish to participate in the process as a consulting party, please respond by March 22, 2021. If you have additional questions, please contact me.

Regards,

Tim Alexander

Environmental Protection Specialist

Federal Aviation Administration

Memphis Airports District Office

2600 Thousand Oaks Boulevard

Suite 2250

Memphis, Tennessee 38118

timothy.l.alexander@faa.gov

901-322-8188



U.S. Department
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**Federal Aviation
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Memphis Airports District Office
2600 Thousand Oaks Blvd., Suite 2250
Memphis, TN 38118

Phone (901) 322-8180

February 19, 2021

Ted Alexander
Western Office Regional Director
Preservation North Carolina
P.O. Box 2
Shelby, NC 28151

**RE: Section 106 Consultation for Charlotte Douglas International Airport
Request for Participation**

Dear Mr. Alexander:

This letter is notification that the Federal Aviation Administration (FAA) has initiated Section 106 consultation through the NEPA process as stipulated in 36 CFR 800.8. The City of Charlotte is preparing an Environmental Assessment (EA) to review the potential impacts from proposed capacity enhancement projects at the Charlotte Douglas International Airport (CLT).

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If you have any questions regarding the EA or this request, please contact Tim Alexander at (901) 322-8188 or by email at timothy.l.alexander@faa.gov.

Sincerely,

Duane Leland Johnson

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Leland Johnson
Date: 2021.02.19 09:20:38
-06'00'

Duane L. Johnson, PE
Assistant Manager, Memphis Airports District Office

ENCLOSURES

cc: Amber Leathers, City of Charlotte Aviation Department
Renee Gledhill-Earley, NCDNCR

From: Ted Alexander <talexander@presnc.org>
Sent: Friday, February 26, 2021 8:06 AM
To: 'Alexander, Timothy L (FAA)' <timothy.l.alexander@faa.gov>
Cc: 'Leathers, Amber' <amber.leathers@cltairport.com>; 'Gledhill-earley, Renee' <renee.gledhill-earley@ncdcr.gov>; Sarah Potter <Sarah.Potter@landrumbrown.com>; 'Dupree, Tommy (FAA)' <Tommy.Dupree@faa.gov>; 'Johnson, Duane (FAA)' <Duane.Johnson@faa.gov>
Subject: RE: Section 106 Consultation CLT

We are interested. Thank you

W. Ted Alexander

Regional Director, Western Office
Preservation North Carolina
PO Box 2
Shelby, NC 28151
704-482-3531
Cell: 704-473-0005
FAX: 919-832-1651
talexander@presnc.org
www.presnc.org

From: Alexander, Timothy L (FAA) <timothy.l.alexander@faa.gov>
Sent: Friday, February 19, 2021 3:50 PM
To: talexander@presnc.org
Cc: Leathers, Amber <amber.leathers@cltairport.com>; Gledhill-earley, Renee <renee.gledhill-earley@ncdcr.gov>; Sarah Potter <Sarah.Potter@landrumbrown.com>; Dupree, Tommy (FAA) <Tommy.Dupree@faa.gov>; Johnson, Duane (FAA) <Duane.Johnson@faa.gov>
Subject: Section 106 Consultation CLT

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Regards,

Tim Alexander

Environmental Protection Specialist
Federal Aviation Administration
Memphis Airports District Office
2600 Thousand Oaks Boulevard
Suite 2250
Memphis, Tennessee 38118
timothy.l.alexander@faa.gov
901-322-8188



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Memphis Airports District Office
2600 Thousand Oaks Blvd., Suite 2250
Memphis, TN 38118

Phone (901) 322-8180

February 19, 2021

Adria Focht
President & CEO
Charlotte Museum of History
3500 Shamrock Drive
Charlotte, NC 28215

**RE: Section 106 Consultation for Charlotte Douglas International Airport
Request for Participation**

Dear Ms. Focht:

This letter is notification that the Federal Aviation Administration (FAA) has initiated Section 106 consultation through the NEPA process as stipulated in 36 CFR 800.8. The City of Charlotte is preparing an Environmental Assessment (EA) to review the potential impacts from proposed capacity enhancement projects at the Charlotte Douglas International Airport (CLT).

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Sincerely,

**Duane Leland
Johnson**

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Leland Johnson
Date: 2021.02.19 09:16:37
-06'00'

Duane L. Johnson. PE
Assistant Manager, Memphis Airports District Office

ENCLOSURES

cc: Amber Leathers, City of Charlotte Aviation Department
Renee Gledhill-Earley, NCDNCR



U.S. Department
of Transportation
**Federal Aviation
Administration**

Memphis Airports District Office
2600 Thousand Oaks Blvd., Suite 2250
Memphis, TN 38118

Phone (901) 322-8180

February 19, 2021

Kristi Harpst
Program Manager
City of Charlotte Historic District Commission
600 East Fourth Street, 8th Floor
Charlotte, NC 28202

**RE: Section 106 Consultation for Charlotte Douglas International Airport
Request for Participation**

Dear Ms. Harpst:

This letter is notification that the Federal Aviation Administration (FAA) has initiated Section 106 consultation through the NEPA process as stipulated in 36 CFR 800.8. The City of Charlotte is preparing an Environmental Assessment (EA) to review the potential impacts from proposed capacity enhancement projects at the Charlotte Douglas International Airport (CLT).

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If you have any questions regarding the EA or this request, please contact Tim Alexander at (901) 322-8188 or by email at timothy.l.alexander@faa.gov.

Sincerely,

**Duane Leland
Johnson**

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Leland Johnson
Date: 2021.02.19 09:19:36
-06'00'

Duane L. Johnson, PE
Assistant Manager, Memphis Airports District Office

ENCLOSURES

cc: Amber Leathers, City of Charlotte Aviation Department
Renee Gledhill-Earley, NCDNCR

Gaby Elizondo

Subject: FW: Section 106 Consultation - Charlotte-Douglas International Airport (CLT)

From: Leite, Candice <Candice.Leite@charlottenc.gov>

Sent: Thursday, March 4, 2021 5:37 PM

To: timothy, alexander <timothy.l.alexander@faa.gov>

Cc: Harpst, Kristina <Kristina.Harpst@charlottenc.gov>; Leathers, Amber <amber.leathers@cltairport.com>; Gledhill-earley, Renee <renee.gledhill-earley@ncdcr.gov>; Sarah Potter <Sarah.Potter@landrumbrown.com>; Tommy, Dupree <Tommy.Dupree@faa.gov>; Duane, Johnson <Duane.Johnson@faa.gov>; Gray, Stewart <Stewart.Gray@mecklenburgcountync.gov>

Subject: Section 106 Consultation - Charlotte-Douglas International Airport (CLT)

Good Day,

Thank you for consulting the City of Charlotte Historic District Commission on the Charlotte-Douglas International Airport (CLT) project located in Charlotte, NC.

The proposed site is not located in a City of Charlotte designated local district, governed by City of Charlotte Historic District Commission.

There are, however, several historic resources in the area which may be potentially affected by the project. If you have not already done so, please reach out to the Charlotte-Mecklenburg Historic Landmarks Commission and the NC State Historic Preservation Office.

Thank You.

Candice

Candice R. Leite

Assoc. Planner, Historic Districts

Charlotte Planning, Design + Development

600 East 4th Street | 8th Floor | Charlotte, NC 28002

(704) 336-2302 | candice.leite@charlottenc.gov | charlottenc.gov



From: Alexander, Timothy L (FAA) <timothy.l.alexander@faa.gov>

Sent: Friday, February 19, 2021 3:53 PM

To: Harpst, Kristina <Kristina.Harpst@charlottenc.gov>

Cc: Leathers, Amber <amber.leathers@cltairport.com>; Gledhill-earley, Renee <renee.gledhill-earley@ncdcr.gov>; Sarah Potter <Sarah.Potter@landrumbrown.com>; Tommy, Dupree <Tommy.Dupree@faa.gov>; Duane, Johnson <Duane.Johnson@faa.gov>

Subject: [EXT] Section 106 Consultation CLT

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Ms. Harpst,

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Regards,

Tim Alexander

Environmental Protection Specialist
Federal Aviation Administration
Memphis Airports District Office
2600 Thousand Oaks Boulevard
Suite 2250
Memphis, Tennessee 38118
timothy.l.alexander@faa.gov
901-322-8188



U.S. Department
of Transportation
**Federal Aviation
Administration**

Memphis Airports District Office
2600 Thousand Oaks Blvd., Suite 2250
Memphis, TN 38118

Phone (901) 322-8180

February 19, 2021

Jack Thomson
Executive Director
Charlotte-Mecklenburg
Historic Landmarks Commission
2100 Randolph Road
Charlotte, NC 28207

**RE: Section 106 Consultation for Charlotte Douglas International Airport
Request for Participation**

Dear Mr. Thomson:

This letter is notification that the Federal Aviation Administration (FAA) has initiated Section 106 consultation through the NEPA process as stipulated in 36 CFR 800.8. The City of Charlotte is preparing an Environmental Assessment (EA) to review the potential impacts from proposed capacity enhancement projects at the Charlotte Douglas International Airport (CLT).

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Sincerely,

**Duane Leland
Johnson**

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Leland Johnson
Date: 2021.02.19 09:20:04
-06'00'

Duane L. Johnson, PE
Assistant Manager, Memphis Airports District Office

ENCLOSURES

cc: Amber Leathers, City of Charlotte Aviation Department
Renee Gledhill-Earley, NCDNCR



U.S. Department
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**Federal Aviation
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Memphis Airports District Office
2600 Thousand Oaks Blvd., Suite 2250
Memphis, TN 38118

Phone (901) 322-8180

February 19, 2021

Linda Dalton, President
Mecklenburg Historical Association
P.O. Box 35032
Charlotte, NC 28235

**RE: Section 106 Consultation for Charlotte Douglas International Airport
Request for Participation**

Dear Ms. Dalton:

This letter is notification that the Federal Aviation Administration (FAA) has initiated Section 106 consultation through the NEPA process as stipulated in 36 CFR 800.8. The City of Charlotte is preparing an Environmental Assessment (EA) to review the potential impacts from proposed capacity enhancement projects at the Charlotte Douglas International Airport (CLT).

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Sincerely,

**Duane Leland
Johnson**

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Leland Johnson
Date: 2021.02.19 09:18:43
-06'00'

Duane L. Johnson. PE
Assistant Manager, Memphis Airports District Office

ENCLOSURES

cc: Amber Leathers, City of Charlotte Aviation Department
Renee Gledhill-Earley, NCDNCR

From: Alexander, Timothy L (FAA) <timothy.l.alexander@faa.gov>

Sent: Friday, February 19, 2021 3:46 PM

To: wenonah.haire@catawba.com

Cc: Leathers, Amber <amber.leathers@cltairport.com>; Gledhill-earley, Renee <renee.gledhill-earley@ncdcr.gov>; Sarah Potter <Sarah.Potter@landrumbrown.com>; Dupree, Tommy (FAA) <Tommy.Dupree@faa.gov>; Johnson, Duane (FAA) <Duane.Johnson@faa.gov>

Subject: Section 106 Consultation CLT

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Ms. Haire,

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Regards,

Tim Alexander

Environmental Protection Specialist

Federal Aviation Administration

Memphis Airports District Office

2600 Thousand Oaks Boulevard

Suite 2250

Memphis, Tennessee 38118

timothy.l.alexander@faa.gov

901-322-8188



U.S. Department
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Memphis Airports District Office
2600 Thousand Oaks Blvd., Suite 2250
Memphis, TN 38118

Phone (901) 322-8180

February 19, 2021

Wenonah Haire
Executive Director
THPO and Catawaba Cultural Center
1536 Tom Steven Road
Rock Hill, SC 29730

**RE: Section 106 Consultation for Charlotte Douglas International Airport
Request for Participation**

Dear Ms. Haire:

This letter is notification that the Federal Aviation Administration (FAA) has initiated Section 106 consultation through the NEPA process as stipulated in 36 CFR 800.8. The City of Charlotte is preparing an Environmental Assessment (EA) to review the potential impacts from proposed capacity enhancement projects at the Charlotte Douglas International Airport (CLT).

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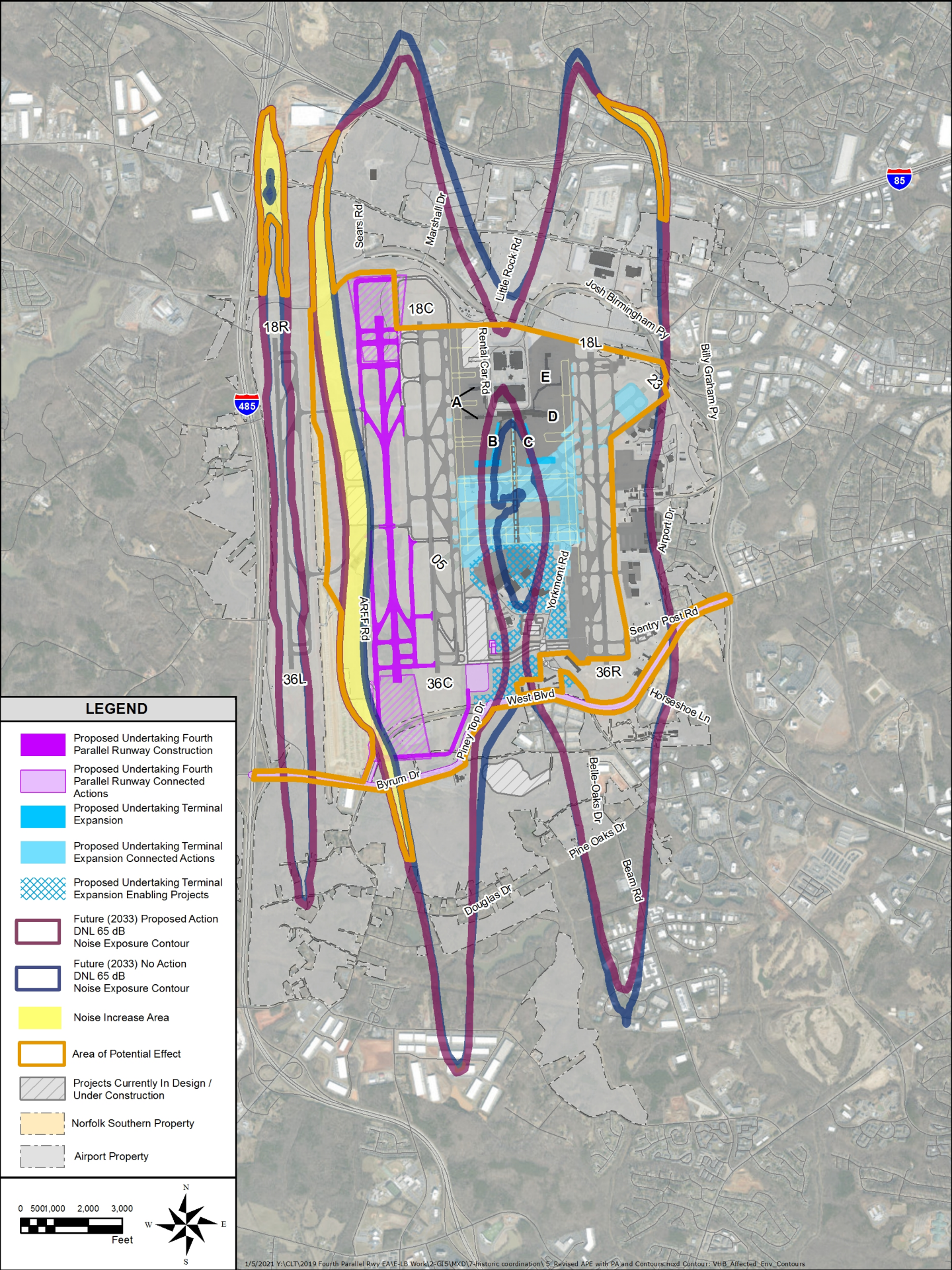
If you have any questions regarding the EA or this request, please contact Tim Alexander at (901) 322-8188 or by email at timothy.l.alexander@faa.gov.

Sincerely,

Duane L. Johnson, PE
Assistant Manager, Memphis Airports District Office

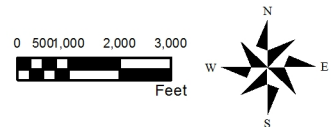
ENCLOSURES

cc: Amber Leathers, City of Charlotte Aviation Department
Renee Gledhill-Earley, NCDNCR



LEGEND

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- Proposed Undertaking Fourth Parallel Runway Connected Actions
- Proposed Undertaking Terminal Expansion
- Proposed Undertaking Terminal Expansion Connected Actions
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- Norfolk Southern Property
- Airport Property





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Memphis Airports District Office
2600 Thousand Oaks Blvd., Suite 2250
Memphis, TN 38118

Phone (901) 322-8180

February 19, 2021

Bill Harris
Chief, Catawba Indian Nation
996 Avenue of the Nations
Rock Hill, SC 29730

**RE: Section 106 Consultation for Charlotte Douglas International Airport
Request for Participation**

Dear Mr. Harris:

This letter is notification that the Federal Aviation Administration (FAA) has initiated Section 106 consultation through the NEPA process as stipulated in 36 CFR 800.8. The City of Charlotte is preparing an Environmental Assessment (EA) to review the potential impacts from proposed capacity enhancement projects at the Charlotte Douglas International Airport (CLT).

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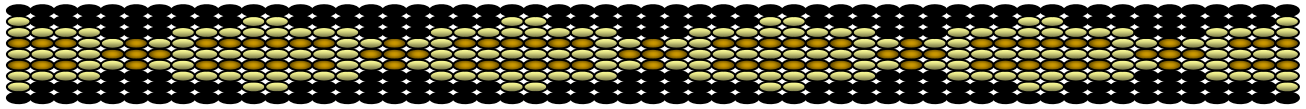
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Sincerely,

Duane L. Johnson, PE
Assistant Manager, Memphis Airports District Office

ENCLOSURES

cc: Amber Leathers, City of Charlotte Aviation Department
Renee Gledhill-Earley, NCDNCR



Catawba Indian Nation
Tribal Historic Preservation Office
1536 Tom Steven Road
Rock Hill, South Carolina 29730

Office 803-328-2427
Fax 803-328-5791

March 29, 2021

Attention: Duane Johnson
Federal Aviation Administration
2600 Thousand Oaks Blvd., Suite 2250
Memphis, TN 38118

Re. THPO #	TCNS #	Project Description
2020-40-1		Proposed capacity enhancement projects at Charlotte Douglas International Airport

Dear Mr. Johnson,

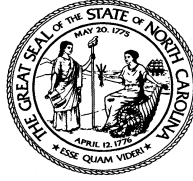
The Catawba have no immediate concerns with regard to traditional cultural properties, sacred sites or Native American archaeological sites within the boundaries of the proposed project areas. **However, the Catawba are to be notified if Native American artifacts and / or human remains are located during the ground disturbance phase of this project.**

If you have questions please contact Caitlin Rogers at 803-328-2427 ext. 226, or e-mail Caitlin.Rogers@catawba.com.

Sincerely,

Wenonah G. Haire
Tribal Historic Preservation Officer

Consultation on APE



**North Carolina Department of Natural and Cultural Resources
State Historic Preservation Office**

Ramona M. Bartos, Administrator

Governor Roy Cooper

Secretary D. Reid Wilson

January 21, 2021

Tommy L. Dupree
Federal Aviation Administration
Memphis Airports District Office
2600 Thousand Oaks Boulevard, Suite 2250
Memphis, TN 38118

tommy.dupree@faa.gov

RE: Capacity Enhancement Project EA, CLT, Charlotte, Mecklenburg County, ER 20-0438

Dear Mr. Dupree:

Thank you for your January 8, 2021 letter asking that we consider a reduced Area of Potential Effects for the above-referenced undertaking, based on changes in the proposed project and additional noise analyses. Having reviewed the map provided, we requested a map showing the revised plan and noise contours. We received the new map on January 12, 2021.

Given the revisions, additional studies and our knowledge of the area, we agree to using the revised areas of potential effect as shown in "goldenrod" on the January 12, 2021 map. We also understand that this may well lead to fewer above-ground properties being addressed in the final Historic Structures Survey Report for the undertaking.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, contact Renee Gledhill-Earley, environmental review coordinator, at 919-814-6579 or environmental.review@ncdcr.gov. In all future communication concerning this project, please cite the above referenced tracking number.

Sincerely,

A handwritten signature in blue ink that reads "Renee Gledhill-Earley".

for Ramona Bartos, Deputy
State Historic Preservation Officer

cc: Tim Alexander, FAA
Amber Leathers, CLT

timothy.alexander@faa.gov
amber.leathers@cltairport.com



U.S. Department
of Transportation
**Federal Aviation
Administration**

Memphis Airports District Office
2600 Thousand Oaks Blvd., Suite 2250
Memphis, TN 38118

Phone (901) 322-8180

January 8, 2021

Ms. Renee Gledhill-Earley
Environmental Review Coordinator
State Historic Preservation Office
109 East Jones St. MSC 4617
Raleigh, NC 27699

**RE: Section 106 Consultation
Capacity Enhancement Projects Environmental Assessment
Charlotte Douglas International Airport (CLT)**

Dear Ms. Gledhill-Earley:

As part of the Federal Aviation Administration's (FAA's) Section 106 review and pursuant to 36 CFR § 800.4, the FAA has undertaken identification efforts for the proposed capacity enhancement projects at the Charlotte Douglas International Airport (CLT), Charlotte, NC. We are formally seeking your concurrence on the above and below-ground Areas of Potential Effect (APE). The proposed APE for above-ground resources is based on the combined elements of the proposed undertaking and their potential to cause adverse effects by construction/demolition, through visual impacts, or from an increase in noise levels. The attached figure graphically depicts the proposed below and above-ground APE.

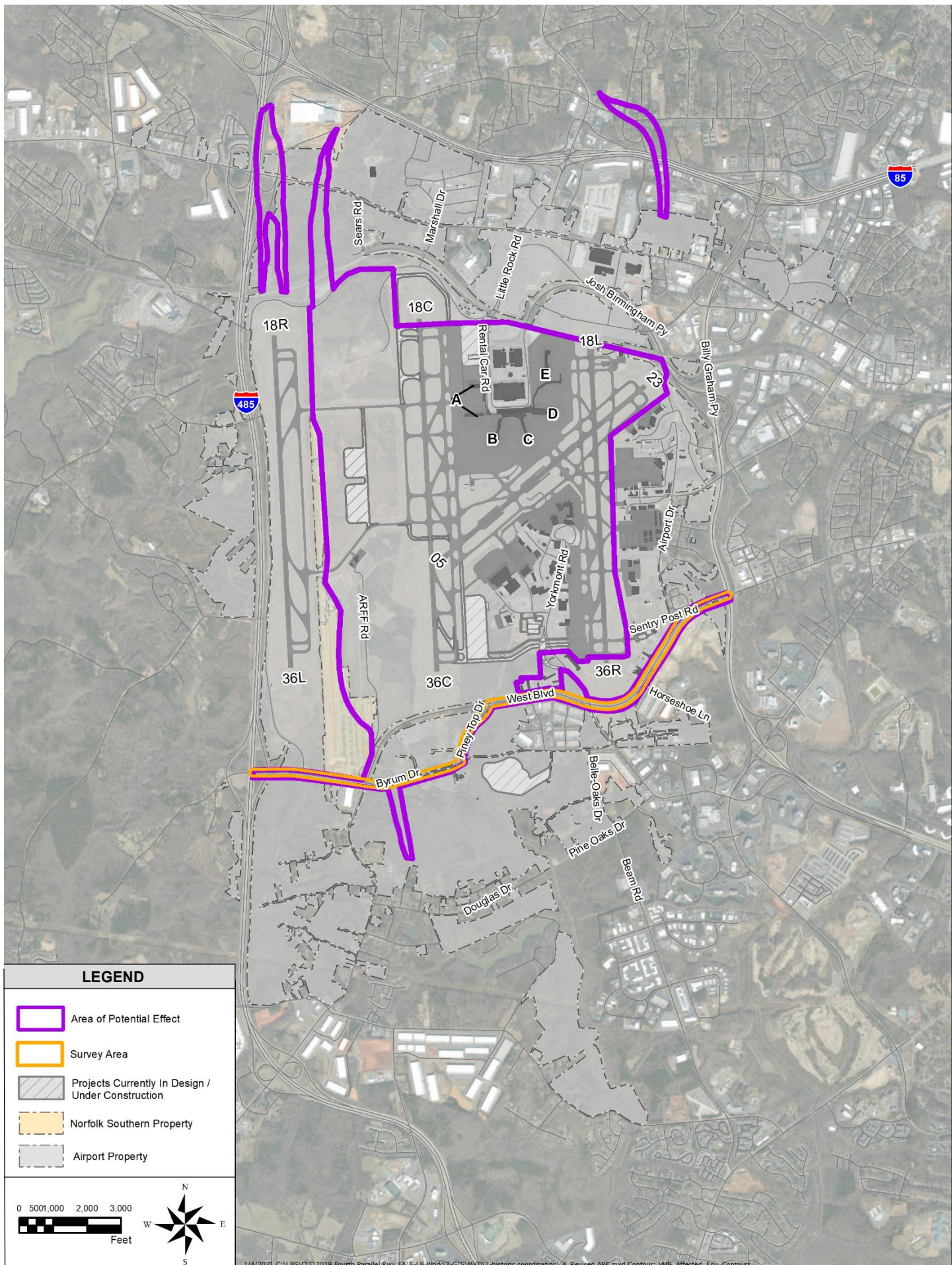
The FAA has determined it appropriate to redefine the APE from that previously submitted to your office on August 31, 2020. The APE has been redefined based on (1) a change in the proposed undertaking, which reduced the proposed runway length from 12,000 feet to 10,000 feet and allowed the relocation of West Boulevard to use existing roadways, and (2) noise analyses prepared that identified the areas where noise would increase from the proposed undertaking.

If you have any questions, please contact me at (901) 322-8181 or by email at Tommy.Dupree@faa.gov.

Sincerely,

Tommy L. Dupree
Manager, Memphis Airports District Office

cc: Amber Leathers, CLT



Gaby Elizondo

Subject: FW: ER 20 0438_5.pdf CLT Increased capacity APE
Attachments: CLT Revised APE with PA and Noise Contours.pdf

From: Alexander, Timothy L (FAA)
Sent: Tuesday, January 12, 2021 2:45 PM
To: Gledhill-earley, Renee <renee.gledhill-earley@ncdcr.gov>
Cc: Dupree, Tommy (FAA) <Tommy.Dupree@faa.gov>
Subject: RE: ER 20 0438_5.pdf CLT Increased capacity APE

Renee,

Good afternoon. Please find attached the revised graphic of the proposed APE, which includes the overlays you requested. If you have questions or need additional information, do not hesitate to contact me.

Thanks,

Tim Alexander
Environmental Protection Specialist
Federal Aviation Administration
Memphis Airports District Office
2600 Thousand Oaks Boulevard
Suite 2250
Memphis, Tennessee 38118
timothy.l.alexander@faa.gov
901-322-8188

From: Gledhill-earley, Renee <renee.gledhill-earley@ncdcr.gov>
Sent: Monday, January 11, 2021 3:35 PM
To: Dupree, Tommy (FAA) <Tommy.Dupree@faa.gov>; Alexander, Timothy L (FAA) <timothy.l.alexander@faa.gov>
Subject: ER 20 0438_5.pdf CLT Increased capacity APE

Gentlemen:

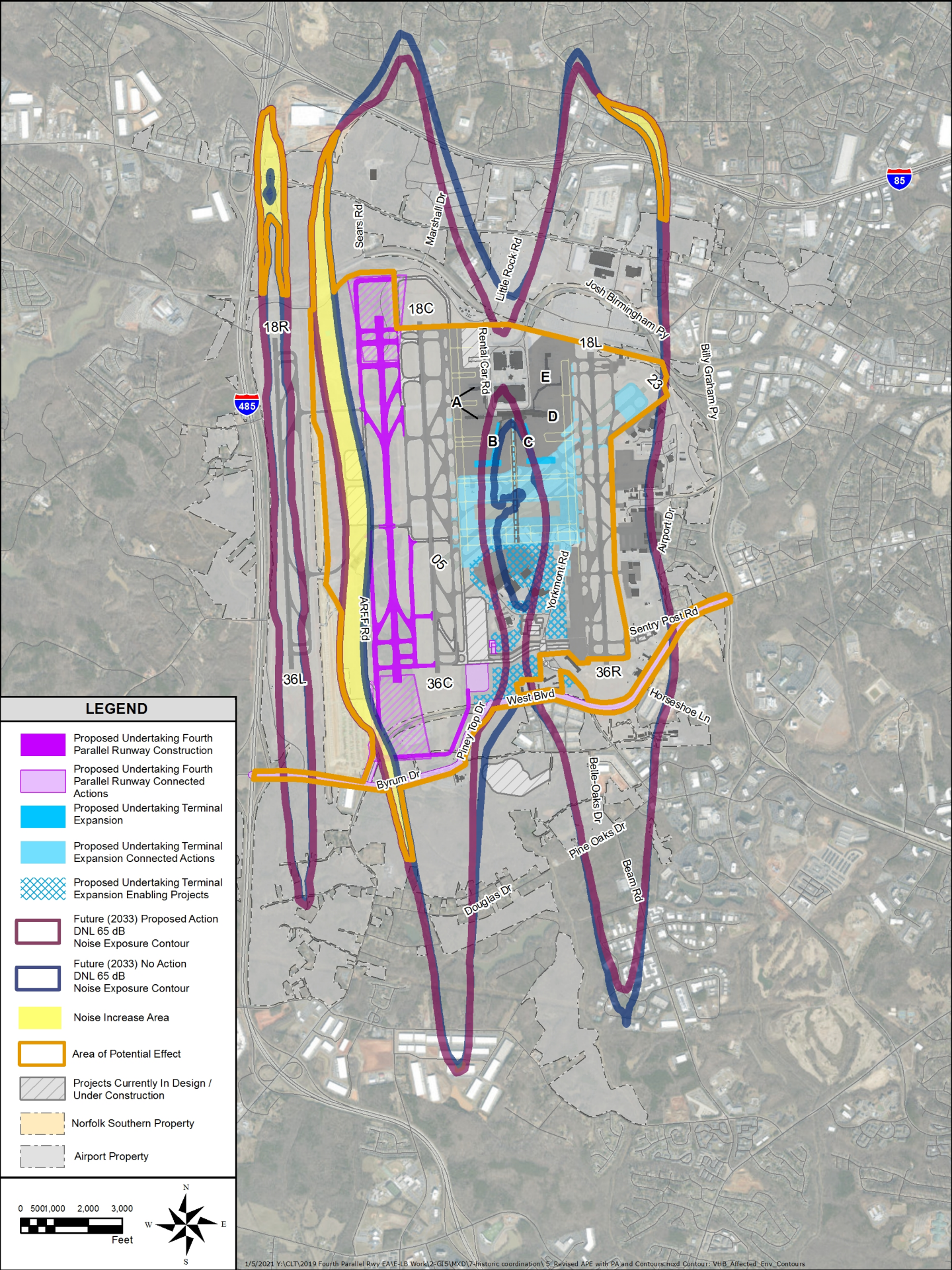
I have your letter and map for the reduced APE for the proposed increased capacity projects at CLT.

It would be very helpful to have the Clear Zones and Noise Contours overlaid to the map so that I can clearly see the reduction in relationship to the undertaking as currently proposed.

I apologize, if I am asking for something that I may already have. But, it would be good to not have to dive into all of the previous submittals to dig it up.

Thank you for your assistance.

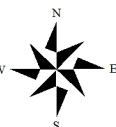
Renee



LEGEND

- Proposed Undertaking Fourth Parallel Runway Construction
- Proposed Undertaking Fourth Parallel Runway Connected Actions
- Proposed Undertaking Terminal Expansion
- Proposed Undertaking Terminal Expansion Connected Actions
- Proposed Undertaking Terminal Expansion Enabling Projects
- Future (2033) Proposed Action DNL 65 dB Noise Exposure Contour
- Future (2033) No Action DNL 65 dB Noise Exposure Contour
- Noise Increase Area
- Area of Potential Effect
- Projects Currently In Design / Under Construction
- Norfolk Southern Property
- Airport Property

0 500/1,000 2,000 3,000
Feet





U.S. Department
of Transportation
**Federal Aviation
Administration**

Memphis Airports District Office
2600 Thousand Oaks Blvd., Suite 2250
Memphis, TN 38118

Phone (901) 322-8180

August 31, 2020

Ms. Renee Gledhill-Earley
Environmental Review Coordinator
State Historic Preservation Office
109 East Jones St. MSC 4617
Raleigh, NC 27699

**RE: Section 106 Consultation
Capacity Enhancement Projects Environmental Assessment
Charlotte Douglas International Airport (CLT)**

Dear Ms. Gledhill-Earley:

As part of the Federal Aviation Administration's (FAA's) Section 106 review and pursuant to 36 CFR § 800.4, the FAA has undertaken identification efforts for the proposed capacity enhancement projects at the Charlotte Douglas International Airport (CLT), Charlotte, NC. At this juncture, we are (1) formally seeking your concurrence on both the above and below-ground Areas of Potential Effect (APE), (2) summarizing the identification efforts that have been completed, and (3) providing our eligibility assessment for the resources identified for your review.

Areas of Potential Effect:

The proposed APE for above-ground resources is based on the combined elements of the proposed undertaking and their potential to cause adverse effects directly by construction/demolition and indirectly through visual impacts. In addition, the APE considers input provided by your office during previous correspondence on the proposed undertaking. Figure 1 graphically depicts the proposed above-ground (Indirect) APE.

The proposed APE for below-ground resources is predicated on the estimated construction limits of the proposed undertaking for all project elements. Figure 2 illustrates the below-ground (Direct) APE.

Identification Efforts:

For above-ground resources, our effort included identifying structures within the entire limits of the APE. Once identified, each structure was assessed for eligibility with the National Register of Historic Places (NRHP).

With respect to below-ground resources, the identification effort was limited to those areas that had not been previously surveyed and are not on existing airport property. This approach was based, in part, on your correspondence dated May 11, 2018. As such, the primary identification effort included the estimated construction limits of the West Boulevard Relocation.

Eligibility Assessment:

For above-ground resources, a Historic Resources Survey (HRS) was conducted within the APE between February 3, 2020 and March 4, 2020. The survey identified a total of 599 above ground resources that were either 50 years of age or greater and properties that would be 50 years of age or greater as of 2033. The following list summarizes the results of the survey:

Properties	Number
Listed in the NRHP	1
Previously Determined Eligible for NRHP	8
Newly Recommended as Eligible for NRHP	56
Recommended as Potentially Eligible for NRHP	138
Recommended as Potentially Eligible for NRHP by 2033	6
No Access	42
No Longer Extant	106
<u>Recommended as Not Eligible for NRHP</u>	<u>242</u>
Total Properties Surveyed	599

Based on an April 3, 2020 call between Environment & Archaeology, LLC (E&A) and your office, 30 properties were identified requiring more or future evaluations. As a result, the 30 properties were further evaluated for potential eligibility in the NRHP (see Attachment 1 HRS Report) and the recommendations are provided in Table 1. To meet the NCDNCR SHPO submittal requirements, E&A will be sending to your office:

- A hard copy of the HRS report with all appendices.
- A digital copy of the report with all appendices on a CD.
- A hard copy of ALL of the resources recorded in the database from the NC SHPO. This is currently spread out between three - 2-inch 3-ring binders. E&A has scanned the database forms regarding this task.
- Digital copies of ALL of the resources E&A recorded in the NC SHPO database on a CD.

Table 1. Properties Further Evaluated for Potential Eligibility in the APE

SITE #	Property	NRHP RECOMMENDATION
PREVIOUSLY RECORDED PROPERTIES		
MK1364/ MK3993	William Grier House (former MK3659)	Eligible*
MK1377	Steele Creek Presbyterian Church	Listed in NRHP
MK1361	John Douglas House*	Eligible*
MK1875	Spratt-Grier Farm*	Eligible*
MK2150	Oakden Motel	Recommended Eligible
MK2189	Old Terminal Building	Recommended Eligible
MK3761	WPA Douglas Airport Hanger (former MK2399)	Recommended Eligible
MK1378	Steele Creek Manse*	Eligible*
MK2841	Byrum Croft House*	Eligible*
MK3192	Fred Webber House	Recommended Eligible
MK3197	Two Guys Auto Repair Body Shop	Recommended Eligible
NEWLY RECORDED PROPERTIES		
MK3770	3100 Yorkmont Road ¹	Recommended Not Eligible
MK3762	3140 Yorkmont Road ²	Recommended Not Eligible
MK3798	FAA Control Tower ³	Recommended Eligible
MK3816	CLT Box Building ¹	Recommended Not Eligible
MK3822	Big Spring Methodist Church	Recommended Not Eligible
MK3823	Officer's Club	Recommended Eligible
MK3825	Thomas W. Ferebee Building	Recommended Eligible
MK3836	9311 Markswood Road	Recommended Not Eligible
MK3853	Cold War Era Fallout Shelter	Recommended Potentially Eligible
MK3960	Housing Building #4	Recommended Eligible
MK4127	Ashley/Eaton Circle Development	Recommended Potentially Eligible
MK3933	Charlotte Speedway Ticket Office/Barn	Recommended Not Eligible
MK4004	Harvest Center Properties: NCO Club and Morris Field Baptist Chapel	Recommended Eligible
MK4023	West Mecklenburg High School	Recommended Not Eligible
MK4047	5455, 5457, 5459, and 5461 Wilkinson Boulevard	Recommended Eligible
MK4115	4824 Parkway Plaza Boulevard ²	Recommended Not Eligible
MK4123	4828 Parkway Plaza Boulevard ⁴	Recommended Not Eligible
MK4126	Stonewall Jackson Homes	Recommended Eligible
MK4407	Steeleberry Drive Neighborhood	Recommended Potentially Eligible

*Covered under existing Memorandum of Agreement (MOA) between the FAA and North Carolina State Preservation Office (SHPO), and the City of Charlotte.

Notes:

1. Recommended as Potentially Eligible in 2031
2. Recommended as Potentially Eligible in 2032
3. Recommended as Potentially Eligible in 2029
4. Recommended as Potentially Eligible in 2033

Our survey effort for below-ground resources identified four (4) archaeological sites within the West Boulevard Relocation Area. All of the archaeological sites were determined not eligible for listing in the National Register of Historic Places (NRHP). For additional details, please refer to the Archaeological Survey and Site Evaluation for the West Boulevard Relocation Area.

As noted above, we are seeking concurrence from your office for the APEs. In addition, we are asking for your concurrence to our eligibility assessments that are listed above. If you have any questions, please contact Tim Alexander at timothy.l.alexander@faa.gov or at (901) 322-8188.

Sincerely,

Tommy L. Dupree
Manager, Memphis Airports District Office

FIGURE 1. Major Capacity Projects EA – Indirect APE

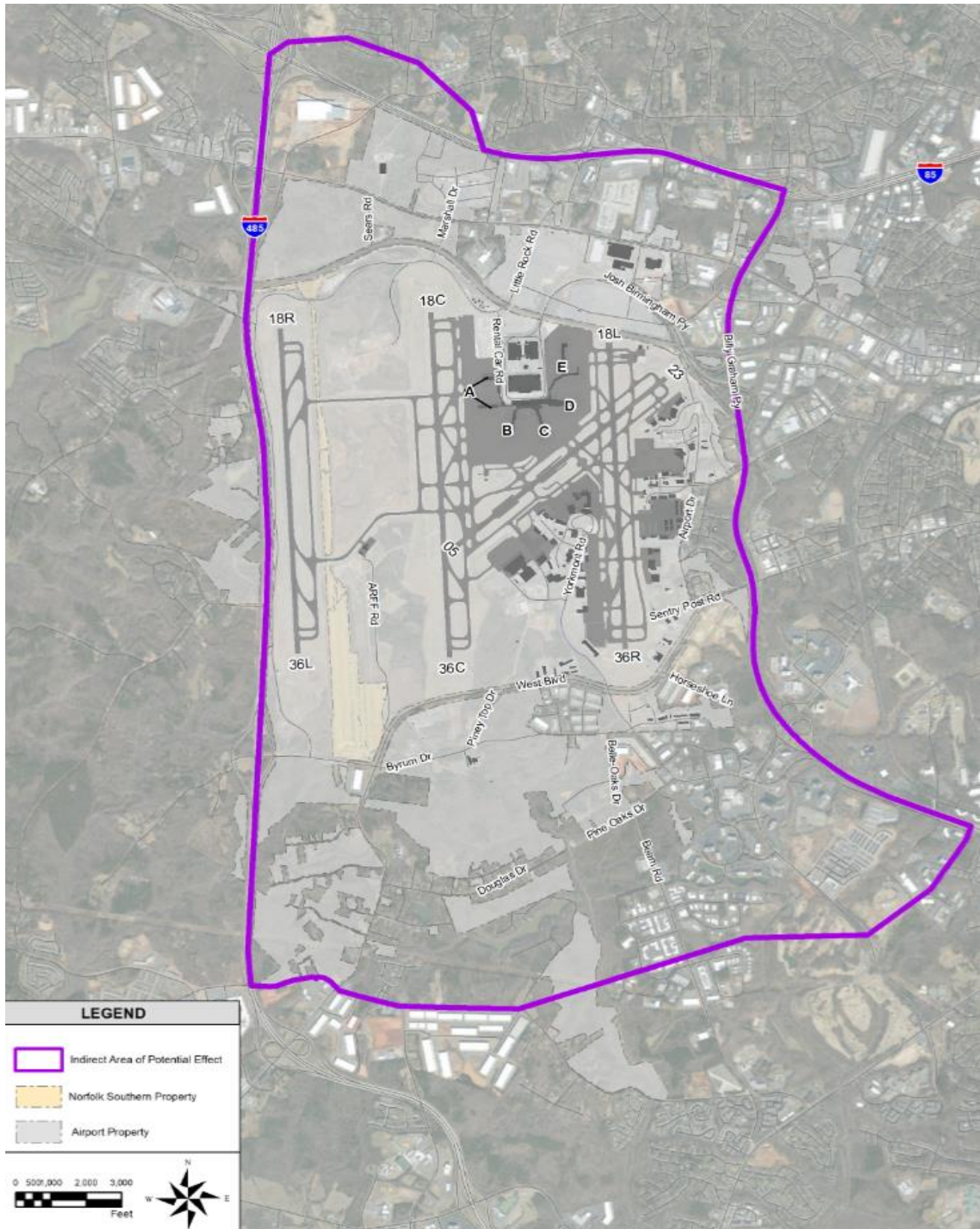
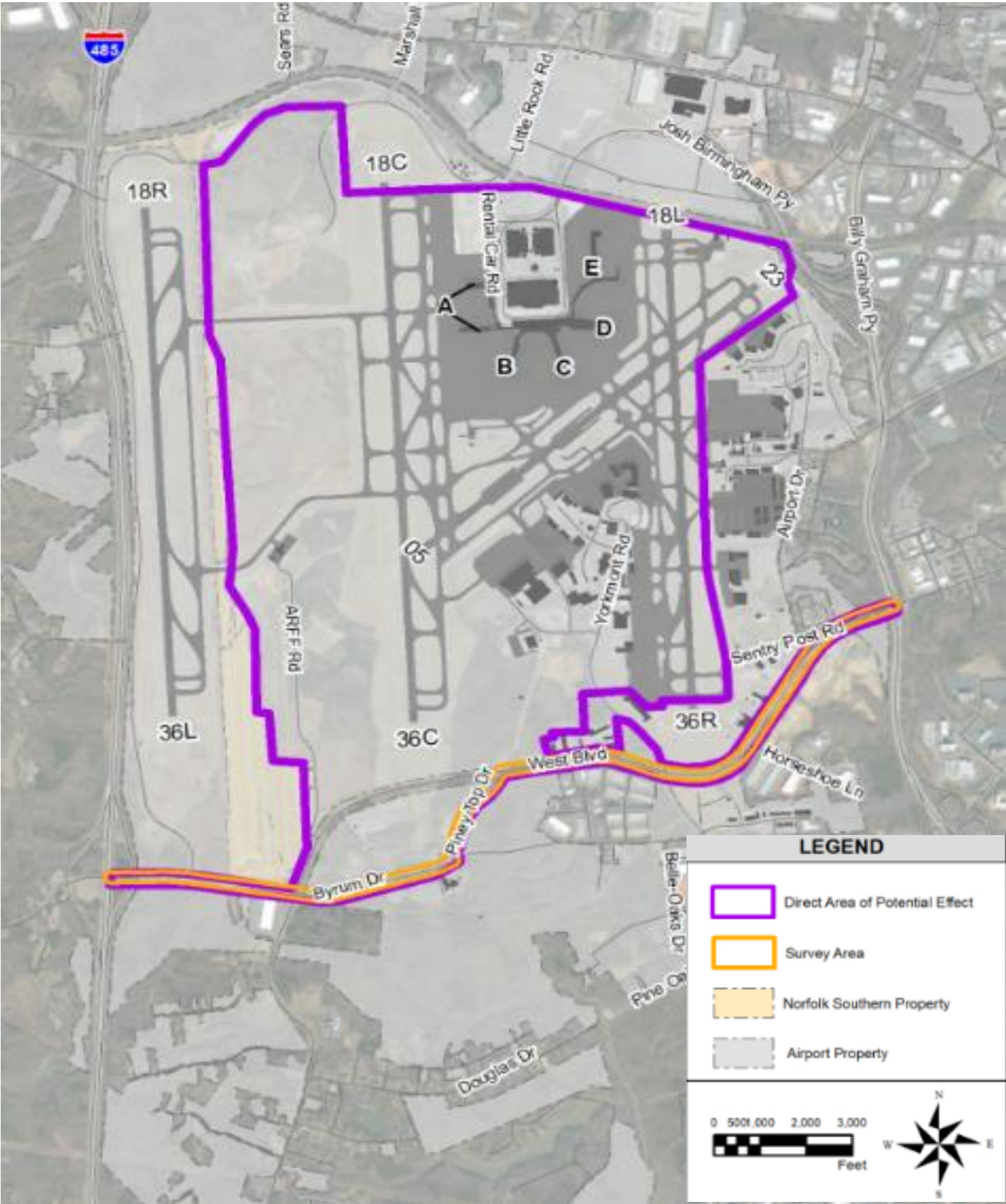


FIGURE 2. Major Capacity Projects EA – Direct APE



DRAFT

**HISTORIC RESOURCES REPORT
CHARLOTTE DOUGLAS INTERNATIONAL AIRPORT
HISTORICAL RESOURCES SUMMARY PROJECT
MECKLENBURG COUNTY, NORTH CAROLINA
ER# 20-0438**

PREPARED FOR:

Federal Aviation Administration
Memphis Airports District Office
2600 Thousand Oaks Blvd., STE 2250
Memphis, TN 38118-2462
(901) 322-8197

PREPARED BY:

Environment & Archaeology, LLC
221 Main Street
Florence, KY 41042
Office: (859) 746-1778

Margo Warminski, Architectural Historian
Courtney Stoll, Principal Investigator
Luke Erickson, Project Manager

June 2020

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INTRODUCTION

Environment & Archaeology, LLC was requested to perform an Historic Resources Survey (HRS) within the vicinity of the Charlotte Douglas International Airport (CLT) in Charlotte, Mecklenburg County, North Carolina (Figure 1). The survey had two primary purposes:

1. To identify all properties within the Area of Potential Effect (APE) that were 50 years of age or greater.
2. To identify additional properties within the APE that would be 50 years of age or greater as of 2033.
3. Provide recommendations as to each property's eligibility for listing in the National Register of Historic Places (NRHP). Properties less than 50 years of age would be assessed for their potential to be eligible for the NRHP once they reached 50 years of age.

The HRS was performed between February 3, 2020 and March 4, 2020. This report provides the results and recommendations from the survey. A total of 599 properties were identified during the survey. The following list summarizes the results of the survey.

Properties Listed in the NRHP	1
Properties Previously Determined Eligible for NRHP	8
Properties Newly Recommended as Eligible for NRHP	56
Properties Recommended as Potentially Eligible for NRHP	138
Properties Recommended as Potentially Eligible for NRHP by 2033	6
Properties with No Access	42
Previously Recorded Properties No Longer Extant	106
Properties Recommended as Not Eligible for NRHP	<u>242</u>
 Total Properties Surveyed	 599

The evaluation of properties included in this report was prepared by *Environment & Archaeology, LLC* Architectural Historians Courtney Stoll and Margo Warminski with report compilation provided by *Environment & Archaeology, LLC* Project Manager/Principal Investigator Luke Erickson.

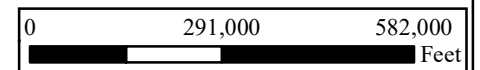
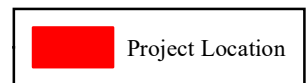
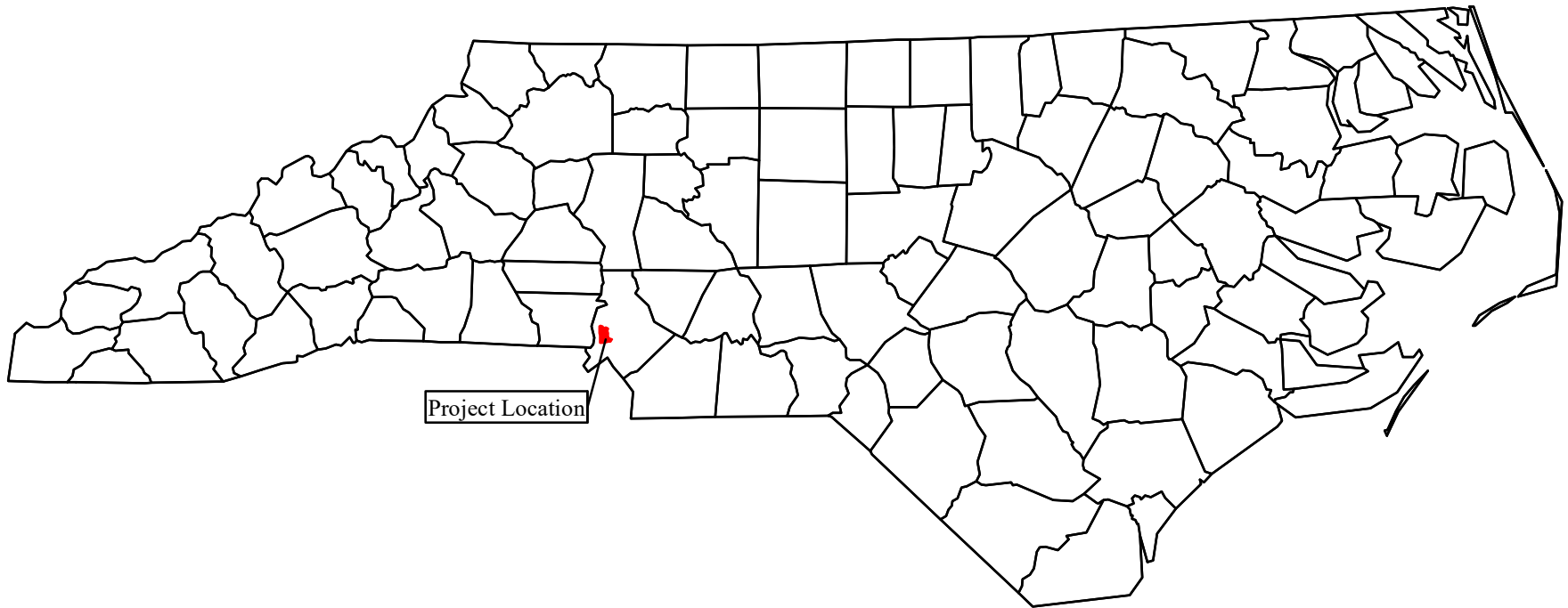


Figure 1

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

State Map of North Carolina with Project Location

Environment & Archaeology
LLC

DESCRIPTION OF AREA OF POTENTIAL EFFECT

The Federal Aviation Administration (FAA) and North Carolina Department of Natural and Cultural Resources (NCDNR) agreed upon the Area of Potential Effect (APE) for the Proposed Project. Figure 2 shows the boundaries of the proposed APE. The APE contains approximately 8,993.5 acres of land extending around the existing airport in Mecklenburg County, North Carolina. This area is roughly bounded by I-485 to the west, by I-85 to the north, and by Billy Graham Parkway to the east. The southern boundary follows Shopton Road as it travels east-west. Once Shopton Road turns to the southeast, the APE boundary continues to the east to meet Billy Graham Parkway. In general, the APE consists of the CLT, numerous housing subdivisions, industrial parks, business parks and heavily forested areas. The Survey Area for this report includes the entirety of the APE.

Within the APE, the project will have areas of Direct Effect, where earth will be disturbed through construction, and areas of Indirect Effect where issues related to noise may result in actions to be taken regarding specific properties. The Survey Area for this report identifies structures within the Direct and Indirect APEs. The report does not specify what, if any, actions may occur to these properties. Future consultation under Section 106 of the National Historic Preservation Act will occur between the FAA and NCDNR concerning potential actions affecting historic properties recommended as eligible or potential eligible for listing in the National Register of Historic Places.

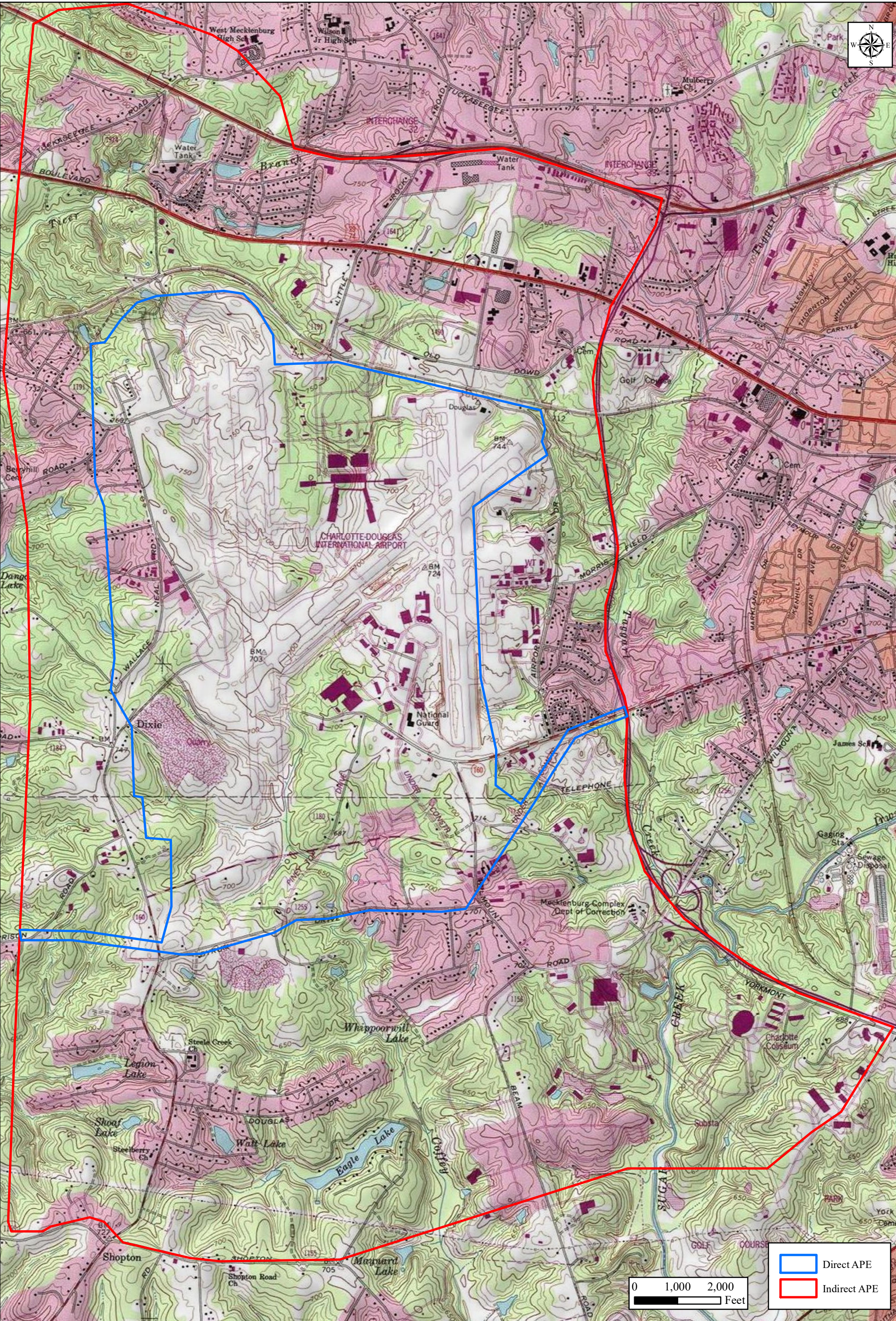


Figure 2	Charlotte Douglas International Airport Historical Resources Summary Project Mecklenburg County, North Carolina	7.5' USGS Charlotte West and Mountain Island Lake, North Carolina Quadrangles <i>Environment & Archaeology</i> LLC
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EVALUATION PROCESS

The historical significance and integrity of these historic properties were evaluated using the criteria outlined in the National Register bulletin, *How to Apply the National Register Criteria for Evaluation* (National Park Service [NPS] 1997). The *National Register of Historic Places* (NRHP) recognizes four categories of significance of a resource possessing integrity of location, design, setting, materials, workmanship, feeling and associations and:

- A. That are associated with events that have made a significant contribution to the broad patterns of our history,
- B. That are associated with the lives of persons significant in our past,
- C. That embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that represent a significant and distinguishable entity whose individual components may lack individual distinction, and
- D. That have yielded, or may be likely to yield, information important in prehistory or history.

For a property to qualify for the NRHP it must meet one of the NRHP criteria for evaluation by being associated with an important historic context, such as agriculture, transportation or architecture, and retaining historic integrity of those features necessary to convey its significance (NPS 1997: 44).

Integrity is the ability of a property to convey its significance. Within the concept of integrity, the NRHP criteria recognize seven aspects or qualities that, in various combinations, define integrity (NPS 1997: 44).

Location is the place where the historic property was constructed or the place where the historic event occurred. The actual location of a historic property, complemented by its setting, is particularly important in recapturing the sense of historic events and persons. Except in rare cases, the relationship between a property and its historic associations is destroyed if the property is moved. Resources therefore retain integrity of location if they remain on their original sites (ibid.).

Design is the combination of elements that create the form, plan, space, structure, and style of a property (ibid.: 44). It includes such elements as organization of space, proportion, scale, technology, ornamentation, and materials (ibid.). form, shape, proportions and solids to voids. Resources retain integrity of design if their original form, scale, footprint and pattern of piercing remain evident.

Setting is the physical environment of a historic property. It refers to the character of the place where they property played its historical role. The physical features that constitute the setting of a historic property can include topography, vegetation, simple manmade features, and relationships between buildings and other features or open space (ibid.: 45). Resources retain integrity of setting if they retain these qualities.

Materials are the physical elements that were combined or deposited during a particular period of time and in a particular pattern or configuration to form a historic property. Indigenous materials are often the focus of regional building traditions and thereby help define an area's sense of time

and place (ibid.). Resources possess integrity of materials if they retain key exterior materials dating from the period of their historic significance.

Workmanship is the physical evidence of the crafts of a particular culture or people during any period in history or prehistory. It is evidence of artisans' labor and skill in constructing or altering a building or structure. Examples in historic buildings include tooling, carving, painting, graining, turning and joinery (ibid.). Resources retain integrity of materials if they retain these features.

Feeling is a property's expression of the aesthetic or historic sense of a particular period of time. It results from the presence of physical features that, taken together, convey the property's historic character. If resources possess sufficient integrity of design, workmanship, materials, location and setting to be identified as products of their particular time and place in history, they will convey the feeling of that place and time (ibid.).

Association is the direct link between an important historic event or person and a historic property. Like feeling, it requires the presence of physical features that convey a property's historic character. A resource retains association if it is the place where the event or activity occurred and is sufficiently intact to convey that relationship to an observer (ibid.).

The steps in assessing integrity are:

- define the essential physical features,
- determine whether they are visible,
- compare the property with similar resources in the area, and
- determine, based on the significance and essential physical features, which aspects of the property are particularly vital to the property and if they are present.

Ultimately, the question of integrity is answered by whether the property retains the identity for which it is significant (ibid.: 45-48).

BRIEF HISTORY OF THE APE

The following section provides a brief historical context of the APE. This context is intended to provide background for the important historical activities within the APE, particularly as they relate to the properties evaluated in this report. Where relevant, specific properties that are present within the APE are discussed as they relate to this historical context. Each of these structures is further discussed in the Site Description section of this report.

Charlotte is North Carolina's largest city, with a population exceeding 600,000. Initially, "Charlotte was a small country crossroads whose dramatic growth into a major city reflects the emergence of Piedmont North Carolina into a leading area of commercial development in the United States" ("Charlotte," *NCpedia* [<https://www.ncpedia.org/charlotte/>]). By the mid-18th century, newly arrived European immigrants had traveled the Great Wagon Road south from Philadelphia into the Carolina backcountry and established small communities along that busy thoroughfare. Mecklenburg County was created in 1762 to accommodate these new settlements, and Charlotte became the county seat in 1768. Located in North Carolina's Piedmont region, Mecklenburg was formed from Anson County and took its name from the German duchy of Mecklenburg-Strelitz (ibid.).

"After the Revolutionary War, North Carolina's hinterland began to flourish" (ibid.). The discovery of gold nearby drew settlers to the vicinity. Several banks were opened in the town, which by the mid-19th century was becoming the heart of the Carolina backcountry. "In the mid-1700s, Scotch-Irish settlers established what would later become Charlotte at the intersection of two Indian trading routes near the Catawba River" (ibid.). Like most cities in the state, "Charlotte emerged from the Civil War with its infrastructure largely intact, ready for renewed growth" (ibid.).

"In the postwar years, Charlotte became one of the leading cities of the New South" (ibid.). The state's booming textile industry intensified economic activity through the area, and Charlotte experienced rapid development. In the 20th century, Charlotte continued to expand and modernize, mirroring the rise of central North Carolina as one of the nation's most prosperous regions, home to some of its largest banking institutions (ibid.).

Numerous industries have relocated to Charlotte to take advantage of the city's position as a major transportation hub with an expanding mass transit system, major airport, and several highways. The Charlotte Douglas International Airport is a civil-military airport and the second-largest hub for American Airlines. As of 2019 it ranked as the 11th-busiest airport in the United States (ibid.). Between 2007 and 2015, the airport completed \$1.5 billion worth of construction projects, part of which later became known as the "CLT 2015" plan, and more improvements are planned for the future (ibid.).

The small Charlotte Municipal Airport (later renamed for Charlotte Mayor Ben Douglas) was taken over by the Army several months prior to the attack on Pearl Harbor. The outbreak of World War II caused the Army Air Force to take control of the airport as well as a larger tract to the southwest. The federal government spent six (6) million dollars to develop Morris Field/CLT into

a pilot training base where pilots were trained to fly fighter aircraft. The Army constructed barracks to house 1,800 enlisted men as well as a church and mess halls. Some of these buildings are still extant and have been repurposed for new uses. An example is the former Morris Field/CLT Officers Club-Civil Air Patrol Headquarters (MK3823), built in 1941 and is recommended eligible for NRHP listing under Criterion A for military history and social history.

In 1946, the federal government returned portions of the airport to local officials. Some barracks and other structures were converted to apartments to relieve the postwar housing shortage in the area. In addition, a local construction company built off-base officer apartments, some of which may still be extant. While no individual project was called out, the author may have been referring to the Stonewall Jackson Homes (MK4123), a complex of single- and multi-family brick residences located across the road from the airport. These apartments were occupied by veterans once peace returned and were offered for sale to them in 1946. *Environment & Archaeology, LLC* recommended the Stonewall Jackson Homes for National Register listing under Criterion A because of their association with wartime and post-war housing. Also recommended for listing under Criterion A, for the same reason, were the Eaton Apartments (MK3960), a nearby apartment complex built on Eaton Drive in 1941, and the neighboring Ashley Circle homes (MK3981).

In 1948, an Air National Guard base was established north of the former Morris Field/CLT to provide continental air defense. In 2013, the North Carolina State Historic Preservation Office (NCSHPO) commissioned a cultural resource survey of the base, which discovered nine (9) historic properties. Of these, all but two (2) did not qualify for National Register listing due to age, and the remaining two (2) lacked integrity due to alterations. It was also concluded that the Air National Guard Base installation did not qualify as a historic district.

The Charlotte airport saw important improvements beginning in the 1950s, including a runway extended to accommodate jet aircraft, and a new, Modernist passenger terminal, which opened in 1954 (MK2189). Designed by noted local architect Walter Hook, the building retains a high degree of integrity. When the present terminal was completed, the 1954 building was used for a time as a cargo terminal. *Environment & Archaeology, LLC* recommended the 1954 terminal be evaluated in the context of other Modernist public buildings of the era to determine if it meets National Register criteria.

An airport landmark of a later period, the FAA Control Tower (MK3798), was built in 1979. *Environment & Archaeology, LLC* recommended it be considered for listing under Criterion C as a significant example of Modernist/Brutalist architecture once it reaches 50 years of age in 2029. The airport was considered for National Register district designation in the past but was found to lack sufficient integrity for Register listing.

In the years after World War II, pent-up demand for housing, and postwar federal programs intended to subsidize home ownership, led to a proliferation of housing construction, especially in new suburbs. A notable local example is the Steeleberry Subdivision, where a number of wartime

pilots are said to have made their homes. While the houses for the most part lack individual distinction, the subdivision as a whole exhibits a strong sense of place characterized by large lots, consistent setbacks, winding streets, linear plans (ranch, split-level, L-plan), horizontal orientation, attached garages and porte-cocheres. A characteristic example of a Steeleberry residence is 7724 Cooper Lane (MK4097), a one-story ranch-style house with low-hipped roof, end chimney and multi-light metal windows.

Local tradition holds that numerous Steeleberry veteran/owner-built fallout shelters on their properties during the Cold War. One has been identified so far at 9001 Whispering Pines Lane (MK3853). *Environment & Archaeology, LLC* recommended further research on these unique resources to assess their potential eligibility as part of a group nomination or as contributing resources of a potential Steeleberry Historic District.

The suburbanization of America in the 1950s and 1960s, and widespread ownership of automobiles, gave rise to new, informal lifestyles and ways of getting around. Wilkinson Boulevard, the first four-lane paved highway in all of North Carolina, attracted new kinds of businesses to cater to the needs of motorists. Several historic properties on the Boulevard that housed these businesses have been recommended eligible for Register listing. A former filling station (MK3197), built in 1927, was recognized as a significant example of an early 20th c. gas station influenced by suburban domestic architecture. The Oak Den Motel at 5104 Wilkinson Boulevard (MK2150) is an early example of a linear, one-story motel that catered to auto travelers; it is very well preserved. It was recommended eligible under Criterion C by the firm of Mattson, Alexander & Associates in 2005, with concurrence by the NCSHPO.

A multi-unit early strip shopping center of Mid-Century Modern design (MK4047), located at 5455-5461 Wilkinson Boulevard, was recommended eligible by *Environment & Archaeology, LLC* under Criterion C as a notable example of a small postwar suburban strip center in remarkably original condition.

Entrepreneur Fred Webber prospered from his restaurant chain that expanded through the South. His well-preserved bungalow home at 5300 Wilkinson Boulevard (1931, MK3192) is recommended eligible by *Environment & Archaeology, LLC* under Criterion B for association with innovations in eateries during the early 20th century.

The new sport of automobile racing gained popularity in Charlotte during the mid-20th century. The former Charlotte Speedway (MK3993) was built in 1946 and remained in operation for ten years. Eventually key buildings and the track itself were removed, leaving only a barn and a possible ticket office standing. These properties were recommended by *Environment & Archaeology, LLC* as not eligible because of diminished context and integrity, and because the track site suffered in comparison with another, much more complete example of an early racetrack elsewhere in the state that was already Register-listed. (See Jennifer Martin, Sarah Woodard and Virginia Freeze, "Oconechee Speedway," National Register of Historic Places Nomination,

August 1, 2001. Available online at North Carolina State Historic Preservation Office web site: <https://www.ncdcr.gov/about/history/division-historical-resources/nc-state-historic-preservation-office>.)

In the late 20th century, trucking became a pillar of Charlotte/Mecklenburg's regional economy. This led to the creation of a new kind of commercial building, the truck terminal. Two local examples built in the 1970s and 1980 reflect the influence of new Modernist architectural styles: Post-Modernism and Brutalist. *Environment & Archaeology, LLC* suggested several built in the 1970s and 1980s be considered for listing once they reach 50 years of age.

SURVEY METHODS AND RATIONALE

Environment & Archaeology, LLC was directed to investigate the APE/Survey Area with an expectation that potential impacts to historic properties could occur over the span of more than a decade. Therefore, the Survey Area was reviewed with an expectation that the results should make recommendations on properties built as late as 1983. As a result, properties constructed prior to 1983 were identified and assessed for their NRHP eligibility or for their potential to be eligible for the NRHP within the timeframe of the proposed Project.

A survey was conducted between February 3, 2020 and March 4, 2020. Properties included in the survey were identified based on a desktop review of the following sources:

- Mecklenburg County, North Carolina, PVA Database
- Google Earth
- Google Maps
- ArcMap
- HPO Web
- Available Internet Real Estate web pages such as Trulia.com
- Mecklenburg County Charlotte, North Carolina, Historical Landmark Commission
- Shapefiles from Landrum & Brown of prior survey work

Field efforts included identifying any additional properties within the APE/Survey Area that were not identified during the desktop review. All accessible properties were photographed and assessed according to *How to Apply the National Register Criteria for Evaluation* (National Park Service [NPS] 1997).

PRIOR MEMORANDA OF AGREEMENT

Portions of this reports' Survey Area have been surveyed in the past. Certain properties within the Survey Area were already considered eligible for listing in the NRHP and have been documented in a Memorandum of Agreement (MOA) between the NCSHPO and the Federal Aviation Administration (FAA) (1999 amended in 2018). The following properties included in the MOA and its Amendment and included in the current survey area were:

- John Douglas House (MK 1361)
- Spratt-Grier Farmhouse and Slave House (MK 1875)
- William Grier House (MK 3659)
- Byrum-Croft House (MK 2841)
- Steele Creek Presbyterian Church Manse (MK 1378)

These properties were photographed during the HRS and are documented in this report. No further recommendations of eligibility are provided in this report.

PREVIOUSLY SURVEYED RESOURCES WITHIN THE APE

Based on consultation with the NCDNCR, it was determined that 108 historic properties had been previously documented within the Project Area (Figure 3). The NRHP recommendation, determination or status for these resources is provided in Table 1. Previously recorded properties that have been demolished were field-verified.

Table 1. Previously Recorded Historic Resources within the APE/Survey Area.

SSN	Site Name	Location	Year	Status/ NRHP Eligibility
MK1357	Cathey House	NE side NC 160 .1mi SE of jct w/ SR 1148	c. 1840	No longer extant
MK1361	John Douglas House	N side SR 1152 (Christie Ln), 0.2 mi NE jct w/ NC 160 (W. C*	c. 1867	Listed, Landmark
MK1363	Freeman House	W side NC 160 .1 mi N junction SR 1148	1914	No longer extant
MK1368	Herron House	SE side SR 1148 .6 mi SW of jct W/ NC 160	Not	Unknown
MK1369	House	North side of SR 1662, 0.1 mile NE of jct w/ US85	c. 1880	Unknown
MK1371	McCorkle House	North side SR 1155 0.3 mi East of jct w/ NC 160 Steele*	Unknown	Unknown
MK1373	Dr. Richard Z. Query House	W side NC 160 (Steele Creek Rd./Dixie Rd) at jct. w/ SR 118*	c. 1880	No longer extant
MK1377	Steele Creek Presbyterian Church	7407 Steele Creek Rd, Charlotte	1889	Listed, Landmark
MK1378	Steele Creek Presbyterian Church	West side NC 160, 0.7mi NE of jct w/ SR 1155 7500 Steele C*	1910	Listed, Landmark
MK1697	Service Station	NW corner US 29/74 and SR 1641	c. 1960	No longer extant
MK1698	J. W. Auten House	East side SR 1180 .2 mi S of jct w/ NC 160	N/A	No longer extant
MK1699	House	NE corner SR 1662 and SR 1637	c. 1930s	No longer extant
MK1705	House	N side Old Dowd Rd (SR 1191) across junction w/ SR 1193	Late 19 th	No longer extant
MK1706	Hovis-Grier-Cochran House	E side Wilmount & Beam Rd SR1177.1 mi NW junction w/	c. 1903	No longer extant
MK1709	McCoy Service Station	5315 Wilkinson Blvd, Charlotte	1927	No longer extant
MK1710	Robert McGinn House	West side Piney Top Dr (SR1180)	c. 1855	Unknown
MK1873	Asbury House	6901 Wilkinson Blvd. (at Sears Rd. Charlotte	1925	Unknown
MK1874	Samuel Brown Farm	End of Farm Lane 1/4 mi. N. Garrison Rd., W. of Dixie Rd *	19 th C.	No longer extant
MK1875	Spratt-Grier Farm	8006 Robbie Circle Charlotte	Mid 19 th C.	Unknown
MK1909	Tenant house	N side Old Dowd Rd (SR 1191) across jct w/ SR 1193	Early 20 th	No longer extant
MK2101	Commercial Buildings	5104-6225 Wilkinson Boulevard Charlotte	Unknown	Potentially Eligible
MK2134	Paul Cathey House	S side Byrum Drive between Timberly & Yorkmount	Mid 20 th c.	No longer extant
MK2149	Charlotte Motor Court	5616 Wilkinson Boulevard Charlotte	c. 1959	No longer extant
MK2150	Oakden Motel	5104 Wilkinson Blvd. Charlotte	1952	Listed
MK2154	Akers Motor Lines Inc.	4101 S. I-85 Service Road Charlotte	c. 1963	No longer extant
MK2189	Douglas Municipal Airport	Terminal Drive Charlotte	c. 1955	Potentially Eligible
MK2202	Valentine Barn	End of Timberly Place, S sd Byrum Drive	1930s	No longer extant
MK2214	Byrum House	7611 Byrum Drive Charlotte	1938	Not Eligible
MK2220	Berryhill Baptist Church	7438 Old Dowd Road Charlotte	1952	No longer extant
MK2234	Henry Wingate Rental House I	N side of Byrum Drive, E of Piney Top	Mid 20 th c.	No longer extant
MK2236	Henry Wingate Rental House #2	N side Byrum Drive, 4th E of Piney Top	Unknown	No longer extant
MK2258	Berryhill House	8109 Old Dowd Rd, Charlotte	Unknown	No longer extant
MK2260	Parnell House	8406 Trexler Street Charlotte	c. 1925	No longer extant
MK2289	McDowell House	7924 Old Dowd Road Charlotte	c. 1930	No longer extant
MK2292	Mitchell House	7831 Old Dowd Road, Charlotte	c. 1925	No longer extant

Table 1 (cont.). Previously Recorded Historic Resources within the APE/Survey Area.

SSN	Site Name	Location	Year Built	Status/ NRHP Eligibility
MK2310	B. A. Kendrick House	7751 Old Dowd Road, Charlotte	c. 1925	No longer extant
MK2323	Simpson House	7729 Old Dowd Road Charlotte	1948	No longer extant
MK2324	Lowder	7711 Old Dowd Road Charlotte	1947	No longer extant
MK2327	House	N side Walkers Ferry Road, 1/8 mi W of Wallace Neal Road	c. 1950	No longer extant
MK2329	House	N side of Walkers Ferry Road @ W of Wallace Neal Road	c. 1925	No longer extant
MK2332	House	8624 Walkers Ferry Road, Charlotte	c. 1925	No longer extant
MK2337	Walker Homestead	8723 Walkers Ferry Road, Charlotte	c. 1830	No longer extant
MK2338	House	S side of Walkers Ferry Road, W of Berry Hill Cemetery	Unknown	No longer extant
MK2339	Berryhill Baptist Church Retreat	7438 Old Dowd Road, Charlotte	c. 1916	No longer extant
MK2356	House	7224 Newell Rd, Charlotte	c. 1920	No longer extant
MK2408	House	S side Newell Road, W of Wallace Neal Rd, Charlotte	c. 1920	No longer extant
MK2412	Jack Patterson House	9801 Dixie River Road Charlotte	1938	No longer extant
MK2413	Palmer House	9539 Dixie River Road Charlotte	c. 1925	No longer extant
MK2418	House	W side of Wallace Neal Rd, South of Old Dowd Rd,	c. 1925	No longer extant
MK2419	Howard Brown House	4304 Wallace Neal Road Charlotte	c. 1925	No longer extant
MK2420	McGinniss House	4620 Wallace Neal Road Charlotte	Unknown	No longer extant
MK2421	House	W side of Dixie Rd, S of Garrison Rd	c. 1940	No longer extant
MK2422	McGee House	4800 Wallace Neal Road Charlotte	c. 1945	No longer extant
MK2423	House	5712 Wallace Neal Rd, Charlotte	c. 1930	No longer extant
MK2424	Griffith Barn	5726 Dixie Rd, Charlotte	Unknown	No longer extant
MK2425	Neel House	6312 Dixie Road Charlotte	c. 1910	No longer extant
MK2426	House	SW corner of Wallace Neal Road & Mountain View,	c. 1910	No longer extant
MK2427	Griffith-Shoaf House	7820 Steel Creek Rd, Charlotte	c. 1890	Not Eligible
MK2428	House	W side of Steel Creek Rd, S of Steeleberry Dr, Charlotte	Unknown	No longer extant
MK2429	House	7408 Steel Creek Road Charlotte	c. 1925	No longer extant
MK2430	House	W side Wallace Neal Rd, N of Nannie Price Rd, Charlotte	Unknown	No longer extant
MK2431	Tenant House	W side Wallace Neal Road, N of Garrison, Charlotte	c. 1930	No longer extant
MK2432	Boyles House	6217 Wallace Neal Road	c. 1950	No longer extant
MK2434	House	S side Tuckaseegee Rd at Todd Rd, Charlotte	Unknown	No longer extant
MK2435	Todd House	8311 Todd Rd, Charlotte	Unknown	No longer extant
MK2436	House	N side Todd Rd, W of Tuckaseegee Rd, Charlotte	Unknown	No longer extant
MK2437	House	8007 Tuckaseegee Road Charlotte	c. 1880	No longer extant
MK2438	House	3134 Little Rock Road Charlotte	c. 1925	No longer extant
MK2439	House	3145 Little Rock Road Charlotte	c. 1925	No longer extant
MK2440	House	7744 Newell Road	c. 1945	No longer extant
MK2490	Brown Farmstead	W side Dixie Road (West Charlotte Outer Loop Project) *	c. 1929	No longer extant
MK2841	Byrum-Croft House	West side NC 160, 0.1 mi S jct w/ SR 1255 7100 Steele	Early 20 th	Listed
MK2845	House	North side Dixie River Rd., 0.1 mi. E jct w/ I-485	c. 1925	Not Eligible
MK2933	W.P.A Douglas Airport Hangar	4108 Minuteman Way, Charlotte	1936-1937	Listed
MK3152	Warehouse (Bldg. 001)	Payne Blvd. Charlotte	1956	Not Eligible
MK3153	NC ANG Headquarters (Bldg. 002)	Payne Blvd. Charlotte	1960	Not Eligible
MK3154	Operations Building (Bldg. 003)	Charlotte	1977	Not Eligible
MK3155	Nose Dock Maintenance Building	Charlotte	1968	Not Eligible
MK3156	Training Facility (Bldg. 005)	Charlotte	1982	Not Eligible

Table 1 (cont.). Previously Recorded Historic Resources within the Project Area.

SSN	Site Name	Location	Year Built	Status/NRHP Eligibility
MK3158	Aviation Fuels Office (Bldg. 039)	Charlotte	1984	Not Eligible
MK3157	Maintenance Facility (Bldg. 007)	Payne Blvd. Charlotte	1974	Not Eligible
MK3160	Checkpoint (Bldg. 041)	1st Union Road Charlotte	1985	Not Eligible
MK3161	Civil Engineering Maintenance Facility (Bldg. 043)	Charlotte	1986	Not Eligible
MK3162	Aeromedical Evacuation Squadron Headquarters (Bldg. 045)	Charlotte	1990	Not Eligible
MK3163	Hazardous Storage (Bldg. 048)	Charlotte	1991	Not Eligible
MK3164	Hazardous Materials Pharmacy (Bldg. 049)	Charlotte	1990	Not Eligible
MK3165	Reserve Forces Training Facility (Bldg. 050)	Charlotte	1958	Not Eligible
MK3166	Gymnasium (Former Armory) (Bldg. 069)	Charlotte	1975	Not Eligible
MK3167	Fuel Pump (Bldg. 131)	Charlotte	1984	Not Eligible
MK3168	Fuel Pump (Bldg. 132)	Charlotte	1984	Not Eligible
MK3169	NC ANG Memorial	Charlotte	Unknown	Not Eligible
MK3170	F-86A Sabre Fighter (Static Display) (Bldg. 144)	Charlotte	2011	Not Eligible
MK3171	C-130 Hercules (Static Display) (Bldg. 151)	Charlotte	2010	Not Eligible
MK3189	House (Gone)	5106 Wilkinson Blvd.	1920s	No longer extant
MK3190	House	5200 Wilkinson Blvd.	1920s	Not Eligible
MK3191	House	5204 Wilkinson Blvd.	1940s	Not Eligible
MK3192	House	5300 Wilkinson Blvd.	Unknown	Not Eligible
MK3193	House	5312 Wilkinson Blvd.	Mid 20 th	Not Eligible
MK3195	House	5500 Wilkinson Blvd.	Unknown	Not Eligible
MK3196	House	5820 Wilkinson Blvd.	Unknown	Not Eligible
MK3197	Gas Station	5401 Wilkinson Blvd, Charlotte	c. 1927	Eligible
MK3198	House	5625 Wilkinson Blvd.	1920s	No longer extant
MK3199	Copal Grill	5923 Wilkinson Blvd, Charlotte	1947	No longer extant
MK3200	House	7414 Wilkinson Boulevard,	Unknown	No longer extant
MK3263	Bridge No. 166	SR 1156 over Sugar Creek	Unknown	Unknown
MK3348	House	7533 Tuckaseegee Road, Charlotte	c. 1940	Unknown
MK3725	Building No. 213 (CLT) (Express Catering)	4812 Express Dr, Charlotte	Unknown	Not Eligible
MK3761	W.P.A Douglas Airport Hangar (Current site)	4108 Minuteman Way, Charlotte	1936-37	Eligible

Previously Performed Historical Surveys within the Survey Area

Gatza, Mary Beth

1989 *Historic Structures Survey and Evaluation Report, Charlotte Outer Loop (West Section), R-2248A.* ER# 90-8066. Prepared by Archaeological Research Consultants for Kimley-Horn & Associates.

This report detailed the results of the survey of the proposed routes for a 13-mile long segment of a four (4)-lane limited access highway proposed to circle the City of Charlotte. As a result of this survey, eight (8) properties and one (1) district were identified as eligible for the NRHP. Among these resources were MK1377 and MK1361 from Table 1, above, as well as the Shopton Historic District which was located just outside the southern boundary of the Survey Area.

Warminski, Margaret

1998 *Historic Resources Survey for the Proposed Expansion of the Charlotte-Douglas International Airport, Mecklenburg County, North Carolina*. ER# 98-8723. Prepared by *Environment & Archaeology, LLC* for Landrum & Brown.

This document detailed the results of the historic resources survey for a proposed new north-south runway and ancillary facilities at CLT. A total of 78 properties were identified during this survey. Among those resources were MK2490, MK1373, MK1361, MK1875, MK1873, MK1874, MK1363 and MK2337 from Table 1 above.

Woodward, Sarah A. and S.J. Wyatt

2001 *Industry, Transportation and Education, The New South Development of Charlotte and Mecklenburg County*. Prepared by AIA.

The report detailed the documenting and analyzing of the remaining, intact early 20th-century industrial and school buildings in Charlotte and Mecklenburg County, North Carolina. None of the buildings identified during this survey were located within this report's Survey Area.

Silverman, Richard

2005 *U-2704B, Improvements to US 29-74 (Wilkinson Boulevard) and US 521 (Billy Graham Parkway), Charlotte, Mecklenburg County, NC*. ER# 01-9611. Prepared by the North Carolina Department of Transportation (NC DOT).

This document detailed the NRHP eligibility statements for MK2150 and MK1709 from Table 1 above. Oakden Motel (MK2150) is still standing while McCoy Service Station (MK1709) has been torn down but was once within this report's Survey Area.

2006 *Addendum, Improvements to US 29-74 (Wilkinson Boulevard) and US 521 (Billy Graham Parkway), Charlotte, U-2704B, Mecklenburg County*. ER# 01-9611. Prepared by the NC DOT.

This document detailed the eligibility concurrence for MK2150 and MK1709.

Mattson, Richard L. and F.P. Alexander

2005 *Phase II Architectural Resources Survey Report, West Corridor Rapid Transit Project, Charlotte Area Transit System, Mecklenburg County*. ER# 06-1955. Prepared by Mattson, Alexander and Associates, Inc. for Gannett Fleming, Inc.

This report detailed the results of the architectural resources survey of a 10-mile long corridor from the City of Charlotte west to Interstate 485 near CLT. This survey identified a high number of

historic resources, of which MK1709, MK2150, MK3197, and MK3199 from Table 1 above were recorded.

National Guard Bureau and Air National Guard

- 2013 *Cultural Resources Survey of the Charlotte-Douglas International Airport Air National Guard Base and the Stanly County Airport Air National Guard Station, North Carolina.* ER# 13-1453.

This report detailed the results of the investigations at the 103-acre Charlotte IAP ANGB. None of the 17 buildings or properties investigated were evaluated as eligible to the NRHP. All of these resources are located within this report's Survey Area.

Mattson, Alexander and Associates, Inc.

- 2014 *Final Report, Charlotte Comprehensive Architectural Survey, Phase I Charlotte, North Carolina.* Prepared for the Charlotte Historic District Commission.

A total of 423 properties and 9 historic districts were reinvestigated and 21 new properties were investigated during the course of this survey. None of these resources were within the proposed Survey Area.

- 2015 *Final Report, Charlotte Comprehensive Architectural Survey, Phase II Charlotte, North Carolina.* Prepared for the Charlotte Historic District Commission.

This report documented the continuation of the work performed in 2014.

Alexander, Francis

- 2018 *Historic Structures Survey Report, NC 160, Widen to Multi-Lanes from South Carolina Line to I-485, Mecklenburg County.* Prepared by Mattson, Alexander and Associates, Inc. for the NC DOT. ER# 18-3431.

This report detailed the survey and evaluation of MK1367 and MK2498, both of which are just south of and outside of the proposed Survey Area.

Armstrong, Kirsten and N. Linville

- 2020 *Historic Structure Survey Report, Section 106 Compliance for the Carolinas Aviation Museum Apron Project at the Charlotte-Douglas International Airport, Charlotte, North Carolina.* Prepared by SEARCH for RS&H.

The report detailed the evaluation of MK3761/MK2933, which is present in Table 1 above.

ER# 16-1791 – Architectural Survey Report not submitted. Project possibly dropped due to change in scope of work.



Figure 3	Charlotte Douglas International Airport Historical Resources Summary Project Mecklenburg County, North Carolina	7.5' USGS Topographic Map with Records Check Results Charlotte West and Mountain Island Lake, NC Quadrangles <i>Environment & Archaeology LLC</i>
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SITE DESCRIPTIONS

The historic resources discussed on the following pages were selected through coordination with NCDNCR. Due to CLT's decade(s)-spanning project timeline and the high number (n=599) of historic resources identified during the HRS of the APE, *Environment & Archaeology, LLC*, participated in an April 3, 2020 conference call with the NCDNCR. The conference call objective was to discuss the findings of the HRS, provide guidance on creating a streamlined report, and facilitate an efficient review of resources by NCDNCR (Appendix A). In advance of the conference call, *Environment & Archaeology, LLC* provided the NCDNCR with a tabular summary of initial findings - grouped by decade of construction –with accompanying mapping and photographs. The conference call successfully concluded with NCDNCR identifying those properties that could require more or future evaluations over the life of the airport's improvement projects. The 30 historic resources discussed in this report are the result of the April 3, 2020 conference call (Table 2 and Figure 4).

Table 2. Properties within the APE/Survey Area with opportunity for listing in the NRHP.

SITE #	Property	NRHP RECOMMENDATION
PREVIOUSLY RECORDED PROPERTIES		
MK1364/ MK3993	William Grier House (former MK3659)	NRHP Eligible, Covered under MOA
MK1377	Steele Creek Presbyterian Church	Listed in NRHP
MK1361	John Douglas House	NRHP Eligible, Covered under MOA
MK1875	Spratt-Grier Farm	NRHP Eligible, Covered under MOA
MK2150	Oakden Motel	Previously Recommended, Currently Recommended
MK2189	Old Terminal Building	Recommended as Eligible
MK3761	WPA Douglas Airport Hanger (former MK2399)	Previously Recommended, Currently Recommended
MK1378	Steele Creek Manse	NRHP Eligible, Covered under MOA
MK2841	Byrum Croft House	NRHP Eligible, Covered under MOA
MK3192	Fred Webber House	Recommended as Eligible
MK3197	Two Guys Auto Repair Body Shop	Previously Recommended, Currently Recommended
NEWLY RECORDED PROPERTIES		
MK3770	3100 Yorkmont Road	Recommended as Potentially Eligible in 2031
MK3762	3140 Yorkmont Road	Recommended as Potentially Eligible in 2032
MK3798	FAA Control Tower	Recommended as Potentially Eligible in 2029
MK3816	CLT Box Building	Recommended as Potentially Eligible in 2031
MK3822	Big Spring Methodist Church	Recommended as Not Eligible
MK3823	Officer's Club	Recommended as Eligible
MK3825	Thomas W. Ferebee Building	Recommended as Eligible
MK3836	9311 Markswood Road	Recommended as Not Eligible
MK3853	Cold War Era Fallout Shelter	Recommended as Potentially Eligible
MK3960	Housing Building #4	Recommended as Eligible
MK4127	Ashley/Eaton Circle Development	Recommended as Potentially Eligible
MK3933	Charlotte Speedway Ticket Office/Barn	Recommended as Not Eligible
MK4004	Harvest Center Properties: NCO Club and Morris Field Baptist Chapel	Recommended as Eligible
MK4023	West Mecklenburg High School	Recommended as Not Eligible
MK4047	5455-5457-5459-5461 Wilkinson Boulevard	Recommended as Eligible
MK4115 MK4123	4824 and 4828 Parkway Plaza Boulevard	Recommended as Potentially Eligible in 2032/2033
MK4126	Stonewall Jackson Homes	Recommended as Eligible
MK4407	Steeleberry Drive Neighborhood	Recommended as Potentially Eligible

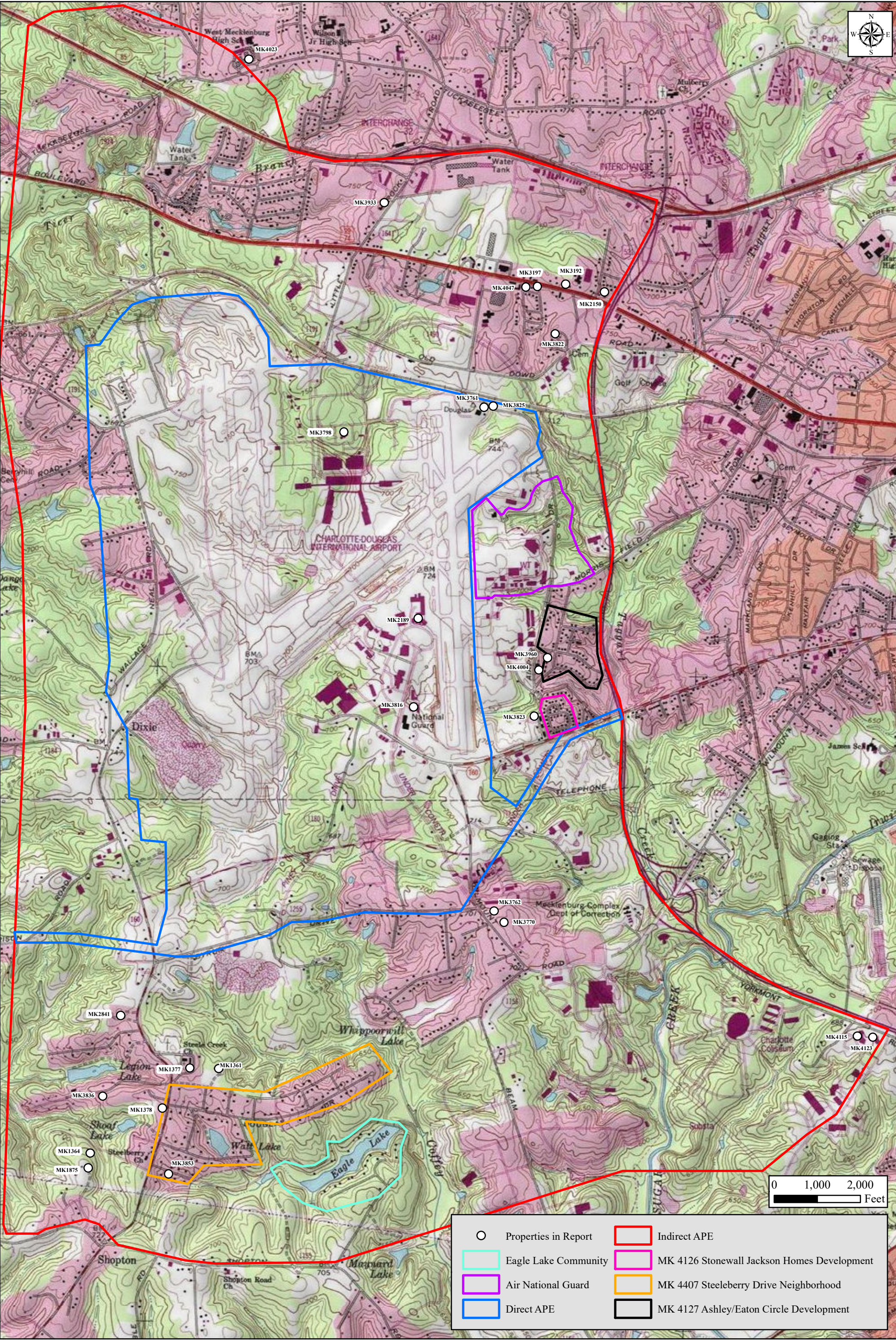


Figure 4	Charlotte Douglas International Airport Historical Resources Summary Project Mecklenburg County, North Carolina	7.5' USGS Topographic Map Charlotte West and Mountain Island Lake North Carolina, Quadrangles <i>Environment & Archaeology</i> LLC
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William Grier House (MK1364/MK3993)

The William Grier House is associated with a prominent planter in Mecklenburg County (Figure 5 and Figure 6). In 1978, the Charlotte-Mecklenburg County Historic Landmarks Commission judged the house, a county landmark, potentially eligible for National Register listing for the following reasons: “First, the structure formed the focal point of an antebellum plantation in Mecklenburg County. Second, the structure is one of the few Federal style plantation houses which survives in Mecklenburg County. Third, individuals of local prominence inhabited the structure.”

Since that time, however, the house has been relocated to a new site and has lost much of its integrity of setting. Important features have been removed as part of the move, most notably the monumental gable-end brick chimneys, diminishing its integrity of design, materials and workmanship. The house also has been abandoned by its owner and is seriously deteriorated. Originally set on a brick foundation, the house has been haphazardly reset on concrete block piers and is suffering from structural failure. The porch is pulling away from the house, its roof is collapsing, windows are broken and paint peeling (Photo Proof Set 1).

NRHP Recommendation: The William Grier House has been previously determined eligible for listing in the NRHP. CLT purchased this property not knowing its condition nor its potential historical nature. This property is covered under the existing MOA between the FAA and NCDCNR.

Spratt-Grier Farm (MK1875)

This one-story frame L-plan residence was previously determined eligible for Register listing by the North Carolina SHPO in 1998 (Figure 5 and Figure 6). It represents a variation on a popular late-19th-century house type, the L- or T-plan cottage. Little altered on the exterior, it retains numerous historic features including a distinctive wrap-around porch with raised-seam metal shed/pyramidal roof, supported by wood posts; wood siding; front door with arched glazed panels; and original 2/2 and 6/6 wood windows. The brick porch piers are replacements of the originals. At the same time, however, the long-vacant house is suffering serious deterioration that threatens its material integrity, including brick chimney collapse, a failing porch with warped and patched metal roofing and collapsing ceiling, and a porch enclosure in poor condition (Photo Proof Set 2).

NRHP Recommendation: The Spratt-Grier Farm has been previously determined eligible for listing in the NRHP. This property is covered under the existing MOA between the FAA and NCDCNR.

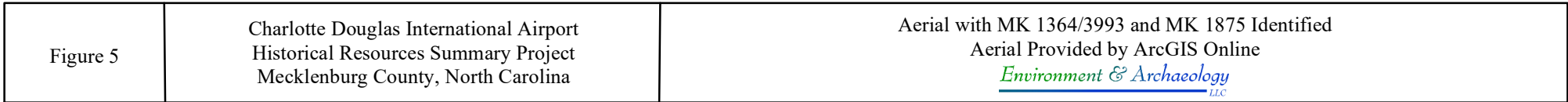




Figure 6

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 1364/3993 and MK 1875 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

MK1364/3993, William Grier House, 8116 (8416) Robbie Circle, V.W., February 2020



MK1364/3993_WilliamGrierHouse_02 20_V.W._01

Front View



MK1364/3993_WilliamGrierHouse_02 20_V.W._02

Back View



MK1364/3993_WilliamGrierHouse_02 20_V.W._03

Left View



MK1364/3993_WilliamGrierHouse_02 20_V.W._04

Right View

MK1875, Spratt-Grier Farm, 8006 Robbie Circle, V.W., March 2020



MK1875_SprattGrierFarm_03 20_ V.W._01

Front View



MK1875_SprattGrierFarm_03 20_ V.W._02

Back View



MK1875_SprattGrierFarm_03 20_ V.W._03

Right View



MK1875_SprattGrierFarm_03 20_ V.W._04

Left View

Steele Creek Presbyterian Church (MK1377)

Steele Creek Presbyterian Church was founded in 1760 (Figure 7 and Figure 8). It was the first Presbyterian congregation in the vicinity and became the “mother church” of five other congregations. In 1991, the church and cemetery were listed in the NRHP under Criterion C in the area of architecture as a significant example of Gothic Revival ecclesiastic architecture and under Criterion A for their association with Scots-Irish settlement in Mecklenburg County (Photo Proof Set 3). CLT owns this structure.

John Douglas House (MK 1361)

Built c. 1867, the John Douglas House is a significant example of both the Greek Revival style and the center-passage, double-pile (“Georgian Cottage”) plan type: two rooms deep with a formal center hall. Largely intact inside and out, the main house retains a remarkable degree of integrity, with its original plan still evident. The property includes two historic outbuildings and no new structures have been added, so the site retains its sense of place (Figure 7 and Figure 8).

However, the rear portion of the house and the outbuildings are in serious deterioration which, if left unchecked, will undermine the site’s integrity and eventually lead to demolition by neglect. The brick foundation is starting to crumble, the back-porch roof has collapsed, wood is rotting and paint peeling. In addition, the log cabin and the barn are in heavy decline with missing boards and roofs and haphazard repairs (Photo Proof Set 4).

Behind the house is a barn that, judging from local aerial maps, dates from at least 1938. It has a brick foundation, vertical boards with fading paint, and a gabled metal roof. Some windows and doors have been removed, leaving gaps.

Also on the property is a log cabin. The main body of the house is built of hewn logs with half-dovetailed joints that are chinked with concrete, with some older brick chinking visible underneath. The building rests on stacked stone supports. A metal-roofed shed addition has been appended to the southeast side of the cabin.

NRHP Recommendation: The John Douglas House has been previously determined eligible for listing in the NRHP. This property is covered under the existing MOA between the FAA and NCDCNR but this property is not owned by CLT.



Figure 7

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 1361 and MK 1377 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

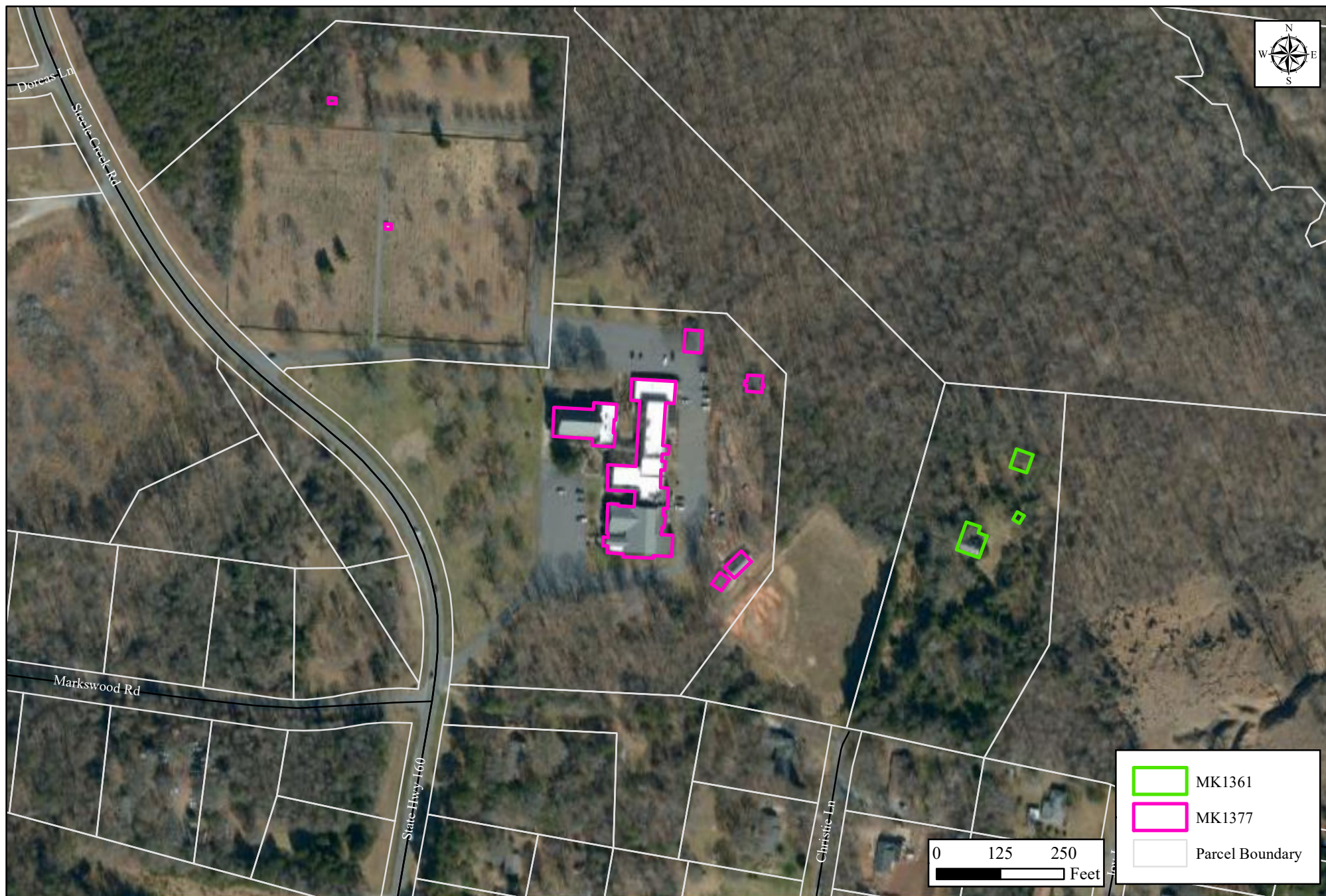


Figure 8

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 1361 and MK 1377 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

MK1377, Steele Creek Presbyterian Church, 7407 Steele Creek Road, V.W., February 2020



MK1377_SteeleCreekPresbyterianChurch_02 20_V.W._01

Front View



MK1377_SteeleCreekPresbyterianChurch_02 20_V.W._02

Back View



MK1377_SteeleCreekPresbyterianChurch_02 20_V.W._03

Outbuilding 1



MK1377_SteeleCreekPresbyterianChurch_02 20_V.W._04

Outbuilding 2

MK1361, John Douglas House, 7601 Christie Lane, V.W., February 2020



MK1361_JohnDouglasHouse_02 20_V.W._01

Front View



MK1361_JohnDouglasHouse_02 20_V.W._02

Back View



MK1361_JohnDouglasHouse_02 20_V.W._03

Side View



MK1361_JohnDouglasHouse_02 20_V.W._04

Outbuilding

Oakden Motel (MK2150)

The Oakden Motel is a well-preserved example of a postwar suburban motel, built in 1952 (Figure 9 and Figure 10). The complex consists of two parallel, one-story buildings with engaged porches with metal posts. The two buildings face each other across the parking lot. There is brick skirting below the level of the window sills and on the gable ends. The site includes a neon street-side sign with a boomerang shape pointed towards the motel. A small addition has been made to Building 1 to allow for more rooms. The property includes one outbuilding: a frame storage shed clad in vertical wood boards (Photo Proof Set 5).

The motel was recommended eligible by Mattson, Alexander & Associates via draft report for the Charlotte Area Transit System (CATS) West Corridor Rapid Transit Project, 2005. The North Carolina State Historic Preservation Office concurred with this recommendation.

“For purposes of compliance with Section 106 of the National Historic Preservation Act (NHPA), the Oakden Motel is recommended eligible for the National Register of Historic Places under Criterion A for commerce and Criterion C for architecture. Due to intensive commercial development along major highways leading into Charlotte, only a few 1940s-1950s motels survive intact. An informal survey conducted by Mattson, Alexander & Associates of Charlotte revealed just three comparable intact examples. The Oakden Motel meets the registration requirements set forth for motels in Charlotte’s ‘Post-War Architectural Survey: National Register of Historic Places Multiple Property Documentation Form, 2001’ (Woodard and Wyatt, 2001). The Oakden Motel is not eligible under any other criteria.”—U27048, NCDOT [North Carolina Department of Transportation], Evaluation for National Register Eligibility, Property #5—Oakden Motel, Charlotte, NC

NRHP Recommendation: The Oakden Motel retains the historic integrity detailed in the Mattson, Alexander & Associates report. No significant alterations or deterioration have occurred at the property. As such, the property retains its recommendation as eligible for listing in the NRHP.

Fred Webber House (MK3192)

Built in 1931, this house is a well-preserved example of the dormer-front bungalow house type popular in the early 20th century (Figure 9 and Figure 10). The original owner and inhabitant for several decades was restaurateur Fred Webber. Webber founded the S&W Cafeteria in Charlotte, the first commercial eatery in the area, which quickly became franchised throughout the South. Eventually Webber was bought out by his partner.

The Webber House is built of brick with side-gabled roof and gabled frame dormers front and rear. It features an engaged porch, heavy tapered piers and exposed brackets. It is in fair condition. At the rear of the lot is a rundown frame shed with gabled roof and vertical and horizontal wood siding. The house has seen little alteration and retains a high degree of integrity under all its aspects (Photo Proof Set 6).

NRHP Recommendation: The Fred Webber House is recommended as eligible under Criterion B as the residence of an entrepreneur who established a successful regional restaurant chain in the early-

to-mid 20th century, a time period when fast food franchise restaurants such as A&W Root Beer (1919), White Castle (1921) and Dairy Queen (1940) were becoming popular across the country.

Two Guys Auto Repair Body Shop (MK3197)

Presently used as an auto repair facility, this former filling station was built in 1927 to serve motorists on the newly constructed Wilkinson Boulevard (Figure 9 and Figure 10).

“When it opened about 1926, Wilkinson Boulevard was the first four-lane paved highway in all of North Carolina. A 40-foot swath of concrete with a 10-foot dirt shoulder on each side, the roadway was landscaped with grass, vines and shrubbery from Charlotte all the way to the Gaston County line. ‘The South’s finest highway,’ the Observer called it, ‘a showplace of the South.’ “Wilkinson Boulevard remained Charlotte’s western ‘front door’ til the 1960s, when Interstate 85 supplanted it.”-- Tom Hanchett, *Charlotte Observer*, November 2000 (used with permission in “History South: Wilkinson Boulevard: When Wilkinson Made History,” <https://www.historysouth.org/wilkinsonblvd/>).

The gas station’s gabled roof with boxed pediment; tall, multi-light arched windows; and nine-light, double-leaf doors, likely made of wood, echo the romantic revival architecture of suburban America in the inter-war period, as did many early roadside establishments (Photo Proof Set 7).

“...Gas Station is recommended as eligible under Criterion A for commerce and under Criterion C for architecture. The resource is a well-preserved and rare surviving example of the domestic-style gas station design constructed throughout Mecklenburg County and nationwide in the early twentieth century. The gas station stands among the first roadside establishments along Wilkinson Boulevard, the state’s first four lane highway.”—Correspondence from the North Carolina Department of Cultural Resources to Chris M. Lloyd, Parsons Brinckerhoff Quade & Douglas, Inc., March 9, 2006.

NRHP Recommendation: The building has seen some modifications to the garage bay façade and some modest rear additions not visible from Wilkinson Boulevard, but retains sufficient integrity to meet Criteria A (highway-oriented suburban expansion) and C (innovations in filling station design).

Big Spring Methodist Church (MK3822)

The Big Spring Methodist congregation was organized in 1866 (Figure 9 and Figure 10). A sign on the church says it was rebuilt in 1907 and in the 1950s, and its Colonial Revival design does suggest a mid-20th-century construction date. The building’s predominant feature is a projecting, gabled, frame porch carried by four square columns and partly clad in artificial siding. The porch supports a louvered, wood-frame, octagonal tower with a cross-tipped polygonal spire. At the rear is a two-story, side-gabled wing with entrances at either side. A small cemetery near the church contains fewer than 25 burials. The church building is in fair condition and in need of maintenance (Photo Proof Set 8).

NRHP Recommendation: The Big Spring Methodist church building has been little altered, with only minor exterior modifications. It represents a common ecclesiastical building type widely used by Protestant denominations in the mid-20th century and lacks architectural distinction. Therefore, it is not recommended for Register listing under Criterion C.

5455-5457-5459-5461 Wilkinson Boulevard (MK4047)

This is a distinctive Modernist commercial building, one story in height, consisting of four separate storefronts, each with a different address (Figure 9 and Figure 10). Each unit has a double swinging glass entry. Decorative separator walls divide each business. They are built of molded concrete and rest on brick foundations. 5455 and 5457 both have rooflines that jut out to a point to shelter the entrances. 5459 and 5461, by contrast, share a single roof with one point, suggesting they were intended to be rented as one unit. One of the storefronts currently houses an Asian food market; the remaining three are vacant. The building has a “walkout” basement with a folding garage door, several stock metal doors and metal stairs that lead to the first floor. At the rear of the building is a fifth unit, with a separate address of 2305 Eatonton Street, housing Prestige Auto. This storefront has a single glass door and an extended flat roof that shelters the entry. Another address at the rear reads “2311,” but gives no indication of what, if any, kind of business occupies the space (Photo Proof Set 9).

NRHP Recommendation: This complex represents a property type that saw limited popularity in suburban shopping districts during the 1950s and 1960s. It is remarkably well preserved. The storefronts with two-pane glass panels, brick bulkheads, double doors and single-pane transoms appear to be original, as do the decorative latticework dividers and low-gabled roofs. The front parking lot also is typical of this property type. The building is recommended eligible for National Register listing under Criterion C in the area of architecture for design innovation in the shopping strips of postwar suburban communities. The complex is not known to be associated with any persons or businesses of significance, therefore it is not recommended for Criterion A or B.



Figure 9

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 2150, MK 3192, MK 3197, MK 3822 and MK 4047 Identified
Aerial Provided by ArcGIS Online

Environment & Archaeology
LLC

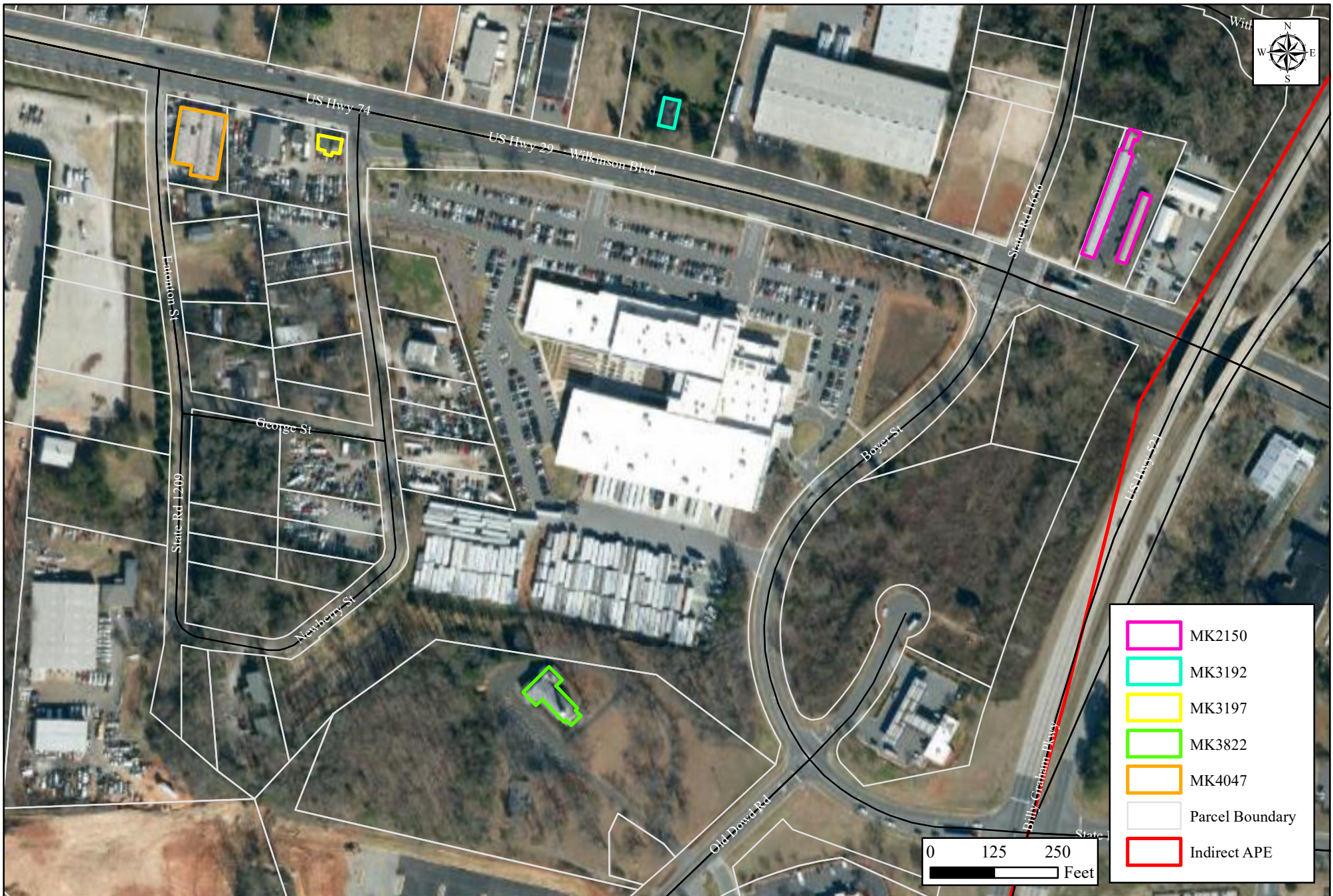


Figure 10

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 2150, MK 3192, MK 3197, MK 3822 and MK 4047 Identified
Aerial Provided by ArcGIS Online

Environment & Archaeology
LLC

MK2150, Oakden Motel, 5104 Wilkinson Boulevard, V.W., February 2020



MK2150_OakdenMotel _02 20_V.W._01

Left View



MK2150_OakdenMotel _02 20_V.W._02

Back View



MK2150_OakdenMotel _02 20_V.W._03

Front View



MK2150_OakdenMotel _02 20_V.W._04

Right View

MK3192, Fred Webber House, 5300 Wilkinson Boulevard, V.W., February 2020



MK3192_FredWebberHouse_02 20_V.W._01

Front View



MK3192_FredWebberHouse_02 20_V.W._02

Back View



MK3192_FredWebberHouse_02 20_V.W._03

Right View



MK3192_FredWebberHouse_02 20_V.W._04

Left View

MK3197, Two Guys Auto Repair Body Shop, 5401 Wilkinson Boulevard, V.W., February 2020



MK3197_TwoGuysAutoRepairBodyShop_02 20_V.W._01

Front View



MK3197_TwoGuysAutoRepairBodyShop_02 20_V.W._02

Back View



MK3197_TwoGuysAutoRepairBodyShop_02 20_V.W._03

Right View



MK3197_TwoGuysAutoRepairBodyShop_02 20_V.W._04

Left View

MK3822, Big Spring Methodist Church, 5308 Old Dowd Road, V.W., February 2020



MK3822_BigSpringMethodistChurch_02 20_V.W._01

Front View



MK3822_BigSpringMethodistChurch_02 20_V.W._02

Back View



MK3822_BigSpringMethodistChurch_02 20_V.W._03

Left View



MK3822_BigSpringMethodistChurch_02 20_V.W._04

Right View

MK4047, Renters Building, 5455, 5457, 5459, 5461 Wilkinson Boulevard, V.W., February 2020



MK4047_RentersBuilding_5455,5457,5459,5461WilkinsonBoulevard_02 20_V.W._01

Front View



MK4047_RentersBuilding_5455,5457,5459,5461WilkinsonBoulevard_02 20_V.W._02

Back View



MK4047_RentersBuilding_5455,5457,5459,5461WilkinsonBoulevard_02 20_V.W._03

Right View



MK4047_RentersBuilding_5455,5457,5459,5461WilkinsonBoulevard_02 20_V.W._04

Left View

Old Terminal Building (MK2189)

The original terminal for the Charlotte Douglas Airport was built c. 1951 and opened in 1954 (Figure 11 and Figure 12). It was designed by local architect Walter Hook. The two-story, steel-frame building has a flat roof and brick exterior. Its footprint is a wide, shallow, U-shape. The front façade is blind apart from the glass central section. A flat canopy extends over the sidewalks. The interior has a full-height lobby with a double staircase of terrazzo. Railings are aluminum. The second-floor balcony overlooks the lower waiting room. There is a fallout shelter in the building (Photo Proof Set 10).

After a new terminal was completed, the building was used as a cargo terminal. The building has seen little alteration and has a high degree of integrity under all its aspects.

NRHP Recommendation: The Old Terminal Building is recommended eligible for NRHP listing under Criterion A for its association with the development of air travel in Charlotte in the mid-20th century. More research is needed to ascertain its eligibility under Criterion C as an example of a postwar Modernist public building in Charlotte/Mecklenburg.



Figure 11

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 2189 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC



Figure 12

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 2189 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

MK2189, Old Terminal Building, 4700 Yorkmont Road, V.W., February 2020



MK2189_OldTerminalBuilding_02 20_V.W._01

Front View



MK2189_OldTerminalBuilding_02 20_V.W._02

Right View



MK2189_OldTerminalBuilding_02 20_V.W._03

Back View



MK2189_OldTerminalBuilding_02 20_V.W._04

Left View

WPA Douglas Airport Hangar (MK2399/MK3761)

This hangar is based on an early, pre-World War II aviation design (Figure 13 and Figure 14). It was built from 1936 to 1937 by the Works Progress Administration (WPA), a federal work program that preserved Charlotteans' skills and self-respect during a period of massive unemployment. The airport was the W.P.A.'s largest project, in allotment of funds, at the time in North Carolina. Of the original five structures built by the W.P.A. at the airport, only the hangar is extant. The establishment of the airport contributed greatly to physical and economic development of the city, ever expanding to supply comprehensive and convenient air transport to Charlotte.

"From 1935 to 1943, the W.P.A. provided approximately eight million jobs at a cost of more than eleven billion dollars and funded the construction of hundreds of thousands of public buildings and facilities" ("W.P.A., Douglas Airport Hangar," <http://landmarkscommission.org/2016/10/04/w-p-a-douglas-airport-hangar/>)

"The W.P.A. / Douglas Airport Hangar is a one story, one hundred feet wide by one hundred feet deep, by thirty feet tall, metal structure. It is typical of aviation hangars built by the Works Progress Administration (later known as the Works Projects Administration), which utilized stock plans and worked on 11 airport projects in North Carolina before 1940.... The exterior structure has a gable roof with rounded cornices composed of prefabricated sheet metal with a pressed corrugated pattern. The exterior roof is covered with weatherproofing tar and painted silver." (Ibid.)

The hangar is presently used as an aviation museum. It was moved slightly from its original location 200 feet to the southwest, next to the current taxiway, to make way for previous construction; it remains in an aviation setting. Its original exterior appearance also has been preserved (Photo Proof Set 11).

NRHP Recommendation: The WPA Douglas Airport Hangar retains integrity under all its aspects. It is recommended eligible for National Register listing under Criterion A for its association with New Deal work programs in North Carolina during the Great Depression.

Thomas W. Ferebee Building, Army Air Corps Morris Field Barracks (MK3825)

This barracks was formerly associated with the Army Air Corps, the aerial warfare service component of the United States Army between 1926 and 1941 (Figure 13 and Figure 14). It was located on Morris Field, the precursor of the Charlotte Douglas Airport. It is presently located on the Charlotte Douglas Airport next to the WPA-built hangar used as an aviation museum.

The barracks was moved to the site from the southeastern portion of the airport, where the pre-1945 barracks were located, and set on a vented concrete block foundation; it remains in an aviation setting near the WPA hangar (air museum). The barracks retains original shiplap siding, paired, 6/6 sash windows, half-glazed doors, and a side-gabled roof. It is currently vacant and undergoing renovation. The airport plans to use it as part of its air museum along with the hangar. The barracks has been renamed for the bombardier who dropped the atomic bomb on Hiroshima (Photo Proof Set 12).

NRHP Recommendation: The barracks retains a high degree of integrity under all its aspects. Although it has been moved, it remains on airport property. It is recommended eligible for Register listing under Criterion A, in the area of aviation heritage, for its association with the US Army Air Corps and with Morris Field.

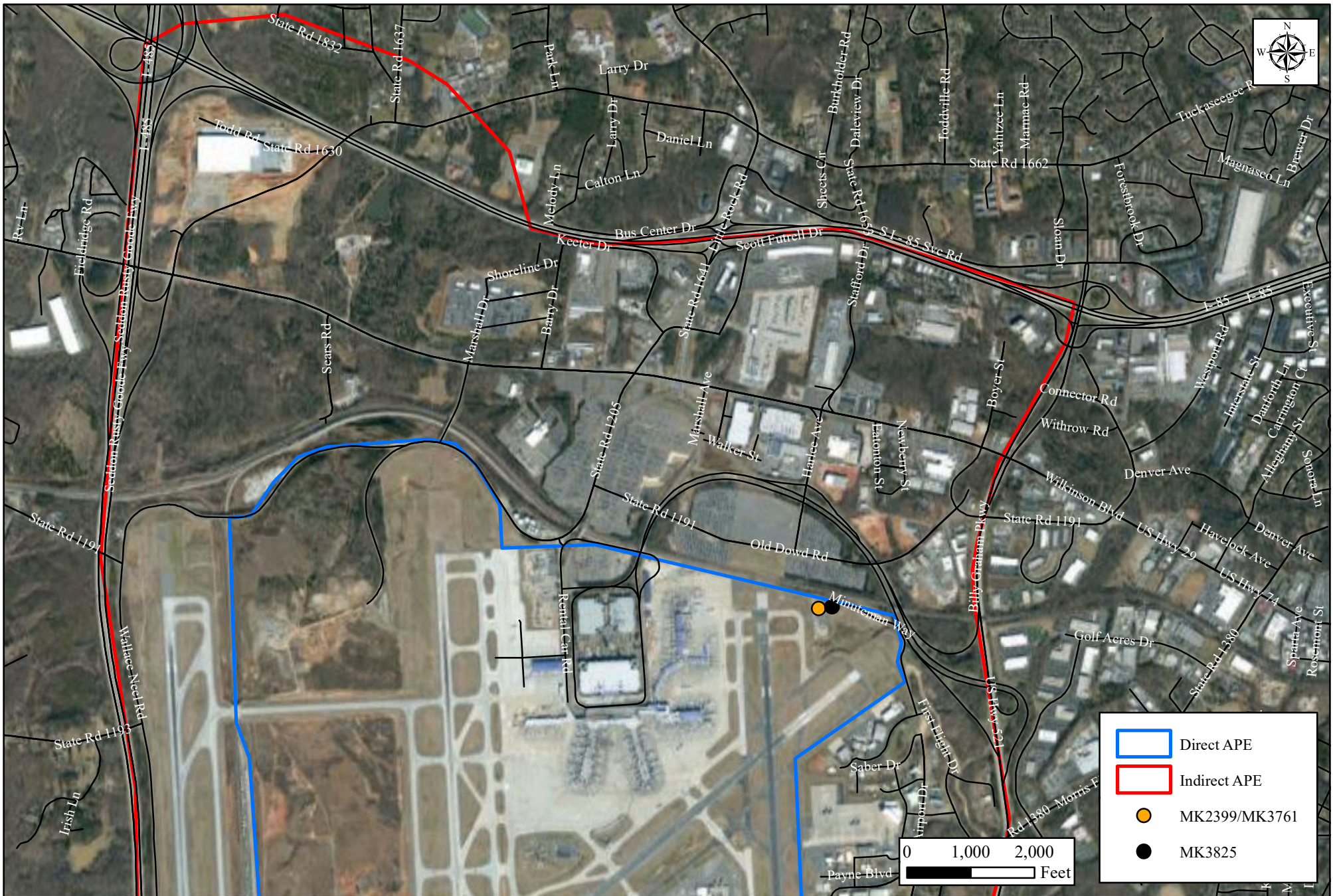


Figure 13	Charlotte Douglas International Airport Historical Resources Summary Project Mecklenburg County, North Carolina	Aerial with MK 2399/MK 3761 and MK 3825 Identified Aerial Provided by ArcGIS Online <i>Environment & Archaeology</i> LLC
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Figure 14

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 2399/MK 3761 and MK 3825 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC



MK3761/2399_WPAHangar,CarolinasAviationMuseum_02 20_V.W._01

Front View



MK3761/2399_WPAHangar,CarolinasAviationMuseum_02 20_V.W._02

Back View



MK3761/2399_WPAHangar,CarolinasAviationMuseum_02 20_V.W._03

Left View



MK3761/2399_WPAHangar,CarolinasAviationMuseum_02 20_V.W._04

Right View



MK3825_ThomasW.FerebeeBuilding,ArmyAirCorpsMorrisFieldBarrac
ks_02 20_V.W._01

Front View



MK3825_ThomasW.FerebeeBuilding,ArmyAirCorpsMorrisFieldBarrac
ks_02 20_V.W._02

Back View



MK3825_ThomasW.FerebeeBuilding,ArmyAirCorpsMorrisFieldBarrac
ks_02 20_V.W._03

Left View



MK3825_ThomasW.FerebeeBuilding,ArmyAirCorpsMorrisFieldBarrac
ks_02 20_V.W._04

Right View

Steele Creek Manse (MK1378)

This property served as the parsonage for the locally prominent Steele Creek Presbyterian Church, which was very active in the community. The main house has seen moderate alteration in the form of residing with vinyl and window replacement, but its form, proportions and overall design remain unchanged. The gabled front porch, a prominent feature, is still in place, along with the pyramidal slate roof and hipped dormer (Figure 15 and Figure 16).

The large barn that once stood on the property has been razed, diminishing our understanding of how the property was used during its period of significance. Two smaller structures remain: a one-story frame shed at the end of the driveway and a concrete block well house at the rear property line.

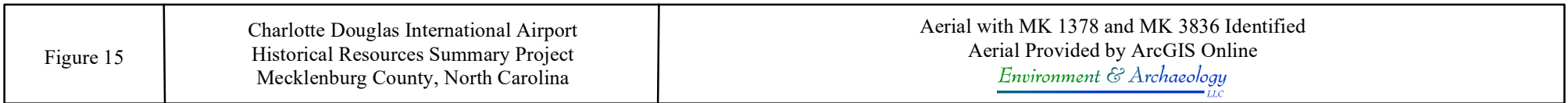
Behind the main house are two sites whose original use has not been determined: a one-story frame structure of simple design that may have been used as a tenant house in the past and a cemetery with arched stone slab bearing the legend “MK CM.” Other headstones are said to have been removed. A neighbor stated that the frame structure reportedly was used as a school for African Americans, but this has not been verified. Little information was readily available concerning the construction, development, period of use, and involvement by local citizens with these two properties. As such, it is difficult to determine their association with the Steele Creek Manse (Photo Proof Set 13).

NRHP Recommendation: The Steele Creek Manse has been previously determined eligible for listing in the NRHP. This property is covered under the existing MOA between the FAA and NCD CNR. The associated cemetery and school warrant additional research to determine if they were associated with the Manse or should be considered a separate property. It is unclear if these two features should be considered contributing structures or a separate property. These two features should be considered potentially eligible for listing as individual properties or as contributing structures to the Steel Creek Manse.

9311 Markswood Road (MK3836)

This house is a typical example of a 20th-century log kit house with matching garage (Figure 15 and Figure 16). Built in 1982, it stands two stories high and three bays wide with an almost-symmetrical façade, gabled roof and exterior end chimney. It is in good condition and appears to have seen little alteration, retaining a high degree of material integrity. However, the house represents a common property type of the late 20th century and lacks individual distinction (Photo Proof Set 14). For this reason, it is not recommended for National Register listing under Criterion C.

NRHP Recommendation: 9311 Markswood Road marks a common property type that lacks individual distinction. It is not recommended as eligible for the NRHP.



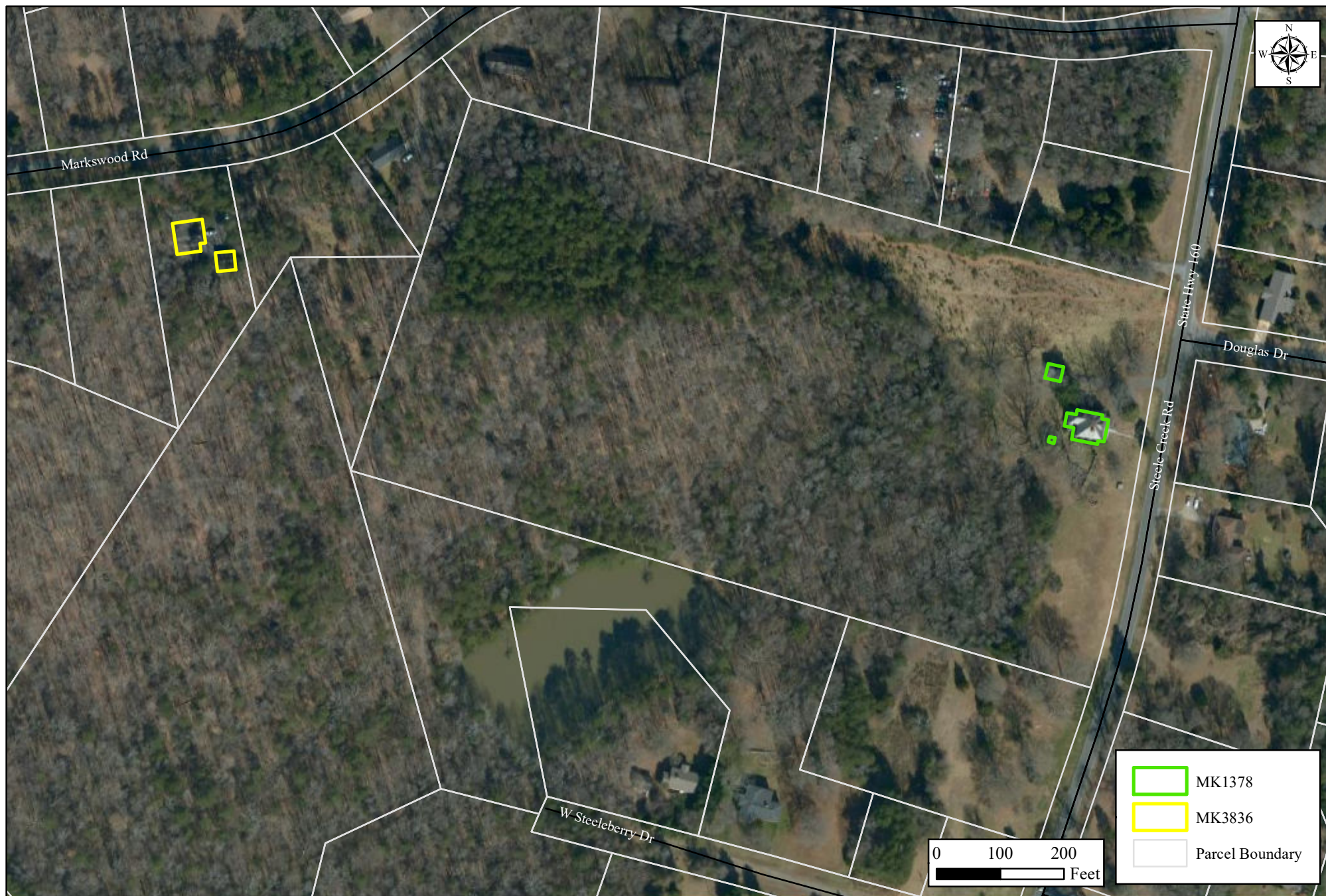


Figure 16

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 1378 and MK 3836 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

MK1378, Steele Creek Manse, 7500 Steele Creek Road, V.W., February 2020



MK1378_SteeleCreekManse_02 20_V.W._01

Front View



MK1378_SteeleCreekManse_02 20_V.W._02

Back View



MK1378_SteeleCreekManse_02 20_V.W._03

Left View



MK1378_SteeleCreekManse_02 20_V.W._04

Right View



MK1378_SteeleCreekManse_02 20_V.W._05

Outbuilding Side View



MK1378_SteeleCreekManse_02 20_V.W._06

Outbuilding Back View

MK3836, House, 9311 Markswood Road, V.W., February 2020



MK3836_House_9311MarkswoodRoad_02 20_V.W._01

Front View



MK3836_House_9311MarkswoodRoad_02 20_V.W._02

Back View



MK3836_House_9311MarkswoodRoad_02 20_V.W._03

Left View



MK3836_House_9311MarkswoodRoad_02 20_V.W._04

Right View

Byrum Croft House (MK2841)

The main house on this property is a substantial dormer-front frame bungalow with a side-gabled roof that changes pitch at front and rear (Figure 17 and Figure 18). The symmetrical main façade contains a centered half-glazed door flanked by paired 8/1 windows with storm sash. A full-width porch extension, apparently an addition, is carried by delicate iron posts with stout, stuccoed piers and archways at either end, and a screened porch. Projecting from the rear roof is a gabled dormer sheathed in asbestos siding. A one-story rear projection, possibly an addition, is covered in wood siding and contains a large, sliding vinyl window.

Behind the house are three small outbuildings: a small brick structure with corrugated metal roof, a ramshackle frame shed, and an apparently owner-built stacked stone barbecue pit that appears to be leaning (Photo Proof Set 15).

Also on the property are two larger structures. One is a one-story, wood-frame barn in rundown condition that, judging from a faded sign over the entrance, may have been used in the owners' auto repair business. The other is a two-story secondary residence built of concrete block. The large openings at the first-story façade suggest it may have been used as a garage or for storage.

NRHP Recommendation: The Byrum Croft House has been previously determined eligible for listing in the NRHP. This property is covered under the existing MOA between the FAA and NCDCLR but is not owned by CLT.

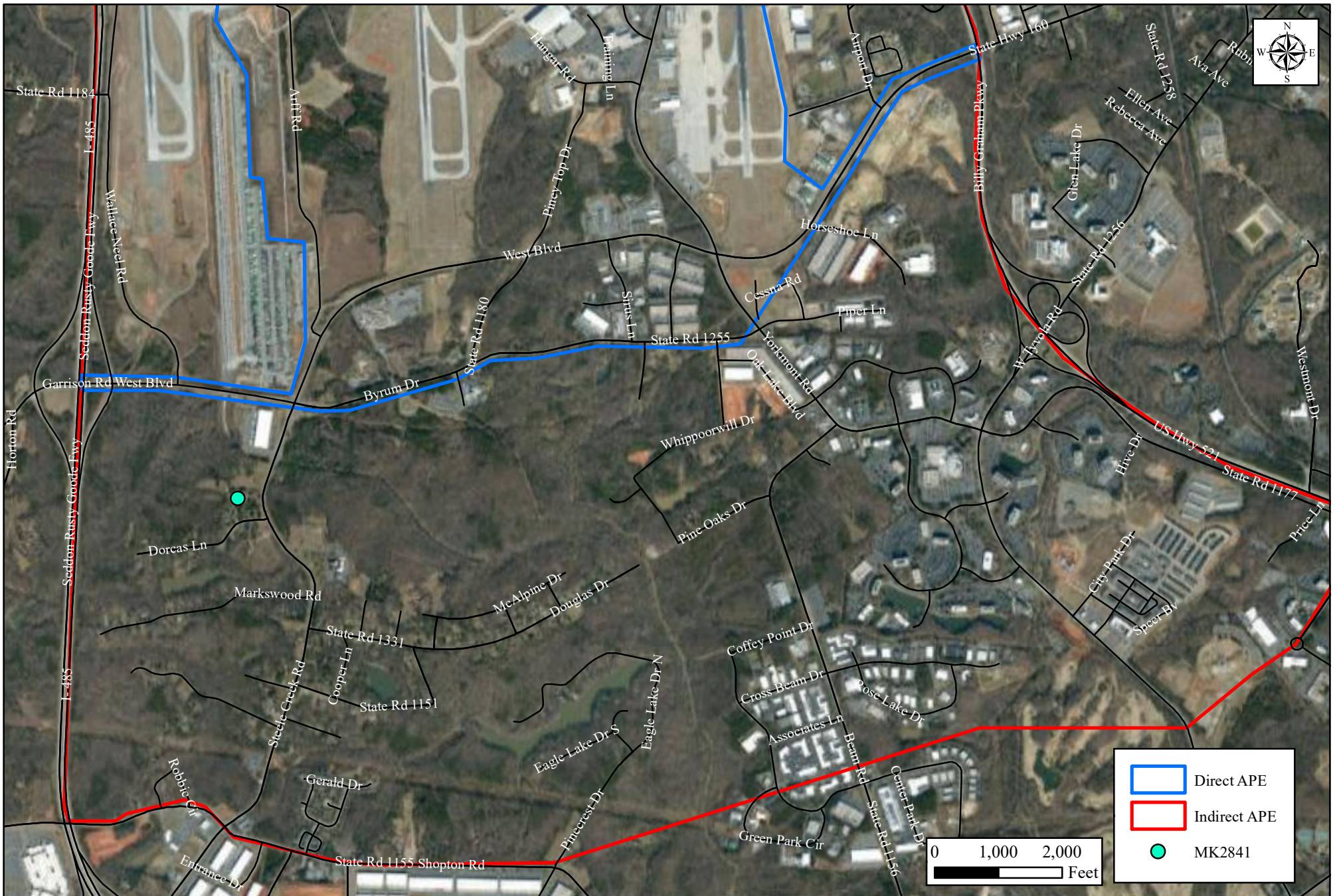


Figure 17

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 2841 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

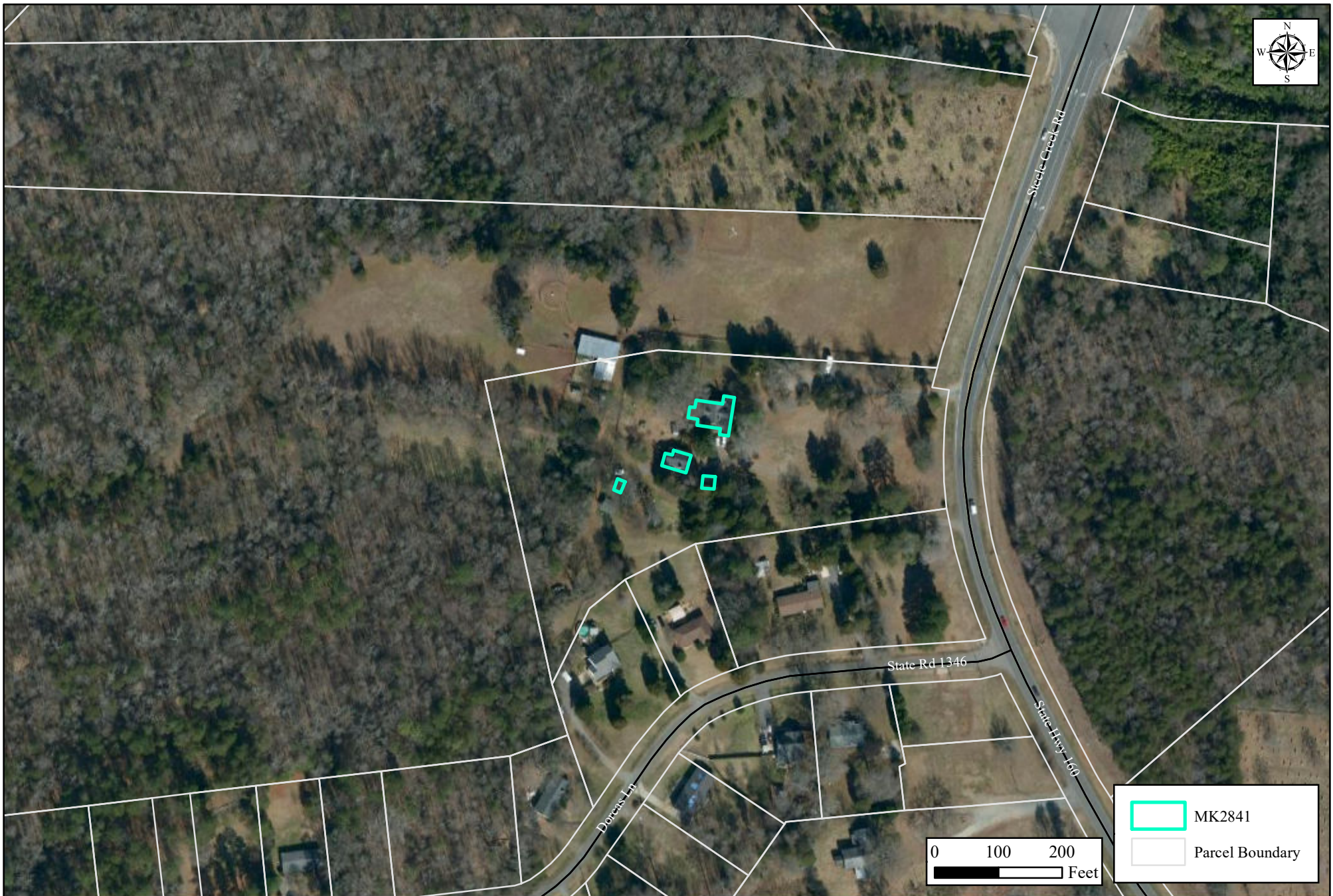


Figure 18

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 2841 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

MK2841, Byrum- Croft House, 7100 Steele Creek Road, V.W., February 2020



MK2841_ByrumCroftHouse_02 20_V.W._01

Front View



MK2841_ByrumCroftHouse_02 20_V.W._02

Back View



MK2841_ByrumCroftHouse_02 20_V.W._03

Left View



MK2841_ByrumCroftHouse_02 20_V.W._04

Outbuilding

3100 Yorkmont Road (MK3770)

This commercial/industrial building is used for freight distribution. It is a low, one-story, steel-frame, Modernist design located in an industrial park (Figure 19 and Figure 20). The building is irregular in plan: The front portion, which likely contains office space, extends outward, and the low-angled sloping roofline merges into the standing-seam metal loading docks. The front façade is clad in textured or seamed weatherboard that resembles concrete, suggesting Brutalist influence. The property includes a large, paved parking lot (Photo Proof Set 16).

NRHP Recommendation: The commercial building at 2100 Yorkmont Road does not meet the age criterion for listing in the NRHP. Once it reaches 50 years of age in 2031, the building may be eligible for Register listing under Criterion C in the area of architecture as an example of architectural distinction applied to an ordinary industrial structure, in this case a truck terminal, provided it is not extensively altered in the meantime. It may also be eligible under Criterion A, in the area of commerce as an expression of the importance of trucking to Charlotte's economy. The building's singular design, mixed materials and high degree of integrity lend it a distinction rarely seen among commercial or industrial structures. Future evaluation is recommended.

3140 Yorkmont Road (MK3762)

3140 Yorkmont Road is an example of minimal Post-Modern influence applied to an industrial structure, in this case a truck terminal (Figure 19 and Figure 20). The building has a scored wall treatment with contrasting banding and geometrical medallions, and unusual freestanding gabled parapets. This decorative treatment sets it apart from contemporaneous truck terminals and suburban commercial/industrial facilities in the area, which are typically utilitarian structures (Photo Proof Set 17).

NRHP Recommendation: The commercial building at 3140 Yorkmont Road does not meet the age criterion for listing in the NRHP, however, it represents a good example of minimal Post-Modern influence in an industrial structure. For this reason, in 2032 it may be eligible for NRHP listing under Criterion C in the area of architecture, provided it is not extensively altered in the meantime. Future evaluation is recommended.

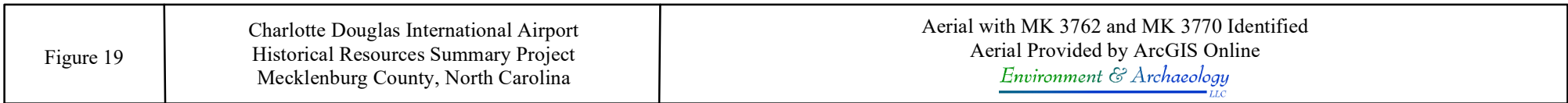




Figure 20

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 3762 and MK 3770 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

MK3770, 3100 Yorkmont Road, V.W., February 2020



MK3770_CommercialBuilding_3100YorkmontRoad_02 20_V.W._01

Front View



MK3770_CommercialBuilding_3100YorkmontRoad_02 20_V.W._02

Side View



MK3770_CommercialBuilding_3100YorkmontRoad_02 20_V.W._03

Left View



MK3770_CommercialBuilding_3100YorkmontRoad_02 20_V.W._04

Right View

MK3762, 3140 Yorkmont Road, V.W., February 2020



MK3762_CommericalBuilding_3140YorkmontRoad_02 20_V.W._01

Front View



MK3762_CommericalBuilding_3140YorkmontRoad_02 20_V.W._02

Back View



MK3762_CommericalBuilding_3140YorkmontRoad_02 20_V.W._03

Right View



MK3762_CommericalBuilding_3140YorkmontRoad_02 20_V.W._04

Left View

FAA Control Tower (MK3798)

This Brutalist/Modernist control tower was built for the Charlotte Douglas Airport in 1979. The monumental structure stands 158 feet tall (three-plus stories) and 39 feet in diameter. It is built of concrete with a steel frame and is supported by three concrete columns, with an open shaft in the center. The top of the structure serves as the control tower, with single-pane windows. A standing seam metal roof covers the structure (Figure 21 and Figure 22).

At the foot of the tower is a one-story, flat-roofed, concrete building that houses office space and equipment, measuring approximately 194 feet by 128 feet. A paved lot with angle parking surrounds the tower, and there are several parking garages nearby (Photo Proof Set 18).

NRHP Recommendation: The control tower is an excellent example of Brutalist / Modernist architecture and is a high-profile, high-visibility local landmark. The tower played an important role in the development of air travel in Charlotte by enabling a modern communications system for the airport. It is in good condition, is virtually unaltered and retains a high degree of integrity under all its aspects.

Because the tower is only 41 years old, however, its significance in the context of Modernism and the development of aviation in Mecklenburg County is best understood by comparing it to other Modernist structures and buildings in the region. Future evaluation of this structure is recommended in 2029 when it reaches 50 years of age.

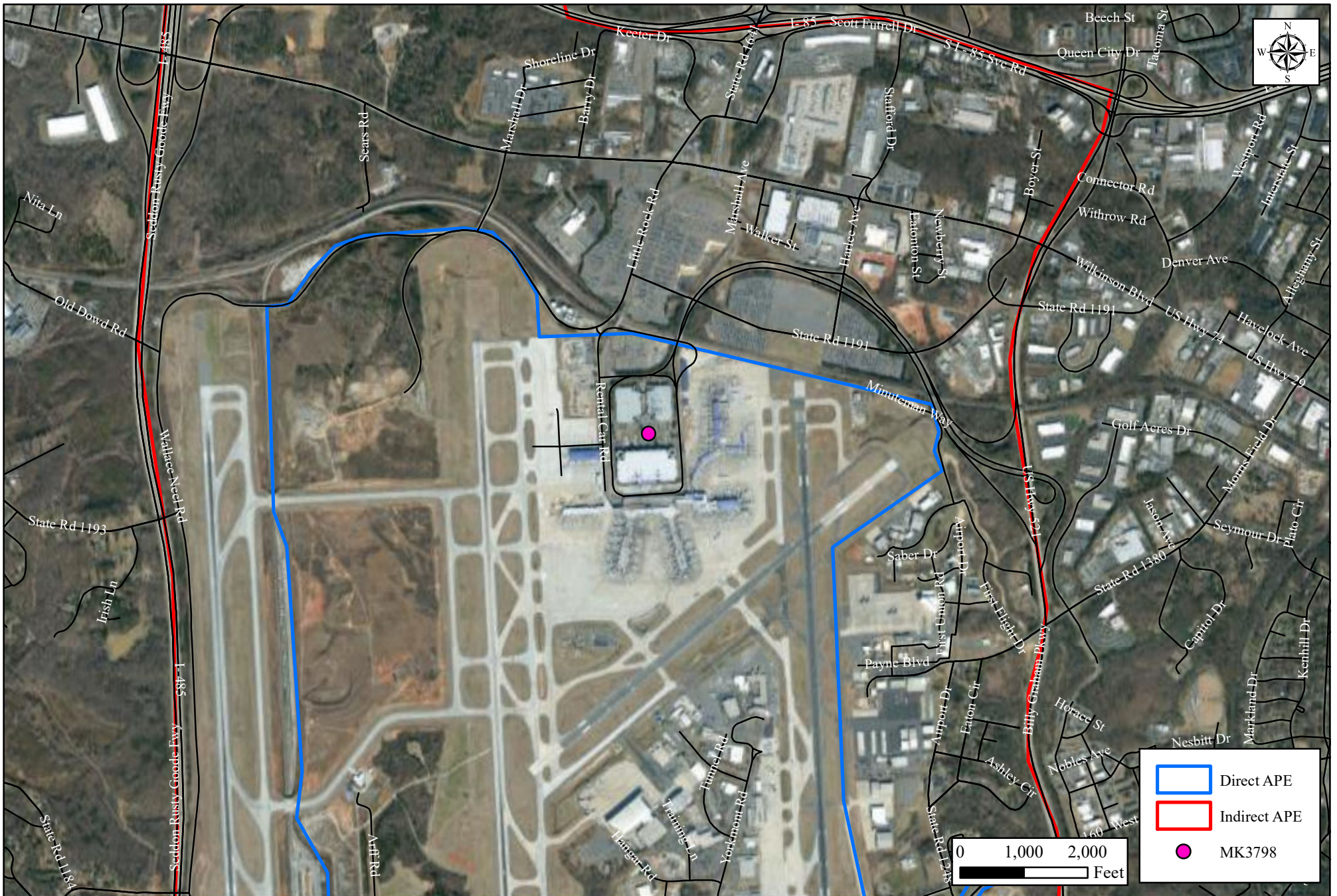


Figure 21

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 3798 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

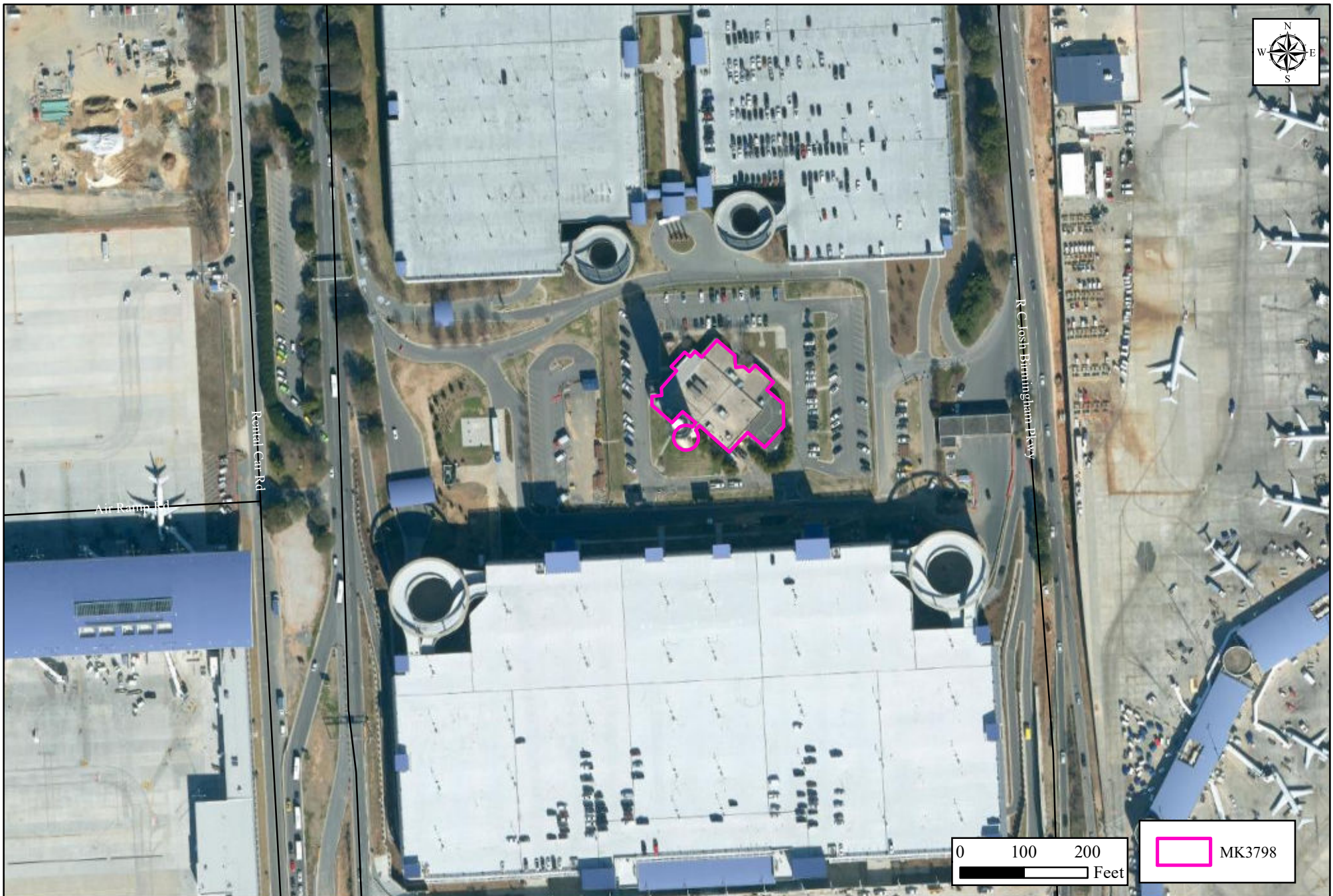


Figure 22	Charlotte Douglas International Airport Historical Resources Summary Project Mecklenburg County, North Carolina	Aerial with MK 3798 Identified Aerial Provided by ArcGIS Online <i>Environment & Archaeology</i> LLC
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MK3798, FAA Control Tower, 5507 Josh Birmingham Parkway, V.W., February 2020



MK3798_FAAControlTower_02 20_V.W._01

Front View



MK3798_FAAControlTower_02 20_V.W._02

Back View



MK3798_FAAControlTower_02 20_V.W._03

Left View



MK3798_FAAControlTower_02 20_V.W._04

Right View

CLT Box Building (MK3816)

The CLT Box Building is an unusual example of a 1980s office/light industrial building with Post-Modern influence (Figure 23 and Figure 24). Characteristic of the style are the playful nature of the design, the mixture of materials (stone, brick, concrete, metal, various types of glass), and the blue accent color carried throughout. The focal point is the main entrance with curved concrete and glass block walls. Even the rear section, which houses semi-trailer loading docks, is design-conscious, with a long, standing-seam metal roof supported by metal beams (Photo Proof Set 19).

NRHP Recommendation: While an unusual example of Post-Modern Influence, this structure is less than 40 years of age and does not meet the criteria for listing in the NRHP. When it reaches 50 years of age in 2031, the CLT Box Building may be eligible for NRHP listing under Criterion C as a locally uncommon expression of the Post-Modern movement applied to a commercial/industrial structure, provided it is not extensively altered in the meantime. Future evaluation of this structure is recommended.

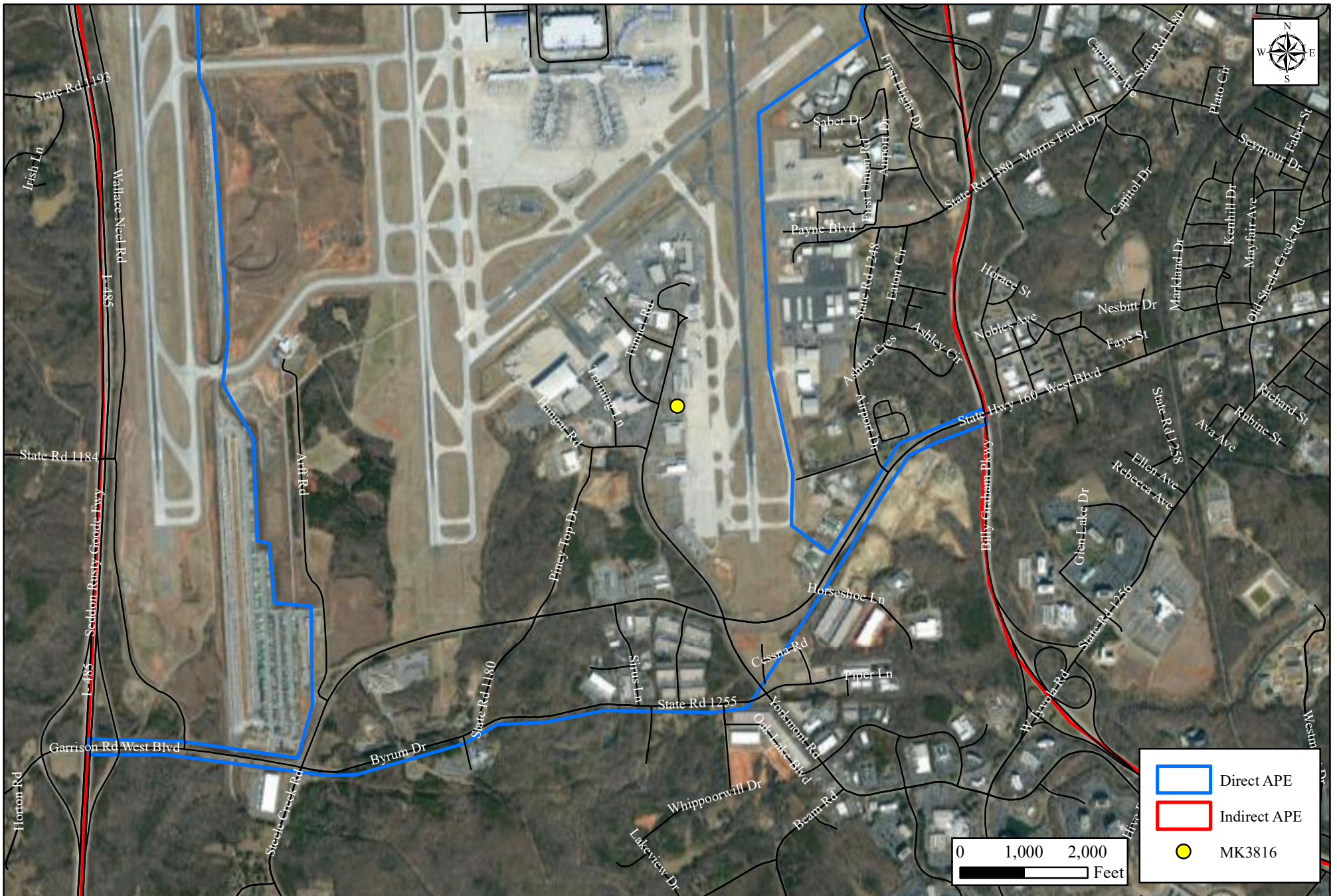


Figure 23

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 3816 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

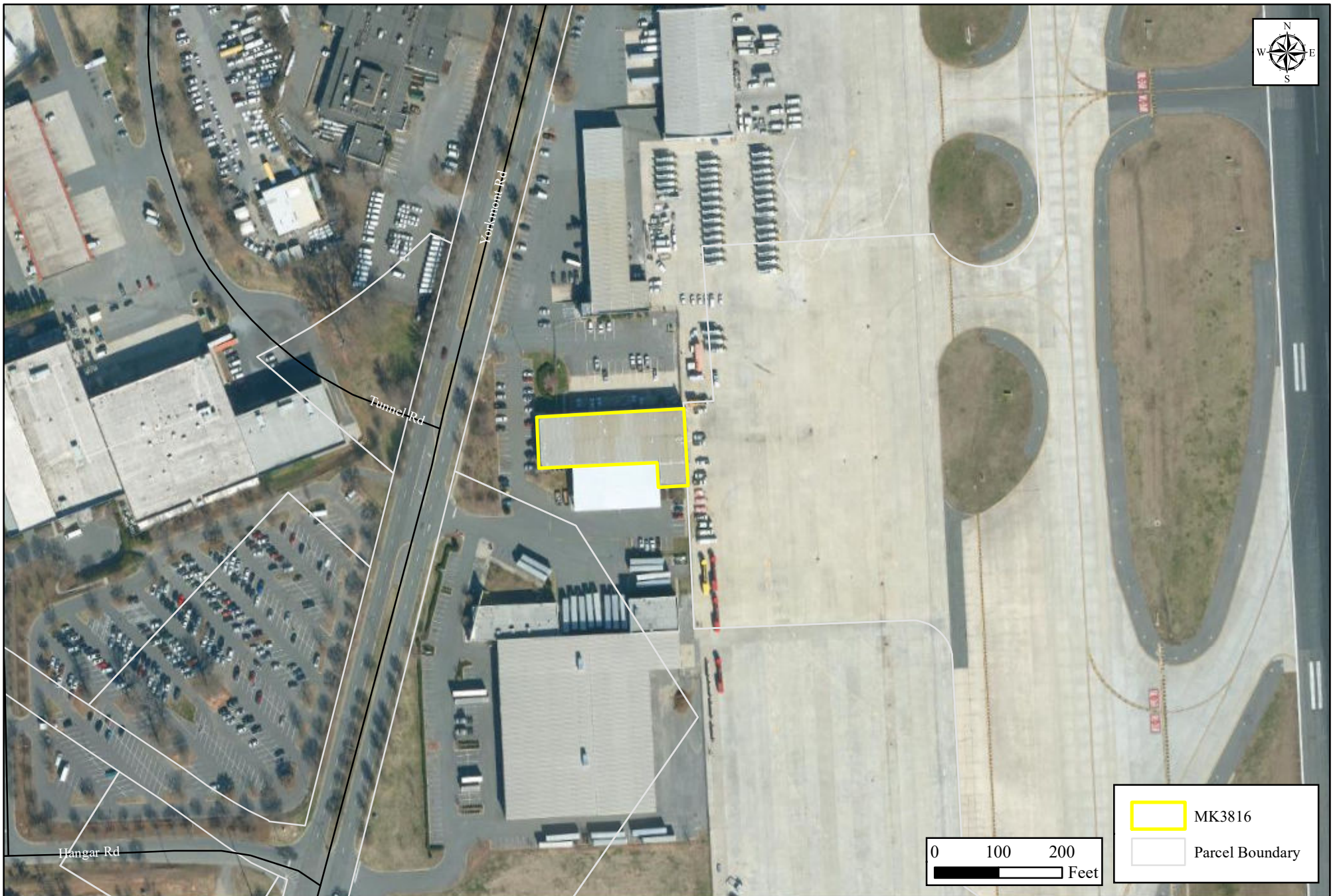


Figure 24

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 3816 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

MK3816, CLT Box Building, 4302 Yorkmont Road, V.W., February 2020



MK3816_CLTBoxBuilding_02 20_V.W._01

Front View



MK3816_CLTBoxBuilding_02 20_V.W._02

Back View



MK3816_CLTBoxBuilding_02 20_V.W._03

Left View



MK3816_CLTBoxBuilding_02 20_V.W._04

Right View

Officers Club (MK3823)

This is a former Army barracks and Civil Air Patrol Officers Club located on airport property (Figure 25 and Figure 26). It is a narrow, one-story, L-shaped frame building under an intersecting gable roof covered with roll roofing and asphalt shingles, with large industrial-style ventilators and a brick chimney. Walls are clad in shiplap and the windows are covered with metal security grids. It rests on a concrete pier foundation (Photo Proof Page 20). The building has a one-story, stepped-gable addition and a shallow bump-out in the angle of the ell. Interior features include a dropped ceiling with acoustical tile, and fluorescent lighting.

NRHP Recommendation: The Officer's Club is clearly recognizable as a barracks and is historically significant. It is recommended as eligible for listing under Criterion A for its associations with the combined military/civilian history of what became the Charlotte Douglas Airport. However, it is in seriously deteriorated condition.

Stonewall Jackson Homes (MK4126)

This is a group of 14 one-story brick-faced residences, most of which are duplexes. They were built during World War II, possibly by the military to house persons stationed or working there. The buildings comprise an intact grouping with no later additions; only one has been demolished, and no new buildings have been added (Figure 25 and Figure 26).

The buildings are sited along winding streets, with a single entrance and exit from the complex. It appears that the project construction preserved the contours of the land, with little cutting and filling evident. Sidewalks are narrow and are located on one side of the street. The buildings utilize a simple, stylistically conservative design, typical of the period, with Colonial Revival influence. They are faced with running bond brickwork with jack arches and rowlock sills, utilizing bricks of various sizes; this irregularity suggests the brick veneer may be a later addition, covering up an earlier material such as asbestos siding. Facades are symmetrical; most duplex units are six bays wide, with centered doors, and two bays deep. There are also some smaller units that are four bays wide and two deep. The windows have recently been replaced with new vinyl units of 4/4, 6/6 or 8/8 configuration, and the doors have been replaced with stock doors with fanlights. The gable ends are covered with vinyl siding and the roofs with asphalt shingles. Slender brick chimneys, found singly or in pairs, rise from the ridgelines. The buildings are in good condition (Photo Proof Page 21).

NRHP Recommendation: The Stonewall Jackson Homes are recommended eligible under Criterion A for their association with the Morris Field air base during World War II.

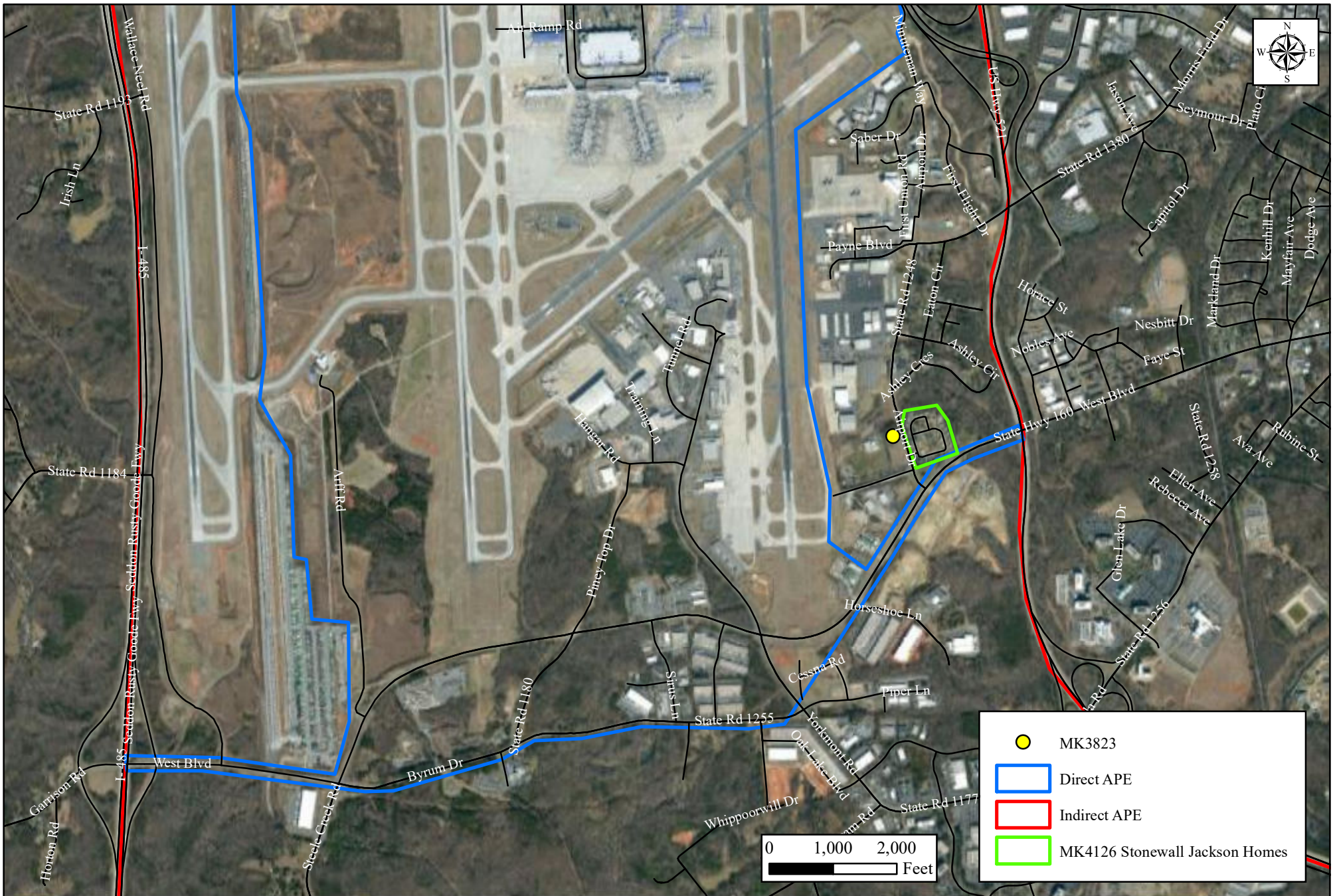


Figure 25

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 3823 and MK 4126 Stonewall Jackson Homes Development Identified
Aerial Provided by ArcGIS Online

Environment & Archaeology
LLC

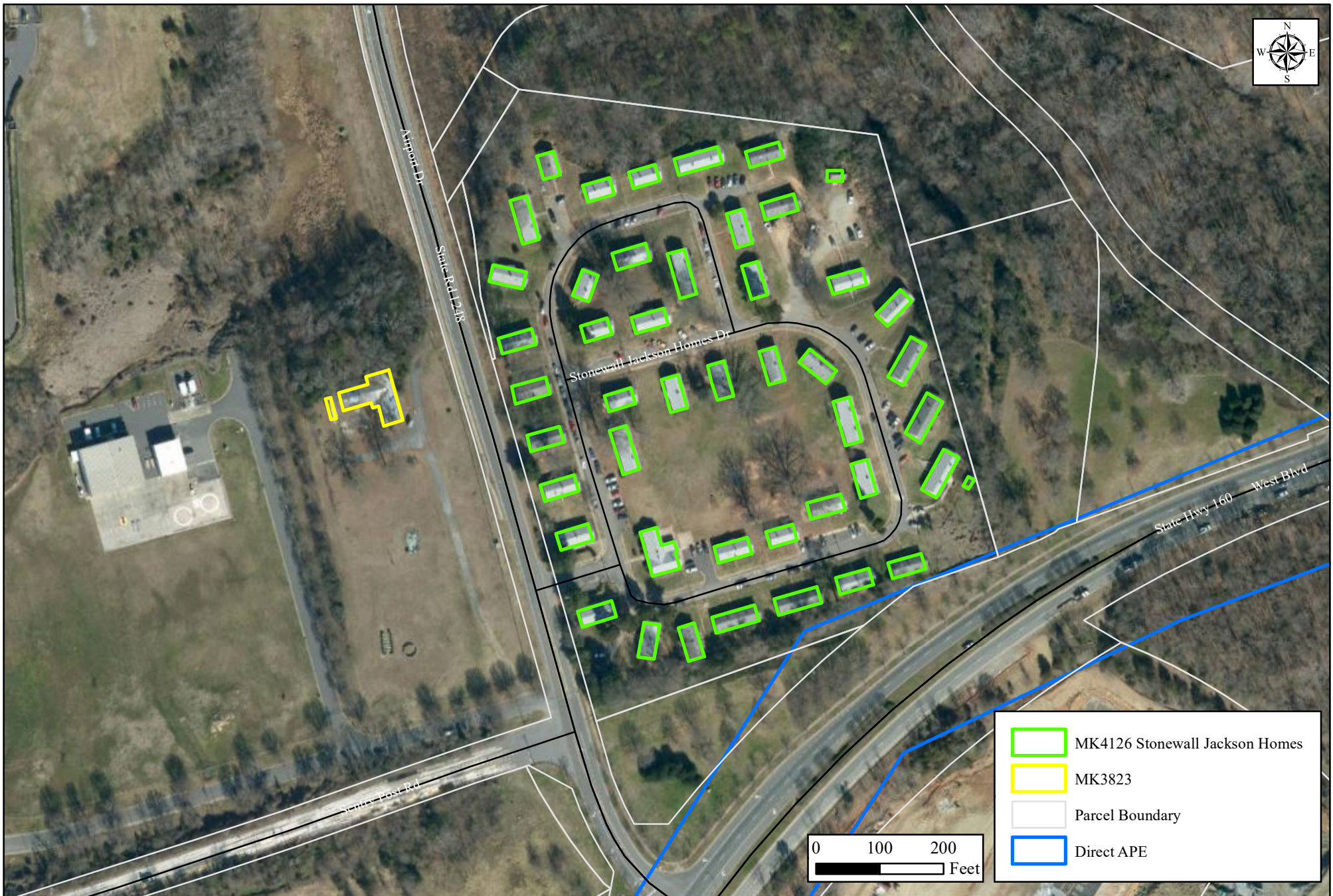


Figure 26

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 3823 and MK 4126 Stonewall Jackson Homes Development Identified
Aerial Provided by ArcGIS Online

Environment & Archaeology
LLC

MK3823, Officer's Club, 5600 Airport Drive, V.W., February 2020



MK3823_OfficersClub_02 20_V.W._01

Front View



MK3823_OfficersClub_02 20_V.W._02

Back View



MK3823_OfficersClub_02 20_V.W._03

Left View



MK3823_OfficersClub_02 20_V.W._04

Right View

MK4126, Stonewall Jackson Homes District, 5751 Airport Drive/ 137 Stonewall, V.W., February 2020



MK4126_StonewallJacksonHomes_02
20_V.W._01

Home Type 1 (Front View)



MK4126_StonewallJacksonHomes_02
20_V.W._02

Home Type 2 (Front View)



MK4126_StonewallJacksonHomes_02
20_V.W._03

Home Type 3 (Side View)



MK4126_StonewallJacksonHomes_02
20_V.W._04

Home Type 4 (Front View)



MK4126_StonewallJacksonHomes_02
20_V.W._05

Office (Front View)



MK4126_StonewallJacksonHomes_02
20_V.W._06

Office (Back View)

Cold War Era Fallout Shelter (MK3853)

This Cold War-Era fallout shelter was built c. 1960 (Figure 27 and Figure 28). According to residents, it is one of several of the structures built by WWII pilots in and around Steeleberry Drive in the Steeleberry Subdivision, a postwar development where a number of them lived. The bunker is built into a hill slope that is held back by a stepped concrete wall with metal door, and a small fence. The shelter is covered with a concrete slab with grass planted on top. Galvanized metal ventilators provide ventilation (Photo Proof Set 22).

The interior of the bunker has an L-shaped plan with an entry hall, a small room and a large open area with toilet and sink. The ceiling is reinforced concrete. The bunker is in deteriorated condition.

NRHP Recommendation: This resource is recommended as potentially eligible for listing in the NRHP under Criteria A. The significance of the Cold War Era Fallout Shelter should be evaluated in the context of Cold War-era civilian preparedness for a possible nuclear war. More research needs to be conducted to identify similar shelters, if any, in the Steeleberry Subdivision, evaluate their condition and integrity, and assess their potential significance as a multiple-properties listing or as contributing elements of a potential Steeleberry Historic District (discussed later in this report).



Figure 27

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 3853 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC



Figure 28

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 3853 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

MK3853, WWII Fallout Shelter, 9001 Whispering Pines Lanes, V.W., February 2020



MK3853_WWIIFalloutShelter_02 20_V.W._01

Front View



MK3853_WWIIFalloutShelter_02 20_V.W._02

Back View



MK3853_WWIIFalloutShelter_02 20_V.W._03

Side View



MK3853_WWIIFalloutShelter_02 20_V.W._04

Top View

Former Charlotte Speedway Ticket Office and Barn (MK3933)

There are two structures standing on this property (Figure 29 and Figure 30). One is a small, front-gabled frame house of simple design built in 1946. One story in height, it is clad in shiplap siding and has a minimal gabled porch. It is presently vacant and deteriorating and suffers from vandalism. Nearby is a barn that appears to date from the mid-20th century. It is built of concrete block with upper story faced in vertical wood siding, and a gabled roof of raised-seam metal. Varied window types include casements, double-hung and industrial-style metal multi-light sash.

Much of the surrounding property, including the house and barn, was formerly the location of the now-demolished Charlotte Speedway, which operated for about ten years before highway construction removed the track. NASCAR held its first stock car race here. Old aerial maps suggest the house was once the ticket office for the now-demolished speedway (Photo Proof Set 23 and 24).

NRHP Recommendation: The site possesses significance for its association with motor sports in North Carolina, which became very popular in the South during the 20th century. However, its integrity of setting, feeling and association has been diminished by removal of the track, and nearby road construction. It is not recommended for NRHP listing because the Occoneechee Speedway in Orange County, which is already listed, is a much more complete expression of an early racetrack in the state, retaining an actual dirt track, four buildings and other original features. This property is not recommended as eligible for listing in the NRHP.

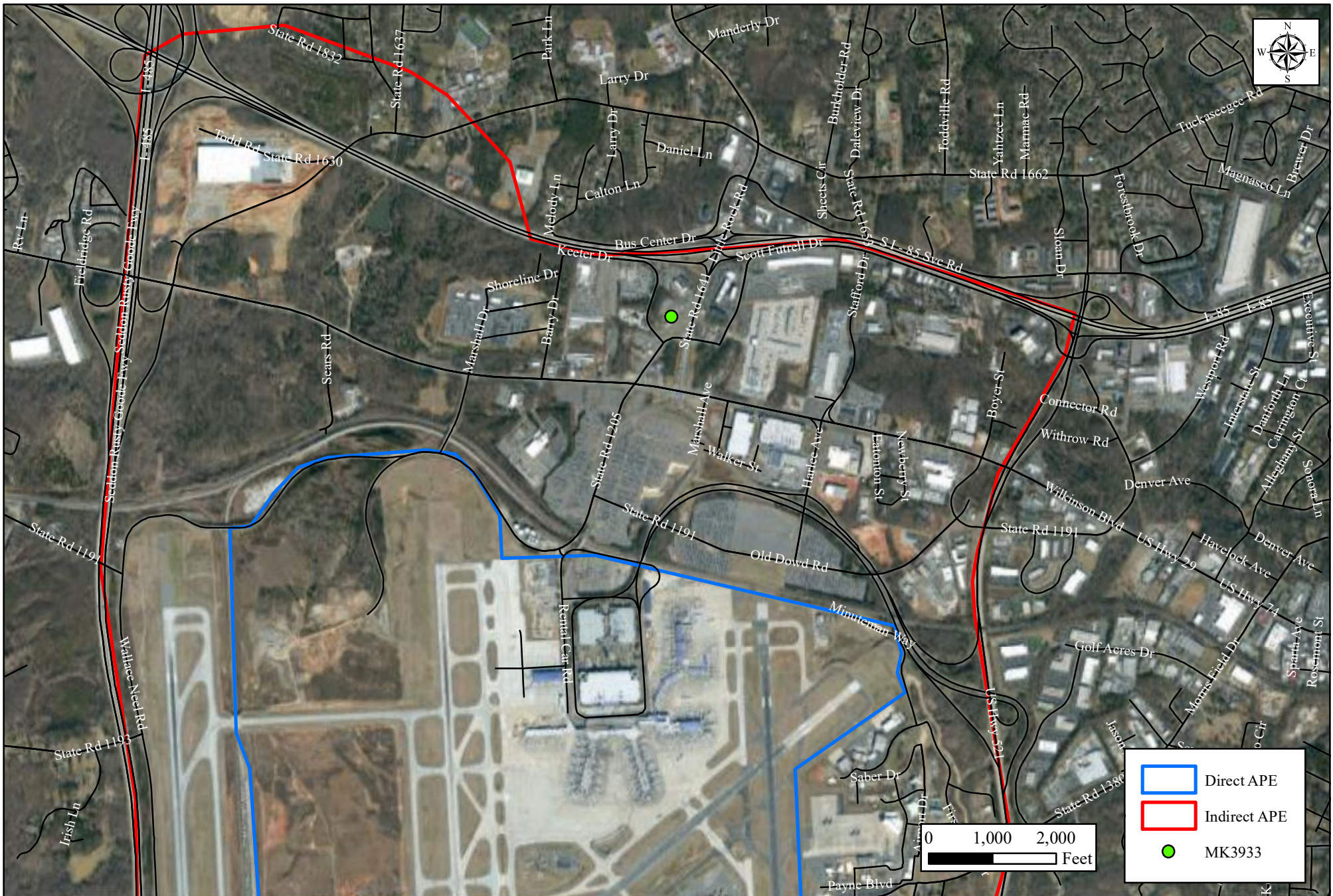


Figure 29

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 3933 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC



Figure 30

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 3933 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC



MK3933_CharlotteSpeedwayOffice_02 20_V.W._01

Back View



MK3933_CharlotteSpeedwayOffice_02 20_V.W._02

Left View



MK3933_CharlotteSpeedwayOffice_02 20_V.W._03

Front View



MK3933_CharlotteSpeedwayOffice_02 20_V.W._04

Right View

MK3933, Former Charlotte Speedway Ticket Office and barn, 2900 Keeter Drive/2840 Little Rock Road, V.W., February 2020



MK3933_FormerCharlotteSpeedwayTicketOfficeandbarn_02 20_V.W._01

Front View



MK3933_FormerCharlotteSpeedwayTicketOfficeandbarn_02 20_V.W._02

Back View



MK3933_FormerCharlotteSpeedwayTicketOfficeandbarn_02 20_V.W._03

Side View



MK3933_FormerCharlotteSpeedwayTicketOfficeandbarn_02 20_V.W._04

Outbuilding

West Mecklenburg High School (MK4023)

The West Mecklenburg High School campus is a complex of 11 buildings built over a period of years in the mid-to-late 20th century: 1940, 1957, 1967, 1993, 1995 and 1997 (Figure 31 and Figure 32). The original school buildings exhibit functional Modernist design with flat roofs, reddish-brown brick, and porches / walkways with metal posts and corrugated roofs. A number of window types are used including metal multi-pane casements, awning windows and sliding sash. There are numerous mobile classrooms as well, likely used as overflow space: prefabricated, side-gabled metal buildings on concrete blocks (Photo Proof Set 25 and 26).

Also on the campus is Building 11, a white-painted, wood-frame Cape Cod that appears to have been built as a residence c. 1940. It features shiplap siding, metal multi-light casements and a side-gabled roof. A sign on the front wall reads “Communities in Schools, White House,” suggesting it serves a school-related function.

NRHP Recommendation: While the high school presumably has played an important role in the lives of students in Charlotte-Mecklenburg for over half a century, the complex is not recommended for NRHP listing under Criterion C, in the area of architecture. It is a characteristic example of a mid-century suburban educational campus that evolved gradually over the decades and its buildings lack architectural distinction individually or collectively.

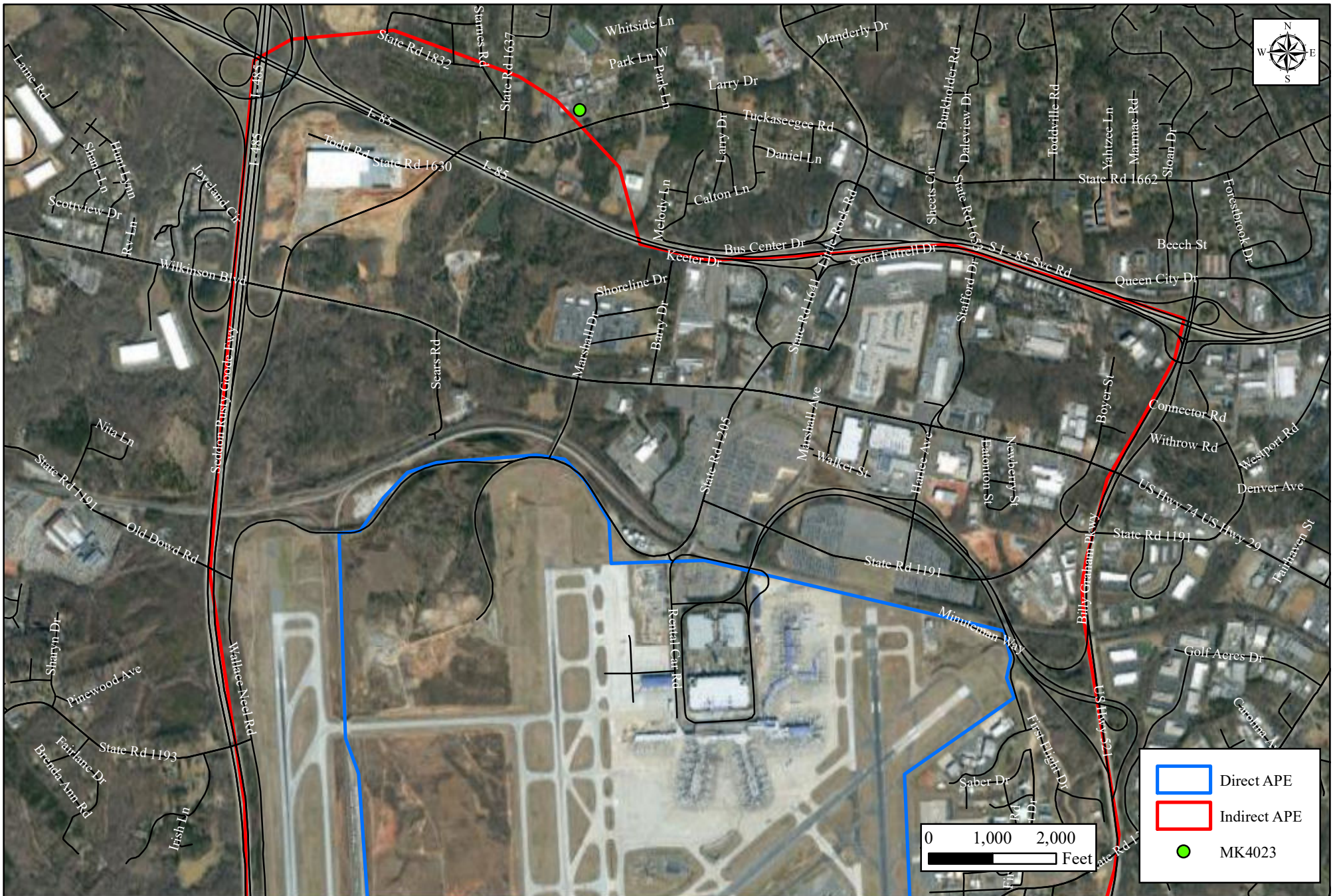


Figure 31

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 4023 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC



Figure 32

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 4023 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

MK4023, West Mecklenburg Highschool, 7400 Tuckaseegee Road, V.W., February 2020



MK4023_WestMecklenburgHighschool_02 20_V.W._01

Front View



MK4023_WestMecklenburgHighschool_02 20_V.W._02

Back View



MK4023_WestMecklenburgHighschool_02 20_V.W._03

Right View



MK4023_WestMecklenburgHighschool_02 20_V.W._04

Left View



MK4023_WestMecklenburgHighschool_02 20_V.W._05

Side View



MK4023_WestMecklenburgHighschool_02 20_V.W._06

Outbuilding

4824 and 4828 Parkway Plaza Boulevard (MK4123) (MK4115)

4824 and 4828 Parkway Plaza Boulevard are corporate headquarters buildings in a suburban office park whose designs share common elements (Figure 33 and Figure 34). They have in common reddish-brown brickwork, flat or circular columns, square-paned glass walls and stepped-back facades. A semicircular glass wall follows the curve of the pond and a prim row of small shrubs edges the parking lot. The buildings' design has a harmony and sophistication that sets them aside from most corporate office parks. No noncontributing elements have been added (Photo Proof Set 27 and 28).

NRHP Recommendation: The buildings at 4824 and 4828 Parkway Plaza Boulevard do not meet the traditional age criterion for listing in the NRHP. In the future, these buildings could potentially be eligible for Register listing under Criterion C for architectural significance once they reach 50 years of age in 2032-2033, provided they are not extensively altered in the meantime. Future evaluation is recommended for these structures.



Figure 33

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 4123 and MK 4115 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC



Figure 34

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 4123 and MK 4115 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

MK4115, Corporate Headquarters, 4828 Parkway Plaza Boulevard, V.W., February 2020



MK4115_CorporateHeadquarters_4828ParkwayPlazaBoulevard_02 20_V.W._01

Side View 1



MK4115_CorporateHeadquarters_4828ParkwayPlazaBoulevard_02 20_V.W._02

Front View 1



MK4115_CorporateHeadquarters_4828ParkwayPlazaBoulevard_02 20_V.W._03

Side View 2



MK4115_CorporateHeadquarters_4828ParkwayPlazaBoulevard_02 20_V.W._04

Front View 2

MK4123, Corporate Headquarters, 4824 Parkway Plaza, V.W., February 2020



MK4123_CorporateHeadquarters_4824ParkwayPlaza_02 20_V.W._01

Side View 1



MK4123_CorporateHeadquarters_4824ParkwayPlaza_02 20_V.W._02

Side View 2



MK4123_CorporateHeadquarters_4824ParkwayPlaza_02 20_V.W._03

Back View



MK4123_CorporateHeadquarters_4824ParkwayPlaza_02 20_V.W._04

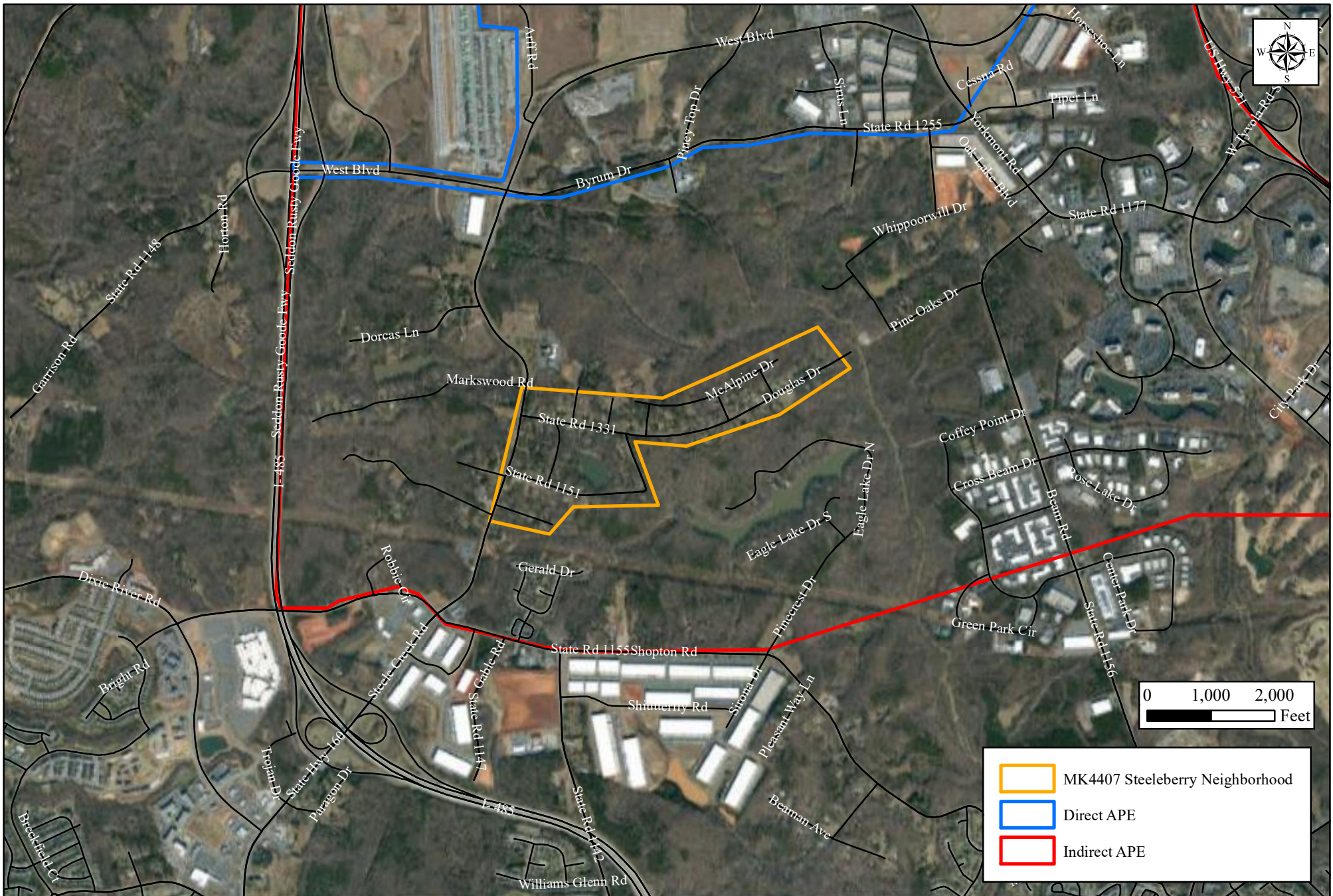
Front View

Steeleberry Drive Neighborhood (MK4407)

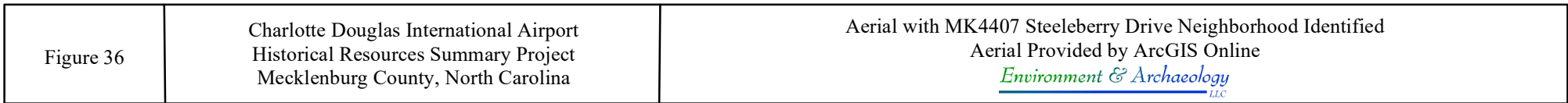
The Steeleberry Drive neighborhood is a postwar suburban subdivision developed in the 1950s, with narrow, winding roads, large lots and mature trees. Consistency of plan and siting lends visual consistency and a strong sense of place. Most houses have ranch-style, split-level or L-shaped plans with attached garages. They are laid out horizontally, facing the road, with deep, consistent setbacks. Most are built of brick with low-hipped or gabled roofs and large picture windows, reinforcing the horizontal emphasis. Individual buildings may lack distinction but virtually all contribute to the larger whole.

A number of WWII pilots built homes in the subdivision, presumably taking advantage of the GI Bill. Some are said to have built fallout shelters in their yards during the Cold War. A representative example is the underground concrete bunker on the property of 19001 Whispering Pines Lane (MK3853).

NRHP Recommendation: The Steeleberry Drive neighborhood may represent a significant example of the Post World War II transition from a wartime footing to a more traditional suburban life style. As such, it is potentially eligible for NRHP listing under Criterion A for association with post-World War II suburban development. Further historical research is required to determine if this is the case and to determine the neighborhood's eligibility.



<p>Figure 35</p>	<p>Charlotte Douglas International Airport Historical Resources Summary Project Mecklenburg County, North Carolina</p>	<p>Aerial with MK4407 Steeleberry Drive Neighborhood Identified Aerial Provided by ArcGIS Online <i>Environment & Archaeology</i> LLC</p>
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Harvest Center, 5415 Airport Drive (MK4004)

This resource consists of two adjacent buildings, both part of the Harvest Center of Charlotte food bank and Social Service Agency (Figure 37 and Figure 38). These resources predate the creation of the Harvest Center (Photo Proof Set 29).

Former NCO Club: This one-story brick building formerly was a Non-Commissioned Officers Club (Building 269) used by the military during the WW II era of the airport. The almost-symmetrical front façade is seven bays wide. Most original windows have been replaced by 1/1 sash, but are covered in an offset metal grid to give the impression of 8/8 windows while providing security. A 2-bay addition, 1 story in height with full basement, later was made to the original structure, filling in a corner of the “T.”

The building has seen minor alterations including window replacement and the rear addition, which has a compatible design and minimal visibility from the street, but retains overall integrity as a mid-20th century public building.

NRHP Recommendation: The former NCO Club building is recommended eligible under Criterion A for its association with the everyday life of the military in Charlotte/Mecklenburg during the World War II era.

Morris Field Baptist Chapel: Per “The Charlotte Mecklenburg Story,” this chapel was built by the US Army. It was dedicated on September 14, 1941, on Airport Drive, and is noted on a 1945 military map as Building #117. It can also be seen on a 1951 aerial map. By 1970 it has been relocated to the present site. The chapel is a gabled frame structure bearing a square tower and four-sided steeple with finial. It has a shallow vestibule and a gabled frame porch carried by Doric columns. A long, gabled porch supported by brick columns connects the chapel to the gymnasium, a large, windowless, brick structure built in 1991.

NRHP Recommendation: The Morris Field Baptist Chapel is under consideration for listing as a Charlotte-Mecklenburg historic landmark. It possesses significance for its association with the everyday life of the military during the World War II era. The building’s integrity of site has been diminished by its relocation and the addition of the covered walkway and gym, but also retains sufficient integrity under its remaining aspects to meet National Register Criterion A for its association with the everyday life of the military in Charlotte/Mecklenburg during the World War II era. This resource is recommended eligible for listing in the NRHP under Criterion A.

“Housing Building #4 (MK3960), Eaton Circle of Ashley/Eaton Circle Development (MK4127)”

This group of apartment buildings includes duplexes, triplexes and quadruplexes. The buildings are two-story brick structures of simple design under side-gabled roofs with vinyl-sided gables, standing five bays wide and two deep (Figure 37 and Figure 38). Walls are clad in running bond brick veneer. The symmetrical facades have centered entrances in shallow, gabled vestibules with brick steps and

iron handrails. Doors are half-glazed replacement units. Windows are 1/1 or 6/6 double-hung vinyl sash. In the rear are prefabricated, vertical wood plank sheds with shed roofs and three doors, presumably one for each unit. The development has a verdant setting with lawns, trees, brick-lined flowerbeds, and wooden bridges spanning a dry creek bed (Photo Proof Set 30). The buildings are in good condition and the housing development has remained intact with no new buildings added.

The buildings represent typical mid-20th-century multi-family design and lack individual or collective distinction. They have seen cosmetic alterations in the form of window and door replacement, but no significant changes have been made. The development is a cohesive entity with no new structures added or removed. The buildings' 1941 construction date and proximity to the airport suggests they were built as part of the war effort; little housing was being built for civilians at the time.

NRHP Recommendation: The original purpose of the Housing Building #4 property and the development as a whole, is not known at this time. If constructed as part of the World War II effort, they both may be eligible for NRHP listing under Criterion A for their association with the war effort on the homefront. As such, more historical and documentary research is needed to assess Housing Building #4 and the overall development's significance or lack thereof.

Ashley Circle of Ashley/Eaton Circle Development (MK4127)

These houses are part of a small development of four modest, one-story ranch homes built in 1945. These frame structures feature asymmetrical four-bay facades with gabled corner porches and off-center roof gables (Figure 37 and Figure 38). They are built on crawl spaces. Window types used include three-part focal windows (large fixed center pane framed by small casements). Roofs are side-gabled, covered in asphalt shingles and pierced by small brick chimneys. The houses occupy ample half-acre lots with large shade trees. They are in fair to good condition. Two of the houses in the grouping—4900 and 5000—have been considerably altered and do not contribute to its significance.

The houses' construction date suggests they may have been built for service members returning from the war, and their families. Several ranch homes similar to the Ashley Circle residences can be found on the intersecting Eaton Drive, suggesting a common builder or design source for the two developments.

NRHP Recommendation: Based on the potential construction of these houses for service members returning from World War II, these properties may be eligible under Criterion A for their association with the domestic war effort. More research is needed to find how they were used in their period of significance, and to assess their significance under National Register criteria.



Figure 37

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 4004, MK 3960 and MK 4127 Ashley/Eaton Circle Development Identified
Aerial Provided by ArcGIS Online

Environment & Archaeology
LLC



Figure 38

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 4004, MK 3960, and MK 4127 Ashley/Eaton Circle Development
Identified Aerial Provided by ArcGIS Online

Environment & Archaeology
LLC



MK4004_
HarvestCenterofCharlotte,Building269ofWWII:N.C.O.Club_02
20_V.W._01

Front View



MK4004_
HarvestCenterofCharlotte,Building269ofWWII:N.C.O.Club_02
20_V.W._02

Back View



MK4004_
HarvestCenterofCharlotte,Building269ofWWII:N.C.O.Club_02
20_V.W._03

Right View



MK4004_
HarvestCenterofCharlotte,Building269ofWWII:N.C.O.Club_02
20_V.W._04

Left View

MK3960, 5408 Eaton Circle, V.W., February 2020



MK3960_5408EatonCircle_02 20_V.W._01

Front View 1



MK3960_5408EatonCircle_02 20_V.W._02

Front View 2



MK3960_5408EatonCircle_02 20_V.W._03

Left View



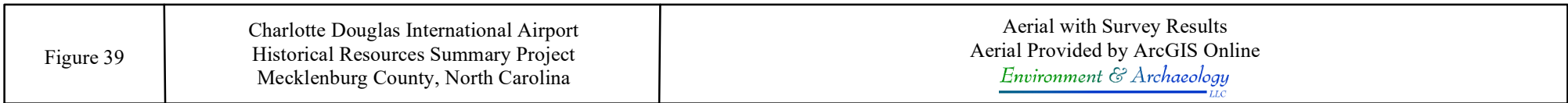
MK3960_5408EatonCircle_02 20_V.W._04

Rear View

PROPERTIES NOT AVAILABLE FOR SURVEY

Eagle Lake Community (multi-year)

The Eagle Lake Community is located at the southern end of the Project Area and consists of a gated community (Figure 39 and Figure 40). Based on the PVA database there are potentially forty-three (43) parcels within this neighborhood with some properties dating back to the 1940's. At the time of the HRS, *Environment & Archaeology, LLC* was unable to gain access to this community due to a locked gate and no property management contact information available. Both L&B and CLT were aware of this community at the time of the HRS with both informing *Environment & Archaeology, LLC* that they were also unable to gain access to this community in previous attempts.



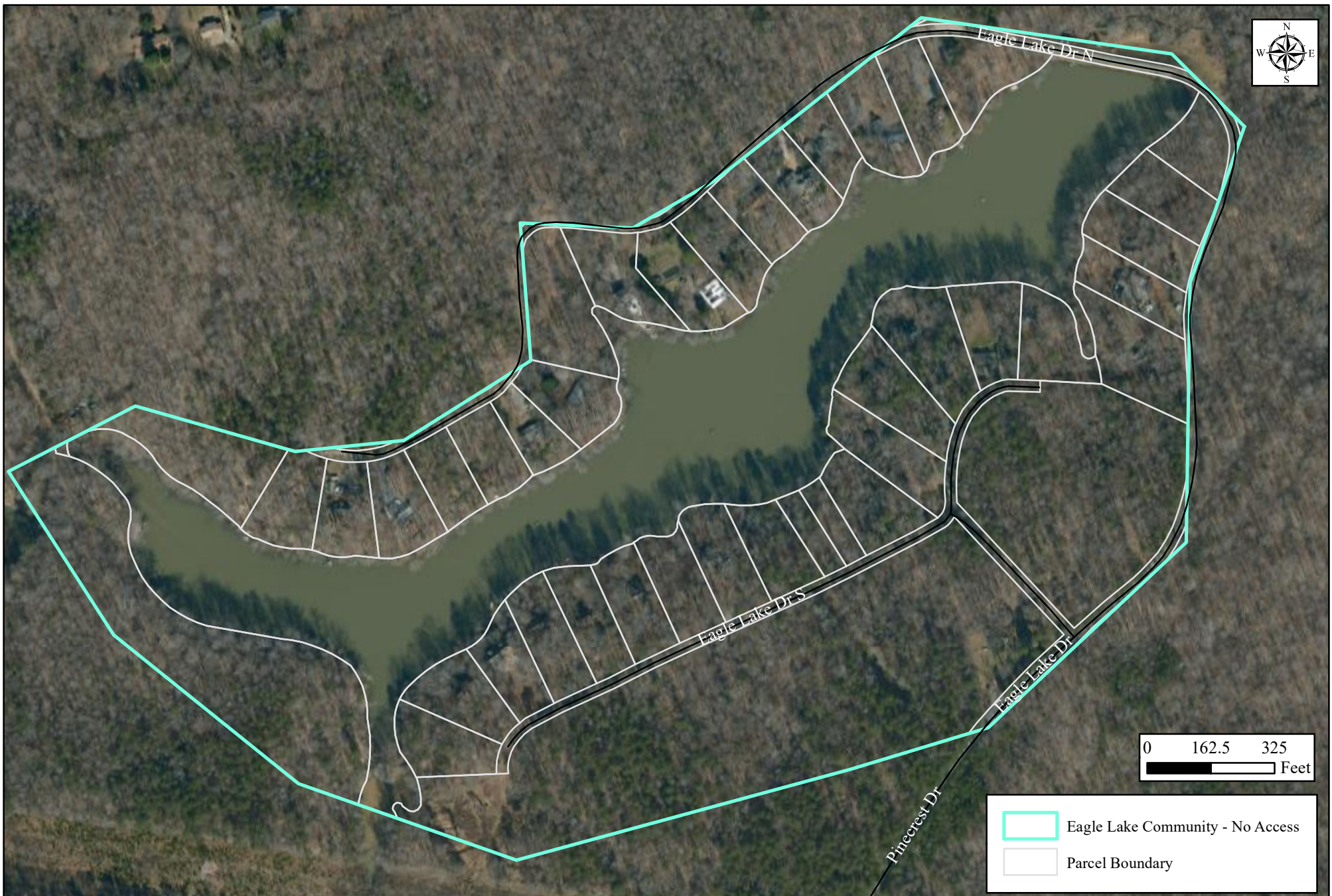


Figure 40

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with Survey Results
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

Air National Guard Property (ANG)

At the time of the HRS, *Environment & Archaeology, LLC* was unable to gain access to the ANG property due to it being on lock-down in anticipation of a visit to Charlotte by The President of the United States (Figure 41 and Figure 42). Additionally, *Environment & Archaeology, LLC* learned that the ANG property would stay on lock-down for the foreseeable future. The ANG property was previously surveyed in 2013, ER# 13-1453, with none of the structures present on the ANG property being evaluated as eligible to the NRHP. Since *Environment & Archaeology, LLC* will not be able to evaluate the ANG property, the 2013 recommendations will stand.

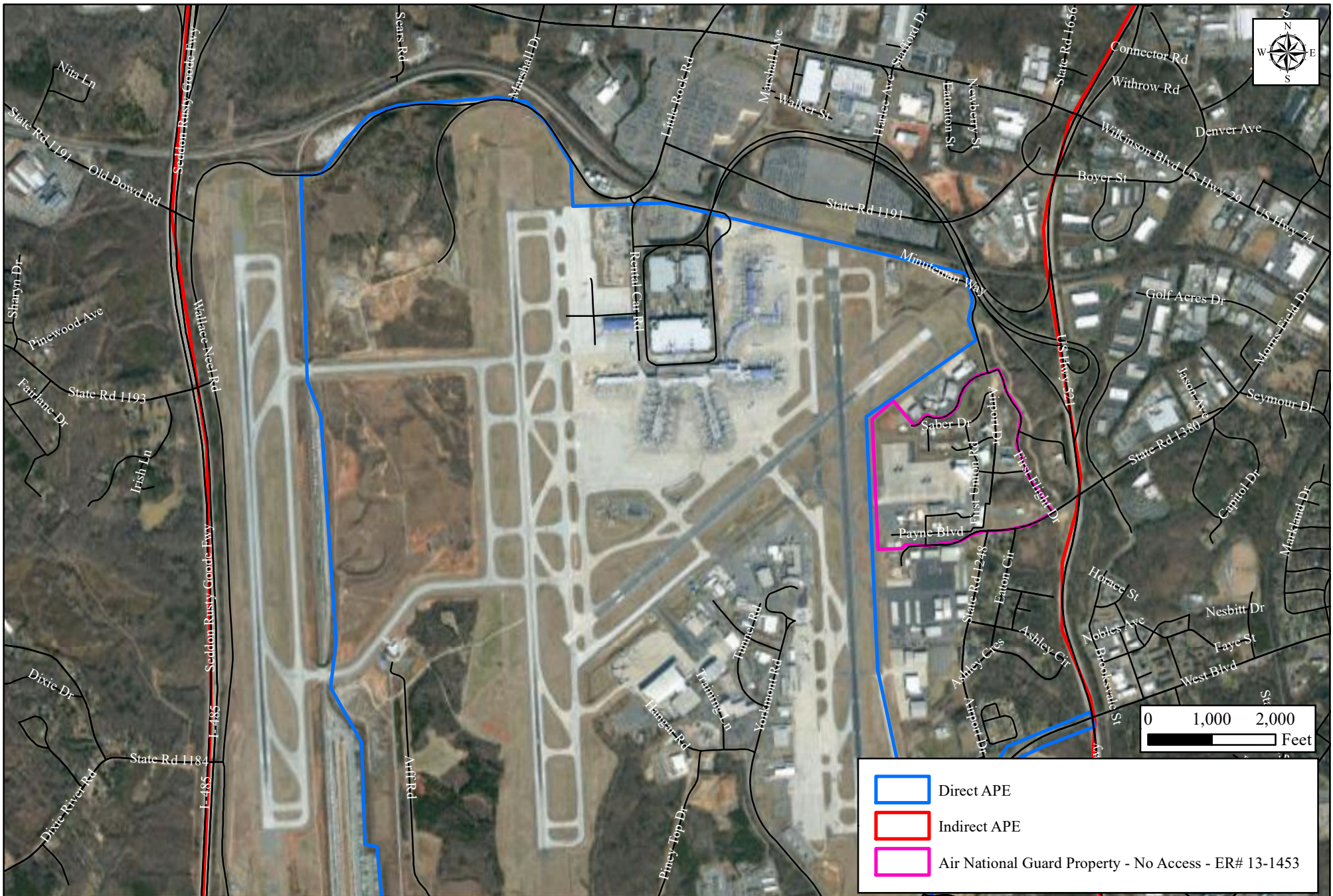


Figure 41

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with Survey Results
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

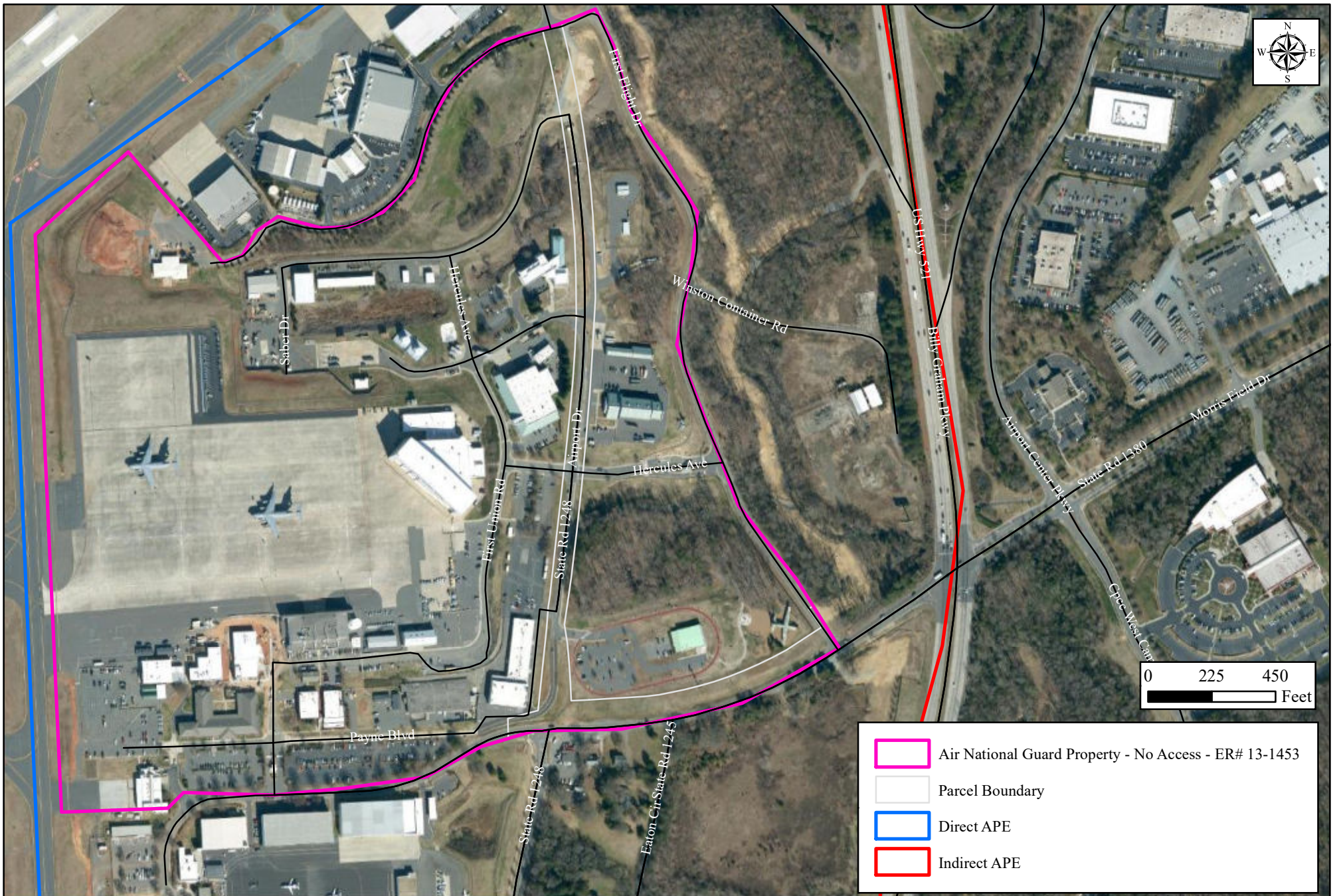


Figure 42

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with Survey Results
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

SUMMARY

Environment & Archaeology, LLC has completed a Historic Resources Survey for the Charlotte Douglas International Airport in Charlotte, Mecklenburg County, North Carolina. A total of 599 properties were included in the survey. The following summarizes the recommendations made in this report.

Properties Listed in the NRHP

One property within the Survey Area was previously Listed in the NRHP:

- Steele Creek Presbyterian Church, MK1377

Properties Previously Determined Eligible for Listing in the NRHP and Covered Under an Existing MOA

- William Grier House, MK1364/MK3993
- John Douglas House, MK1361
- Spratt-Grier Farm, MK1875
- Byrum Croft House, MK2841
- Steele Creek Manse, MK1378 – Possible Contributing Elements Recommended for Additional Research

Properties Previously Recommended as Eligible for Listing in NRHP

- Oakden Motel, MK2150
- WPA Douglas Airport Hanger, MK2399/3761
- Two Guys Auto Repair Body Shop, MK3197

Properties Newly Recommended as Eligible for Listing in the NRHP

- Old Terminal Building, MK2189
- Fred Webber House, MK3192
- Officer's Club, MK3823
- Thomas W. Ferebee Building, MK3825
- Harvest Center Properties: NCO Club and Morris Field Baptist Chapel, MK4004
- 5455-5457-5459-5461 Wilkinson Boulevard, MK4047
- Stonewall Jackson Homes, MK 4126
- Housing Building #4, MK3960

Properties Recommended as Potentially Eligible for Listing in the NRHP

- Cold War Era Fallout Shelter, MK3853
- Steeleberry Drive Neighborhood, MK4407
- Ashley/Eaton Circle Development, MK4127

Properties Recommended for Future Evaluation

- 3100 Yorkmont Road, MK3770
- 3140 Yorkmont Road, MK3762
- FAA Control Tower MK3798
- CLT Box Building, MK3816
- 4824 and 4828 Parkway Plaza Boulevard, MK4115 and MK 4123

Properties Recommended as Not Eligible for Listing in the NRHP

- 9311 Markswood Road, MK3836
- Big Spring Methodist Church, MK3822
- Former Charlotte Speedway Ticket Office and Barn, MK3933
- West Mecklenburg High School, MK4023

Properties Not Available for Survey

- Eagle Lake Community (multi-year)
- Air National Guard Property

APPENDIX A:

NC DNCR Consultation; March 10, 2020 NC DNCR Letter

April 3, 2020 Conference Call Support Materials/Presentation



**North Carolina Department of Natural and Cultural Resources
State Historic Preservation Office**

Ramona M. Bartos, Administrator

Governor Roy Cooper
Secretary Susi H. Hamilton

Office of Archives and History
Deputy Secretary Kevin Cherry

March 10, 2020

Luke W. Erickson
221 Main Street
Florence, KY 41042

lerickson@environment-archaeology.com

Re: Consultation for CLT Resources Survey for EA, Charlotte, Mecklenburg County, ER 16-1791

Dear Mr. Erickson:

Thank you for your January 9, 2020, letter concerning the above-referenced survey. We apologize for our delayed response. Having discussed this matter with you, we understand that you are asking about a Historic Structures Survey for above-ground resources. Further, that the survey will be undertaken by Courtney Stoll of Environment and Archaeology for the proposed CLT Improvement Projects.

Given the history of this project, the long timeline predicted for its completion, and the numbers of properties likely to involved, I contacted Ms. Stoll to discuss a survey methodology that we believe will be more efficient and informative than the usual historic structures survey and resulting report. We agreed that it would help her and us to schedule a meeting for members of our staff to review her initial findings, including mapping, photographs, construction dates, and any other information that would help inform an initial winnowing of properties to those that would be more intensively investigated as well as to determine a strategy for later evaluations over the life of the proposed improvement projects and as properties reach the 50 year mark.

Ms. Stoll agreed to provide us with possible dates for such a meeting, recognizing that her draft report is scheduled for early April. Thus, we look forward to hearing from her to set that meeting.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, contact Renee Gledhill-Earley, environmental review coordinator, at 919-814-6579 or environmental.review@ncdcr.gov. In all future communication concerning this project, please cite the above referenced tracking number.

Sincerely,



Ramona Bartos, Deputy
State Historic Preservation Officer

cc: Courtney Stoll
Sarah David

cstoll@environment-archaeology.com
sarah.david@ncdcr.gov

APPENDIX B:

May 3, 2018 Amendment 1 of MOA between FAA and NC SHPO

APPENDIX C:

June 2, 2000 Version of MOA between FAA and NC SHPO

APPENDIX D:

Historic Resource Forms for Properties in Report

APPENDIX E:

Table of Historic Properties Recorded within the APE/Survey Area

DRAFT REPORT

Archaeological Survey and Site Evaluation
Capacity Enhancements Environmental Assessment
Charlotte-Douglas International Airport, Berryhill Township, Mecklenburg County, North Carolina
North Carolina Environmental Review Number 16-1791

Prepared for:

Landrum and Brown, Inc.
Global Aviation Planning and Development
4445 Lake Forest Drive
Suite 700
Cincinnati, Ohio 45242



On behalf of:

Charlotte-Douglas International Airport
7300 Old Dowd Road
Charlotte, North Carolina 28208



Prepared by:

Deborah Joy MA, Registered Professional Archaeologist
Legacy Research Associates
125 West Woodridge Drive
Durham, North Carolina 27707



17 June 2020

MANAGEMENT SUMMARY

This report presents the results of the archaeological investigations that were conducted for the proposed Capacity Enhancements undertaking at the Charlotte-Douglas International Airport (NC Environmental Review 16-1791). It includes fieldwork conducted in 2019 by New South Associates Inc. (New South) in Greensboro, North Carolina (NC), under contract with VHB Engineering and work conducted in 2020 by Legacy Research Associates (Legacy) in Durham, NC, under contract with Landrum & Brown, Inc. (L&B). The lead Federal agency for the proposed undertaking is the Federal Aviation Administration (FAA).

The services provided were pursuant to Section 106 of the National Historic Preservation Act (NHPA) of 1966, the Archeological and Historical Preservation Act (AHPA) of 1974, Executive Order 11593 *Protection and Enhancement of the Cultural Environment*, and the Advisory Council on Historic Preservation (ACHP) regulations for the *Protection of Historic Properties* (36 CFR Part 800). The work complied with the 2017 North Carolina Office of State Archaeology (NC OSA) *Archaeological Investigations Standards and Guidelines for Background Research, Field Methodologies, Technical Reports, and Curation*.

The proposed Capacity Enhancements undertaking (Project) is comprised of the following elements: construction of a new fourth parallel runway (which requires relocation of West Boulevard) and expansion of Concourse B and Concourse C. On 11 May 2018, the North Carolina State Historic Preservation Office (NCHPO) determined that Project elements located on land developed for airport infrastructure were unlikely to adversely affect any significant archaeological resources. Only the West Boulevard Relocation element was determined to have the potential to adversely affect archaeologically unevaluated areas and previously recorded archaeological sites that had not been evaluated for eligibility to the National Register of Historic Places (National Register).

Three alignments were under consideration for the West Boulevard Relocation.

- Relocation Corridor A follows Byrum Drive from Steele Creek Road to Piney Top Drive and reconnects with existing West Boulevard east of the Runway Protection Zone (RPZ) for the proposed Fourth Parallel Runway.
- Relocation Corridor B follows Byrum Drive from Steele Creek Road to Sirius Lane, where a new roadway section will curve south and east across Whippoorwill Drive to connect with the Beam Road/Yorkmont Road intersection.
- Relocation Corridor C follows existing Byrum Drive from Steele Creek Road to Yorkmont Road and continues northeast on a new roadway section to reconnect with existing West Boulevard near Horseshoe Lane.

The three proposed relocation corridors, a 962-meter (m) segment of existing West Boulevard between Steele Creek Road and Interstate 485 (I-485), and a 950-m segment of existing West Boulevard between the east end of Corridor A and Yorkmont Road were included in the West Boulevard Relocation Study Area (study area). The width of the study area for these alignments was defined as 30 m to either side of the proposed or existing centerlines. The study area covered approximately 116.57 acres.

Field investigations of the study area were conducted by New South in March 2019 and changes to the study area in 2020 were surveyed by Legacy's subconsultant, Environmental Services Inc. (ESI), in April 2020.¹ The survey consisted of systematically excavating shovel tests at 30-m intervals within the study area. Approximately 50 percent of the study area was examined with subsurface testing; the remainder of the study area had extensive ground disturbance or wetlands that were not examined with shovel testing. The objective of the survey was to identify the general distribution, location, and nature of archaeological sites; evaluate them for eligibility to the National Register; and assess Project-related effects on significant sites.

Archaeological investigations in the 116.57-acre study area revisited two previously recorded sites (31MK0228 and 31MK0229) that had been identified by the NCHPO as being unevaluated for the National Register and recorded two new sites (31MK1146 and 31MK1147). All four sites consist of small and/or disturbed artifact assemblages (prehistoric lithic scatters and twentieth-century domestic artifact scatters) that lack potential to contribute meaningful knowledge to the history or prehistory of the area. These sites are recommended as being not eligible for the National Register. No further archaeological work is recommended for the West Boulevard Relocation as it is currently planned.

¹ Information about the March 2019 New South archaeological field investigation throughout this report was extracted from two sources that include (1) the *Affected Environment* in the Final Charlotte-Douglas International Airport Environmental Impact Statement dated 22 May 2019 prepared by VHB Engineering with contributions from Environmental Science Associates, Hart and Hickman PC, KP Environmental Sciences, and New South and (2) North Carolina Archaeological Site Forms for 31MK0228, 31MK0229, 31MK1146, and 31MK1147 prepared by New South.

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Archaeological Survey and Site Evaluation
Capacity Enhancements Environmental Assessment
Charlotte-Douglas International Airport, Berryhill Township, Mecklenburg County, North Carolina
North Carolina Environmental Review Number 15-1391

INTRODUCTION

This report presents the results of the archaeological investigations that were conducted for the proposed Capacity Enhancements undertaking at the Charlotte-Douglas International Airport (NC Environmental Review 16-1791). It includes fieldwork conducted in March 2019 by New South Associates Inc. (New South) in Greensboro, North Carolina (NC), under contract with VHB Engineering, and work conducted in April 2020 by Legacy Research Associates (Legacy) in Durham, NC, under contract with Landrum & Brown, Inc. (L&B). The lead Federal agency for the proposed undertaking is the Federal Aviation Administration (FAA).

The services provided were pursuant to Section 106 of the National Historic Preservation Act (NHPA) of 1966, the Archaeological and Historical Preservation Act (AHPA) of 1974, Executive Order 11593 Protection and Enhancement of the Cultural Environment, and the Advisory Council on Historic Preservation (ACHP) regulations for the Protection of Historic Properties (36 CFR Part 800). The work complied with the 2017 North Carolina Office of State Archaeology (NC OSA) Archaeological Investigations Standards and Guidelines for Background Research, Field Methodologies, Technical Reports, and Curation.

The proposed Capacity Enhancements undertaking (Project) is comprised of the following elements: construction of a new fourth parallel runway (which requires relocation of West Boulevard) and expansion of Concourse B and Concourse C. Figure 1 depicts the boundaries of where actual physical impacts from the Project will occur.

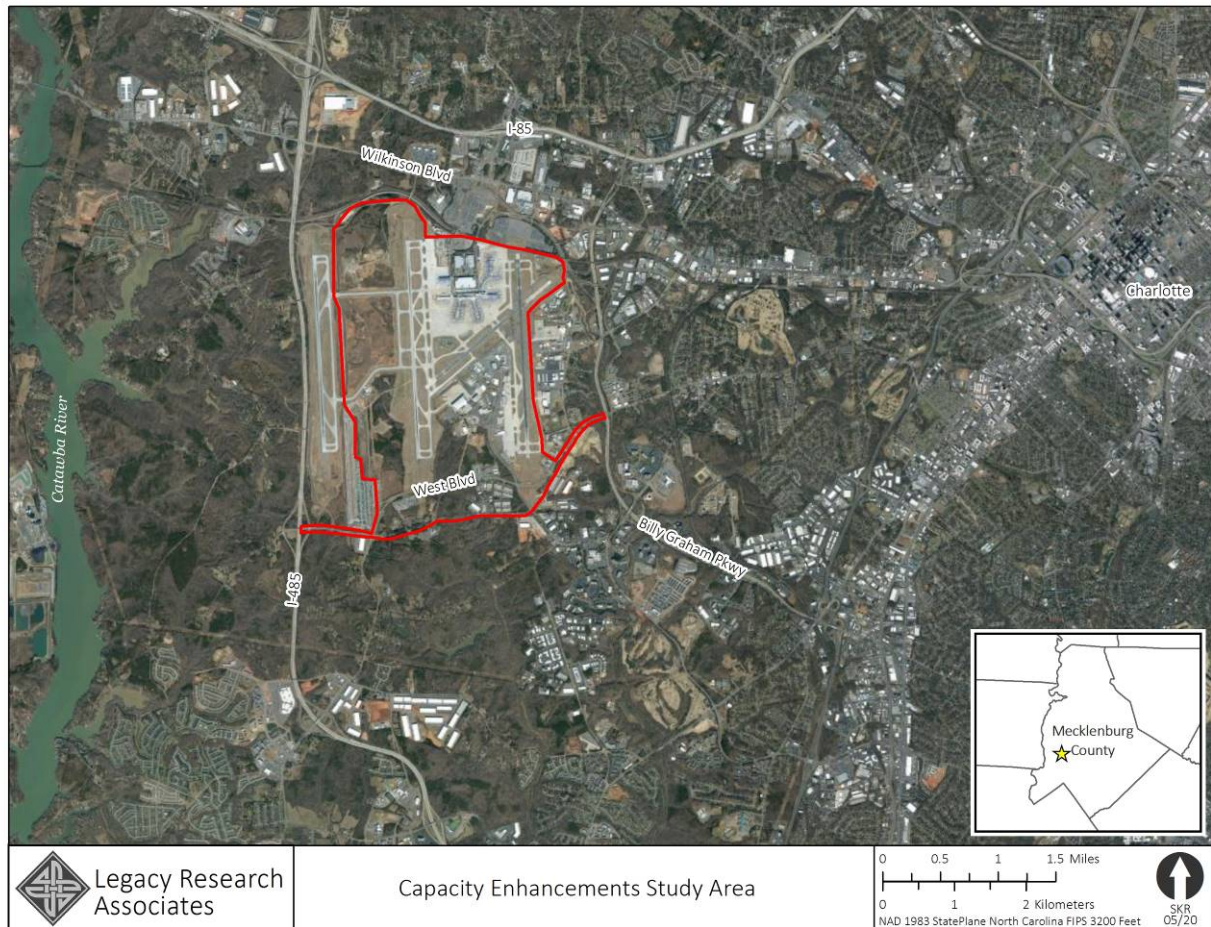


Figure 1. Capacity Enhancements Study Area. Basemap: 2019 Aerial Image, NC CGIA.

On 11 May 2018, the NCHPO determined that Project elements located on land developed for airport infrastructure were unlikely to adversely affect any significant archaeological resources. Only the West Boulevard Relocation element was determined to have the potential to adversely affect archaeologically unevaluated areas and previously recorded archaeological sites that had not been evaluated for eligibility to the National Register of Historic Places (National Register).

Three alignments were under consideration for the West Boulevard Relocation.

- Relocation Corridor A follows Byrum Drive from Steele Creek Road to Piney Top Drive and reconnects with existing West Boulevard east of the Runway Protection Zone (RPZ) for the proposed Fourth Parallel Runway.
- Relocation Corridor B follows Byrum Drive from Steele Creek Road to Sirius Lane, where a new roadway section will curve south and east across Whippoorwill Drive to connect with the Beam Road/Yorkmont Road intersection.
- Relocation Corridor C follows existing Byrum Drive from Steele Creek Road to Yorkmont Road and continues northeast on a new roadway section to reconnect with existing West Boulevard near Horseshoe Lane.

The three proposed relocation corridors, a 962-meter (m) segment of existing West Boulevard between Steele Creek Road and Interstate 485 (I-485), and a 950-m segment of existing West Boulevard between the east end of Corridor A and Yorkmont Road were included in the West Boulevard Relocation Study Area. The width of the study area for these alignments was defined as 30 m to either side of the proposed or existing centerlines. The study area covered approximately 116.57 acres.²

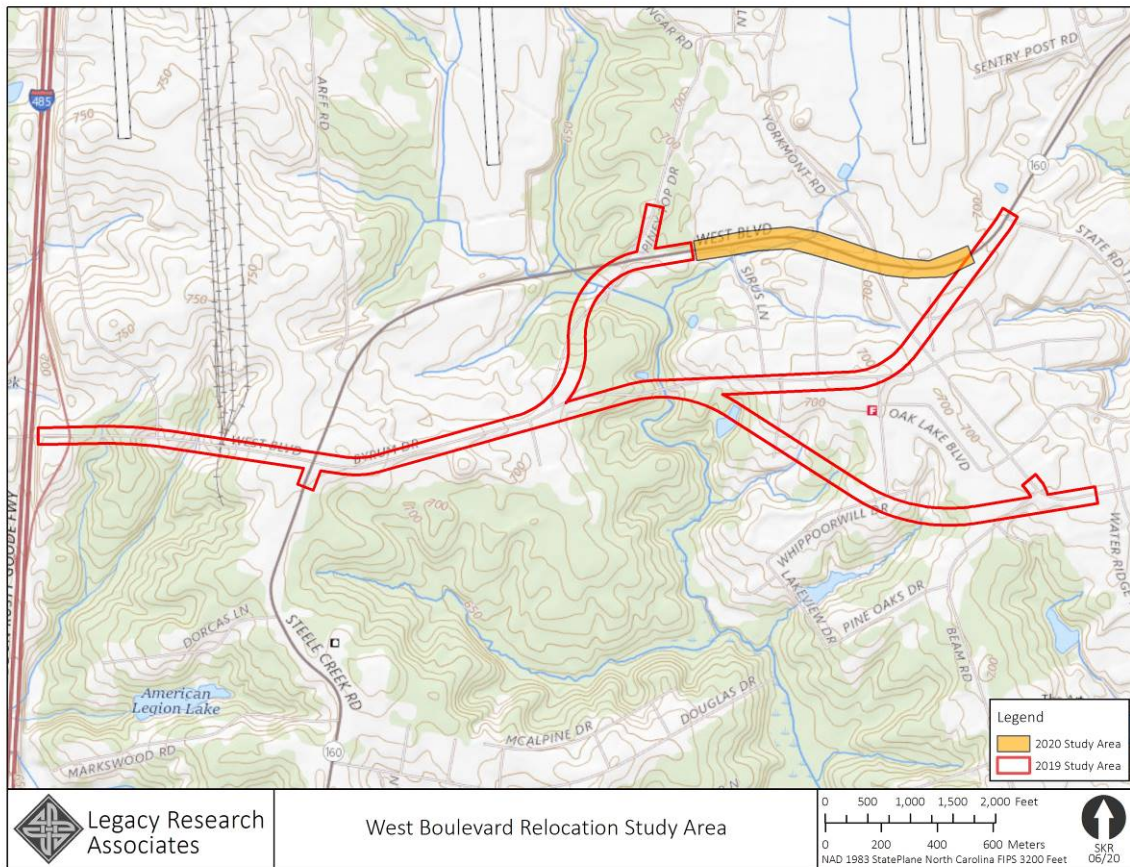


Figure 2. West Boulevard Relocation Study Area. Basemap: 2016 USGS *Charlotte West Quadrangle*, 1:24,000 scale.

² The selected alternate for the West Boulevard Relocation is defined as approximately a one-mile portion of West Boulevard that will be relocated to an existing road that lies in the footprint of the Runway Protection Zone (RPZ) of proposed Runway 01/19 and the south End-Around Taxiway (EAT). The relocated roadway will utilize existing Byrum Drive, Piney Top, and West Boulevard.

Archaeological field investigations of the study area were conducted by New South Associates Inc. (New South) in March 2019 and changes to the study area in 2020 were surveyed by Legacy's subconsultant, Environmental Services Inc. (ESI), in April 2020.³ The objective was to identify the general distribution, location, and nature of archaeological sites; evaluate them for eligibility to the National Register; and assess Project-related effects on significant sites.

The following report sections describe the Environmental Setting, Archaeological and Cultural Background, Methodology, West Boulevard Relocation Survey, Significance Evaluation and Recommendations, and References Cited.

ENVIRONMENTAL SETTING

The following description of the environmental setting relevant to the approximately 116.57-acre West Boulevard Relocation Study Area includes climate, topography, hydrology, geology, soils, vegetation, and fauna. The general setting of the study area has been extensively disturbed by developments for commercial activity. Evidence of disturbance includes borrow pits, catch basins, cut banks, natural gas lines, and fiber optic lines. These disturbances are attributed to road construction, industrial park construction, mid-1990s house demolition, and buried utilities.

Climate

The Mecklenburg County climate is moderate with hot and humid summers and moderately cold but short winters because mountains to the west protect the county against cold fronts. Precipitation is evenly distributed throughout the year, but generally falls between April and September, which is the growing season for many crops. Every few years in late summer to early autumn, tropical storms bring heavier rainfall. Average seasonal snowfall is six inches, although periodically heavy snow covers the ground for a few days to a week.

Topography

Mecklenburg County is in south-central North Carolina (Figure 3). It lies within the southern Piedmont region that is one of three geographic divisions in North Carolina and comprises almost half the land in the state (Figure 4). It begins along the southeastern slope of the Blue Ridge Mountains and extends eastward to the edge of the Coastal Plain. The region is characterized by broad, gently rolling interstream areas and by steep slopes along drainages. The natural terrain in the study area consists of a series of rolling ridges and steep-sided drainages that flow into Catawba River tributaries.

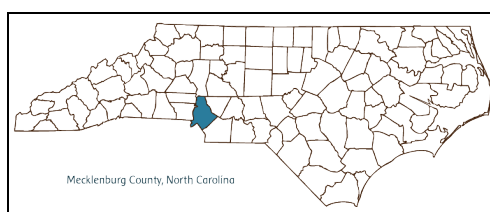


Figure 3. Location of Mecklenburg County in North Carolina (NCPEDIA 2020).

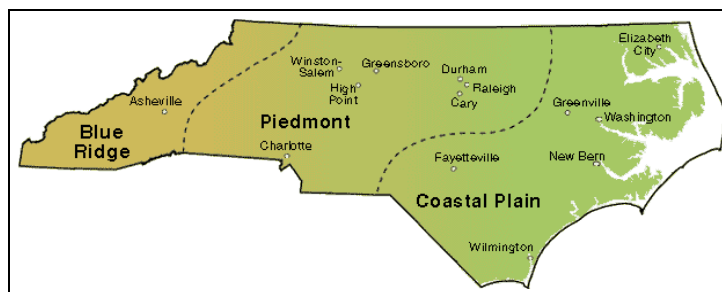


Figure 4. North Carolina Physiographic Regions (USGS South Atlantic Water Science Center - North Carolina Office 2020).

³ Information about the March 2019 New South archaeological field investigation throughout this report was extracted from two sources that include (1) the *Affected Environment* in the Final Charlotte-Douglas International Airport Environmental Impact Statement dated 22 May 2019 prepared by VHB Engineering with contributions from Environmental Science Associates, Hart and Hickman PC, KP Environmental Sciences, and New South and (2) NC Archaeological Site Forms for 31MK0228, 31MK0229, 31MK1446, and 31MK1147 prepared by New South.

Hydrology

In Mecklenburg County, water either flows to the southwest towards the Catawba River or it flows to the southeast towards the Yadkin River. The City of Charlotte and the Charlotte-Douglas International Airport are located on the east side of the Catawba River that falls within the Catawba River Drainage Basin (Figure 5). The Catawba River is the eighth largest river system in the state and contains 3,042 miles of streams (NC DENR 2020).



Figure 5. Catawba River Drainage Basin (NC DENR 2020).

The study area is drained by two permanent streams. The southern and southwestern portions are drained by Sugar Creek - Steele Creek Branch. And, the eastern and southeastern portions of the study area are drained by Sugar Creek - Coffey Creek Branch. Stream gradients are shallow to moderate. None of the permanent streams in the study area have floodplains.

The study area falls within two Catawba River watersheds (Figure 6). The area that lies east of Steele Creek Road is in the Sugar Creek Watershed. Coffey Creek, a tributary of Sugar Creek, flows generally north-south through the eastern third of the Project study area. The study area that lies west of Steele Creek Road is in the Beaverdam Watershed.

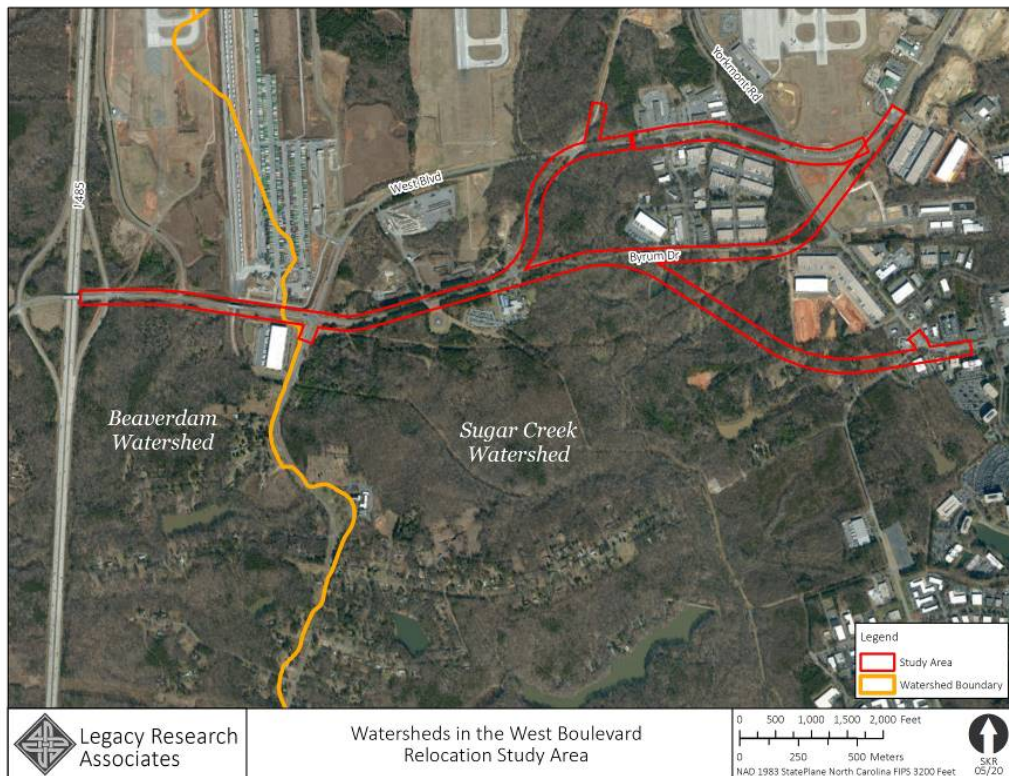


Figure 6. Watersheds in the Study Area. Basemap: 2019 Aerial Image, NC CGIA.

Geology

Mecklenburg County is in the Charlotte Belt (Figure 7). This geological region is composed of igneous and meta-igneous rock, diorite-gabbro, and granite. The oldest rocks are mafic gneiss, amphibolite, and metavolcanic. Gold-bearing rocks in the Charlotte Belt are found in lode and placer deposits. Lodes are narrow veins of variable length that trend to the northeast and placer deposits are found in streams, gravel beds, and inside bends of stream channels (Carpenter 1993). No known gold deposits are within the study area.

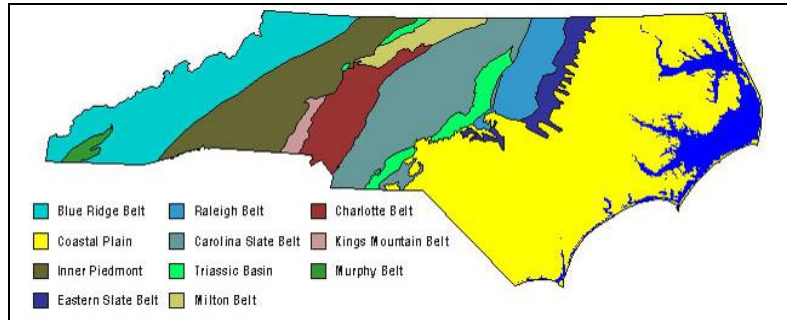


Figure 7. North Carolina Geological Regions (NCPEDIA 2020).

Soils

The USDA Natural Resources Conservation Service (NRCS) identified five soil types within the study area (Table 1 and Figure 8). Most of the West Boulevard Relocation Study Area can be classified as having a low probability for the presence of archaeological sites because of developed land, soil erosion, steep slopes, and wetlands.

Most of the study area, approximately 128.33 acres or 79.23 percent, is located on Cecil soil types coded CeB2 and CeD2. Cecil soils are moderately well-drained acidic soils found on gently to strongly sloping topography with a typical pedon consisting of a 15-cm-deep sandy clay loam plowzone overlying 86 cm of clay subsoil. These soils were formed from igneous and metamorphic strata. Both CeB2 and CeD2 soils are described as eroded sandy clay loam.

Soils classified as Helena (HeB) sandy loam with 2 to 8 percent slopes cover approximately 16.12 acres or 10 percent of the study area. This moderately well-drained sandy loam occurs on ridge summits. This soil typically contains a 20-cm-deep plowzone overlying a 10-cm sandy loam eluvial horizon and clay subsoil.

Pacolet sandy loam (PaE) covers about 13.48 acres or 8.32 percent of the study area. These soils occur on 15-25 percent slopes and are derived from weathered rock residuum. Pacolet soils are composed of a 15-cm-deep sandy loam plowzone overlying a clay subsoil.

Approximately 1.86 acres or 1.15 percent of the study area is identified as Monacan (MO) poorly drained soils along streams and drainageways. This frequently flooded soil develops on floodplains from loamy igneous and metamorphic alluvium. Monacan soils are composed of 16 cm of silty loam plowzone overlying weathered silty loam subsoil.

Urban (Ur) soils comprise 1.67 acres or 1.0 percent of the study area. It consists of areas where more than 85 percent of the surface is covered with asphalt, concrete, buildings, or other impervious cover. Most of the soil material has been cut, filled, and graded and the natural characteristics have been altered or destroyed. The remainder of the Urban land is small lawns or shrub gardens near buildings, sidewalks, and parking lots.

Stream impoundments (W) cover 0.51 acres or 0.3 percent of the study area.

Trimble (1974) described the North Carolina Piedmont as one of the "most severely eroded agricultural areas in the United States," stating that "much of the Piedmont has been stripped of the topsoil, and many areas have been dissected and gullied so badly as to render the land unsuitable for agriculture. The debris from this erosion has filled stream channels and valleys to varying degrees, often swamping the adjacent bottomlands." Trimble estimated that the average depth of soil loss in the North Carolina Piedmont was about 14 centimeters (cm).

Table 1. Soils within the Study Area.				
Type	USDA Code	Description	Acreage within the study area	Percent of study area
Cecil	CeB2	Sandy clay loam, 2-8 percent slopes, eroded	92.42	57.06
	CeD2	Sandy clay loam, 8-15 percent slopes, eroded	35.91	22.17
Helena	HeB	Sandy loam, 2-8 percent slopes	16.12	10.00
Pacolet	PaE	Sandy loam, 15-25 percent slopes	13.48	8.32
Monacon	MO	Poorly drained soil along streams and drainageways	1.86	1.15
Urban	Ur	Urban land	1.67	1.00
Water	W	Stream impoundment	0.51	0.30
Total			161.97	100

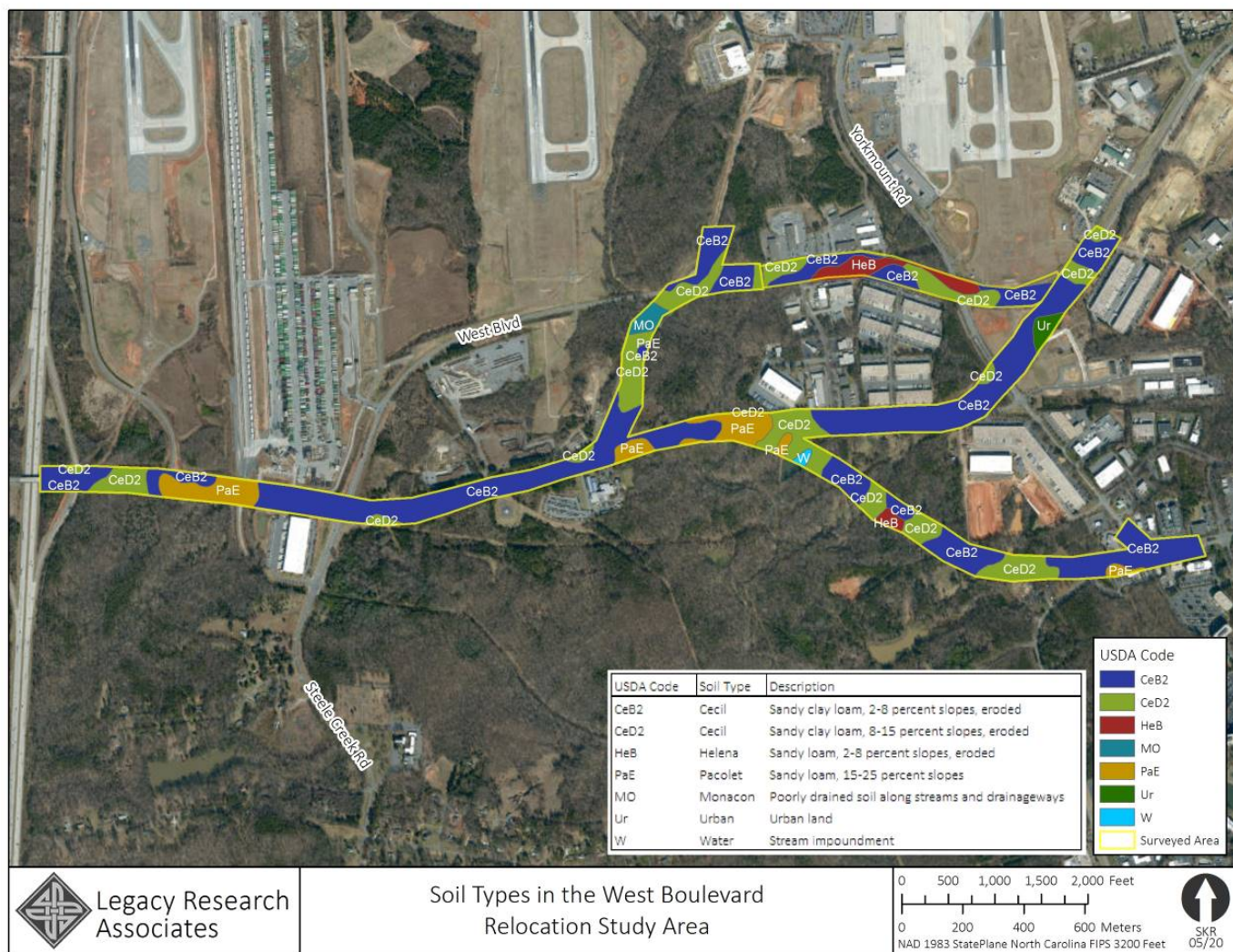


Figure 8. Soil Types in the Study Area. Basemap: 2019 Aerial Image, NC CGIA.

Vegetation

Mecklenburg County and the Charlotte-Douglas International Airport are within Bailey's (1995) *Southeast Mixed Forest Province* (Carolina Biotic Province). Native vegetation includes forests of medium to tall trees, most frequently represented by needleleaf evergreen trees. Common tree types include pines, red cedar, oak, hickory, sweet and black gums, red maple, and elm. The typical understory includes dogwood, haw, blueberry, Virginia creeper, and bluestems. Panic grass and longleaf uniola are the most common grass species.

Tolonen and Clifford (1998) cite an environmental survey of the proposed Charlotte-Douglas International Airport expansion area that was conducted 1996.⁴ According to Tolonen and Clifford, the 1996 study defined seven distinct vegetational assemblages within the Charlotte-Douglas International Airport property. These include: (1) Oak-Pine-Hickory Forest, (2) Bottomland Forest, (3) Old Field-Scrub/Shrub, (4) Urban-Industrial-Turf, (5) Disturbed-Unvegetated, (6) Wetlands, and (7) Open Water. A description of these areas from Tolonen and Clifford follows.

Oak-Pine-Hickory Forest - Typical species include white oak, blackjack oak, post oak, willow oak, red oak, and southern red oak. Tulip-tree, red maple, sweet gum, mockernut hickory, pignut hickory, sweet pignut, Virginia pine, and shortleaf pine are other dominant tree species. Less common species include hackberry, basswood, beech, black locust, white ash, and black walnut. The understory frequently has slippery elm, blueberry, silverberry, red cedar, strawberry bush, blackhaw, flowering dogwood, black cherry, red bud, and American holly. Woody vines are common and include wisteria, Japanese honeysuckle, moonseed, kudzu-vine, Carolina rose, wild grape, poison ivy, blackberry, greenbriar, and Virginia creeper. Frequent herbaceous species include bluegrass, fescue, ryegrass, and sedges. Forb species include wood sorrel, mayapple, violets, wild ginger, snakeroot, wild strawberry, spotted wintergreen, puttyroot, sweet cicely, hepatic, and bloodroot. Common woodland ferns are Christmas fern, grape fern, rattlesnake fern, and ebony spleenwort.

Bottomland Forest - This vegetational community is adjacent to intermittent streams, drainageways, and depressions. Common tree species are tulip-tree, red maple, green ash, sweet gum, sycamore, box elder, cottonwood, and black willow. The understory has river birch, ironwood, and smooth alder. The herbaceous layer includes sedges, bedstraw, spotted touch-me-not, snakeroot, golden ragwort, white avens, jack-in-the-pulpit, sensitive fern, stonecrop, and exotic grass.

Old Field-Scrub/Shrub - Areas of old field are typically represented by widely dispersed empress trees, Canada bluegrass, Kentucky bluegrass, timothy, red fescue, barnyard grass, green foxtail grass, and orchard grass. Common broadleaf herbaceous species include goldenrod, common milkweed, curly dock, ox-eye daisy, wild carrot, poke, bush cover, tick-trefoil, mugwort, small white aster, blackberry, and ragweed. Old Field communities of the Piedmont display various degrees of succession. Clay et al. (1975) state that crabgrass was dominant in harvested agricultural fields. After abandonment, horseweed was dominant, but quickly supplanted by white-topped aster. With age, abandoned agricultural fields see an increase in the presence of broomsedge, pine (including loblolly, shortleaf, and virginia), and scattered hardwoods until an oak-hickory forest becomes established.

Urban-Industrial-Turf - This vegetational community is represented by grassy areas that undergo regular mowing. Dominant species are perennial ryegrass, redtop, red fescue, Kentucky bluegrass, and annual bluegrass. Also present are dandelion, lyre-leaf sage, common plantain, lance-leaf plantain, and white clover.

Disturbed-Unvegetated - A large percentage of the airport property is covered with impervious surfaces, such as concrete and asphalt paving.

Wetlands - Wetland area vegetation includes common woody species like alder, cottonwood, red maple, box elder, sweet gum, sycamore, and willow. Other wetland communities are represented by scrub/shrub and emergent vegetation. These areas possess similar woody species as well as emergent species such as cattail, soft rush, green bulrush, water plantain, and arrowleaf.

Open Water - Common fringe wetland vegetation includes alder, cottonwood, sycamore, and willow. Scrub/shrub areas have swamp rose, multiflora rose, greenbriar, and blackberry. Common herbaceous emergent plants include cattail, soft rush, green bulrush, water plantain, and arrowleaf. Floating aquatic plant species include pondweed and duckweed.

Vegetation in the study area includes landscaped lawns, hardwood forest, and small stands of young pine trees.

⁴ No reference to this study is cited in the Tolonen and Clifford 1998 report references.

Fauna

Also described in Tolonen and Clifford (1998) are the regional fauna. They reported that fauna in the region varies with the age and stocking of timber stands, the percent of deciduous trees, proximity to openings, and the presence of bottomland forests. Whitetail deer and cottontail rabbits are widespread. When deciduous trees are present, the fox squirrel is common. Gray squirrels live along intersecting drainages. Raccoon and fox inhabit the entire region and are hunted in many areas. The eastern wild turkey, bobwhite, and mourning dove are common. Bird species in mature forests are the pine warbler, cardinal, summer tanager, Carolina wren, ruby-throated hummingbird, blue jay, hooded warbler, eastern towhee, and tufted titmouse. Forest snakes include cottonmouth moccasin, copperhead, rough green snake, rat snake, coachwhip, and speckled kingsnake. A recent report documents coyotes in Mecklenburg County. Coyotes are not native to North Carolina but have been seen since 1995 following the extirpation of gray wolves and red wolves that were native in the eastern United States (Mecklenburg County, Parks and Recreation, Natural Resources, Wildlife 2020).

ARCHAEOLOGICAL AND CULTURAL BACKGROUND

This section of the report provides a general overview of the prehistory and history relevant to the study area, a summary of previous archaeological research in the study area, and the expected archaeological potential for the study area.

Prehistoric and Historic Cultural Context

Prehistoric Cultural Context

The chronological sequence for the prehistory of the North Carolina Piedmont region is presented in Table 2. It provides a synopsis of the regional cultural stages beginning with the earliest occupation, known as the Paleo-Indian period, and ending with the Woodland period that lasted until the time of European Contact with indigenous populations around AD 1600.

Table 2. Prehistoric Cultural Sequence for the North Carolina Piedmont Region.			
Date	Cultural Period	Sub-Period/Era	Regional Phase
500 BC - AD 1600	Woodland	Late (AD 1000-European Contact)	Uwharrie and Dan River
		Middle (AD 800-1000)	Yadkin and Uwharrie
		Early (500 BC-AD 800)	Badin and Yadkin
8000-500 BC	Archaic	Late (2500-500 BC)	Gypsy and Savannah River
		Middle (6000-2500 BC)	Stanly, Halifax, Morrow Mountain, and Guilford
		Early (8000-6000 BC)	Hardaway-Dalton, Palmer, Big Sandy, Rowan, Kirk, and MacCorkle-St. Albans-LeCroy
10,000-8000 BC	Paleo-Indian		Clovis, Cumberland, Suwannee, Simpson, Dalton, Hardaway, and Hardaway-Dalton

Paleo-Indian Period

The earliest documented occupation of the North Carolina Piedmont region occurred at the end of the last glacial advance and is referred to as the "Paleo-Indian" period (10,000-8000 BC). Paleo-Indian culture consisted of small nomadic bands that subsisted by hunting and gathering (Purrinton 1983). Fluted and unfluted projectile points/knives (PPKs) from Paleo-Indian sites suggest that early inhabitants hunted bear, deer, elk, and possibly caribou (Chapman 1977) and smaller game (Anderson et al. 1992).

Among the diagnostic Paleo-Indian lithics, fluted Clovis types are the earliest, followed by fluted Cumberland, Suwannee, and Simpson PPKs. Dalton, Hardaway, and Hardaway-Dalton types generally date to the end of the Paleo-Indian period. Evidence of Paleo-Indian occupation in North Carolina is mainly represented by isolated PPK surface finds.

Paleo-Indian settlements were likely small and briefly occupied. Paleo-Indian lithic artifacts have been recovered from high elevations suggesting hunting in a tundra habitat as well as on ridge tops and river terraces. Lithic tools were often manufactured from high-grade material. Researchers suggest that the roaming range of groups was influenced by known primary stone sources (Anderson et al. 1992; Goodyear et al. 1989).

One important archaeological site (Hardaway) in North Carolina that has a Paleo-Indian component is located along the Yadkin River in Stanly County about 60 miles east of Charlotte. Hardaway PPKs represent the earliest occupation at this site. This site is significant because the archaeological investigations identified stratified deposits that formed the basis of establishing the Paleoindian and Early Archaic cultural sequences for the Carolina Piedmont region (Coe 1964).

Archaic Period

Environmental warming at the end of the Ice Age resulted in a cultural adaptation referred to as the “Archaic” period (8000-500 BC). Diagnostic artifacts associated with exploiting temperate forest resources characterize this transition (Chapman 1977; Coe 1964; Purrington 1983). The earliest of these is the Hardaway-Dalton PPK, which dates to the late Paleo-Indian/Early Archaic period. Archaic sites are typically found on upland hills, in floodplains, and on the banks of small streams (Bass 1977). The Archaic period has three divisions – Early, Middle, and Late – defined primarily by changes in stemmed PPK styles with side or corner notching and/or bifurcate bases.

During the Early Archaic period (8000-6000 BC) hunting was the dominant subsistence activity; it was supplemented by fishing aquatic resources, gathering plant foods, and trapping small game. Settlements consisted of base camps on alluvial terraces with smaller hunting camps in the uplands (Chapman 1977). Early Archaic diagnostic PPK types include Palmer, Big Sandy, Rowan, Kirk (corner-notched and stemmed), and MacCorkle-St. Albans-LeCroy; the latter is associated with the transition from the Early to Middle Archaic period.

Settlement patterns changed from the alluvial terraces and uplands to the rivers during the Middle Archaic period (6000-2500 BC). This change is believed to be the result of exploitation of plant foods and aquatic resources (Frankenberg and Herrmann 2000). During this time, populations increased and smaller band territories emerged. Middle Archaic sites are found in various settings but tend to be similar in size and artifact diversity. These attributes suggest that groups were highly mobile. Local lithic materials (quartz, quartzite, and metavolcanic) were used with greater frequency than found in Early Archaic archaeological sites. Stanly, Halifax, Morrow Mountain, and Guilford PPK styles, as well as an increase in groundstone tool production, such as atlatl weights, axes, and pestles occurred during this time.

Late Archaic period (2500-500 BC) sites have evidence of plant cultivation that included squash, goosefoot, knotweed, little barley, maygrass, sumpweed, and sunflower. Ethnobotanical studies indicate that three of these were domesticated during the Late Archaic period: sunflower by 2000 BC, goosefoot by 1500-1000 BC, and sumpweed between 1000 and 500 BC (Scarry 1994).

During the Late Archaic period, groups became more sedentary, which led to the development of regional social, technological, and economic specializations. The manufacture of soapstone vessels, grooved stone axes, ornaments, and copper items occurred during this time. Diagnostic lithic artifacts that mark the Late Archaic period include broad and square-stemmed Savannah River and small-stemmed Gypsy PPKs.

Due to the semi-sedentary nature of people during the Archaic period, archaeological evidence of land use in the Carolina Piedmont region is represented by temporary camps that were usually established near rivers or other water resources and tended to be in upland areas. People moved between the two locations, according to seasons and available resources, where they engaged in hunting and gathering activities.

Woodland Period

The development of ceramics and the bow and arrow are markers of the beginning of the Woodland period (500 BC-European Contact). A shift towards permanent settlement during this time is reflected in population growth and dependence on horticulture. Maize was introduced during the Early and Middle Woodland periods, although undomesticated plants continued to be major food sources into the Late Woodland period (Scarry 1994).

Early evidence of the Woodland period in the North Carolina Piedmont is the Badin culture that has a material cultural characterized by hard-paste, sandy ceramic artifacts and large triangular PPKs. Early Woodland (500 BC-AD 800) sites are often found in the fertile bottomlands, but they are also documented in the uplands. Subsistence and economic patterns that developed during the Late Archaic period continued into the Early Woodland period. Although the data is sparse, there is good evidence that settlements moved to the bottomlands during the succeeding phases (Davis 1990).

Ceramic styles shifted from Badin series to Yadkin and Uwharrie series during the Middle Woodland period (AD 800-1000). Both have similar surface treatments and feature coarse-sand or crushed quartz temper. Dan River ceramics with crushed quartz and coarse-sand temper appear in the Late Woodland period.

During the Late Woodland period, people from present-day South Carolina or northern Georgia migrated into the present-day Charlotte region bringing a more complex cultural tradition of the southeastern Mississippian cultures (Ward and Davis 1999). Their ceramics were made with well-mixed, fine-sand-tempered clay that was smoothed and burnished and had complicated stamped surface treatments.

Late Prehistoric/Protohistoric Period

The Late Prehistoric/Protohistoric period is noted for complex societies organized in chiefdoms, platform mounds associated with a public plaza, and houses constructed of wattle and daub. Cleared field agriculture with maize as the dominant crop was an important subsistence activity. Changes in subsistence were accompanied by changes in settlement patterns. People selected living areas that could yield abundant and predictable resources (Scarry 1994:21).

Approximately 5,000 Catawba were living in the region prior to the arrival of Spanish explorers (Lee 1963). During the mid-sixteenth century, Hernando de Soto and Juan Pardo identified many Catawba settlements along the Catawba and Wateree Rivers. These include: Joara, Guaquiri, Quinahaqui, Yssa, Otari, Aracuchi, Tagaya, Tagaya the Lesser, and Cofitachequi (Moore 2002).

Joara has been recorded as archaeological site 31BK22, also known as the “Berry Site,” which is located about 80 miles northwest of Charlotte. Pardo came to Joara in 1567 and established Fort San Juan at the settlement (Hudson 1990; Moore 2002). He stationed approximately 30 men at Fort San Juan hoping to expand the Spanish territory from the Santa Elena colony on present-day Parris Island, South Carolina. Excavations at the site recovered many Spanish artifacts that include olive jars, majolica, glass beads, nails, and other metal objects like buttons and knife fragments (Moore 2002).

Cofitachequi in Kershaw County, South Carolina, is recorded as archaeological site 38KE12. It is also known as the “Mulberry Site” which is about 60 miles south of Charlotte. This site consists of large earthen mounds and an associated village that served as the capital of Cofitachequi. It was occupied between AD 1100 and 1700 and was visited by de Soto in 1540 and Pardo in 1566. Cofitachequi was nearly abandoned around 1670. In 1701, Lawson reported only seeing a few scatter homes.

John Lawson’s 1709 account of native settlements around the present-day North and South Carolina border identified several Catawba tribes: Esaw, Sugaree, and Kadapu (Lawson 1709, Moore 2002). They lived near Sugar Creek and the Catawba River, between present-day Charlotte, North Carolina, and Rock Hill, South Carolina. The Catawba Nation was noted by Lawson for its large population of about 1,200 people (Rights 1957). The Nation formed in 1716 after the Yemassee War from several groups (Cheraw, Congaree, Wateree, and Waxhaw) that were once part of the Cofitachequi confederation (Hicks 1998).

A 1715 census of British Colonial Province of South Carolina identified only seven Catawba villages, suggesting that the Upper Catawba Valley had been largely depopulated by that time (Moore 2002). Additionally, maps dating to this time show no major Native American settlements east of the Blue Ridge Mountains. In 1763, the Catawba were granted a reservation covering 15 square miles along both sides of the Catawba River in York and Lancaster counties, SC (Rights 1957).

Archaeological investigations by the University of North Carolina-Chapel Hill Research Laboratories of Archaeology (UNC-C RLA) located two Catawba villages depicted on a 1772 map of the Catawba Nation. These villages are located along the Catawba River in present-day York County, SC, that lies about 30 miles south of Charlotte. These settlements are identified as “Old Town” and “New Town.” Old Town was occupied before the American Revolution and New Town between 1800 and 1820. In 1826, some of this land was leased to nearby white settlers. By this time, the Catawba population had dwindled to 110. The UNC-C RLA work analyzed documentary sources and Catawba settlement patterns and found that Catawba settlements were occupied sequentially between 1750 and 1820 and gave rise to the modern Catawba Nation. Their work also provided insight into the transformation of Catawba culture during the late Colonial (1750-1780) and Federal (1780-1830) periods.

Historic Cultural Context

Early European-American settlement in the region is documented by the land acquisition in the mid-1700s. In 1749, John Beatty acquired approximately 944 acres on the west bank of the Catawba River (Brotherton 1993). Soon afterwards, Charles, James, and Henry Connor obtained land on the eastern side of the river across from Beatty. Together, they established Beattie’s Ford where an island and flat rocks made crossing the Catawba River manageable by pedestrians, horses, and wagons.

Beattie’s Ford played an important role in the transportation and migration of many Anglo-American settlers in the region, as it had done for centuries for native people who used the natural ford as part of a trading path. During the mid-nineteenth century, it served as a stagecoach station and was home to Hutchins G. Burton, who served as governor of North Carolina from 1824 to 1827 (Brotherton 1993). Early maps show that seven roads converged at Beattie’s Ford. Use of the ford declined during the late-nineteenth and early-twentieth centuries as railroads, automobiles, and bridges rendered Beattie’s Ford obsolete.

Mecklenburg County was established in 1762 from a part of Anson County and included, at that time, parts of what are now Cabarrus, Union, Lincoln, Rutherford, Cleveland, and Gaston counties (Corbitt 1987). The county, in its present boundaries, was established in 1842. Early settlers chose the name “Mecklenburg” after Queen Charlotte, queen consort to England’s King George III, who was born in Mecklenburg province in Germany. The City of Charlotte, incorporated on November 7, 1768, was also named for Queen Charlotte.

In February 1781, a Revolutionary War battle between General Cornwallis and General William Lee Davidson occurred at Cowan’s Ford on the Catawba River in northwestern Mecklenburg County. General Davidson succeeded in slowing Cornwallis’ advance; however, he was killed during the battle.

Following the Revolutionary War in 1799, gold was discovered near Concord in Cabarrus County, then a part of Mecklenburg County (Carpenter 1993). The quantity of rich ore in the region led to the establishment of a United States Treasury mint in Charlotte in 1836. Charlotte remained the gold-mining capital of the United States until the discovery of gold at Sutter’s Mill in California in 1848.

By 1900, Mecklenburg County had a population of more than 55,000; by 1950, the population tripled and by the 1970s more than doubled. In 2019, the population reached 1.1 million making Mecklenburg County the most populated county in North Carolina. Mecklenburg County also has the status of having one the nation’s top 10 airports - Charlotte-Douglas International Airport that was established in 1935 as “Charlotte Municipal Airport.” In 1954, a 70,000-ft² passenger terminal opened, and the airport was renamed “Douglas Municipal Airport” after former Charlotte mayor Ben Elbert Douglas, Sr. The airport gained its current name in 1982.

Previous Archaeological Research

A review of site files at the NC OSA found that 55 archaeological sites are recorded within a mile of the West Boulevard Relocation Study Area (Figure 9 and Table 3). The 55 sites were recorded between 1978 and 2020 during eight investigations (Mathis 1978; Fischer 1978; McLean and Sellon 1978; Hargrove 1990; Tolonen and Clifford 1998; Robinson 2008; and Joy 2018 and 2020).

Table 3. Inventory of Previously Recorded Archaeological Sites.			
Site Number(s)	Recorded Sites	Revisited Sites	Recorder Year
31MK0095-31MK0097	3		Mathis 1978
31MK0228-31MK0230	3		Fischer 1978
31MK0228-31MK0230		3	McLean and Sellon 1978
31MK0095		1	Hargrove 1990
31MK0548-31MK0553	6		
31MK0228 and 31MK0548		2	
31MK0815, 31MK0816, 31MK0818, 31MK0819, 31MK0822, 31MK0824-31MK0836, 31MK0842, 31MK0844, 31MK0845, 31MK0850, and 31MK0852-31MK0859	30		Tolonen and Clifford 1998
31MK1086-31MK1088	3		Robinson 2008
31MK1143-31MK1145	3		Joy 2018
31MK1148-31MK1154	7		Joy 2020

Table 4 summarizes the 55 previously recorded sites within a mile of the West Boulevard Relocation Study Area. Sites that are within the West Boulevard Relocation Study Area are identified in column 1 by gray shading (#4, #5, #7, and #37-#42).

Most (n=49 or 89 percent) of the 55 sites were found to lack integrity and were recommended as being not eligible for the National Register. Six (11 percent) of the 55 sites were unassessed for National Register eligibility. On 11 May 2018, the NCHPO determined that it was unlikely that unassessed sites on developed land within the airport would have retained integrity and recommended no additional work. Only one of the six unassessed sites (31MK1088) lies outside the developed land within the airport and outside the West Boulevard Relocation Study Area.

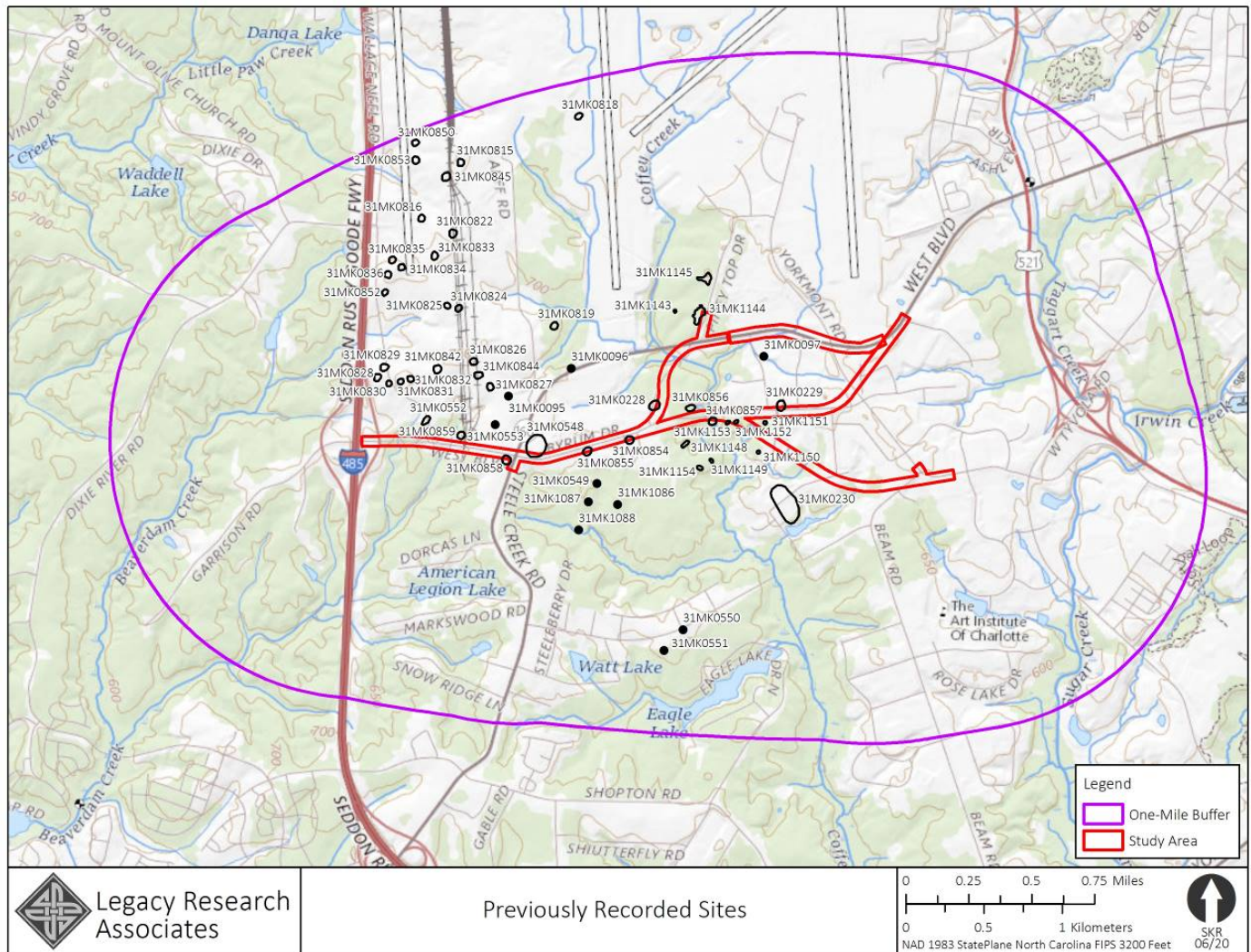


Figure 9. Previously Recorded Sites within a mile of the Study Area. Basemap: 2016 USGS Charlotte West Quadrangle, 1:24,000 scale.

#	Site Number	Component(s)	Description	Assessment	National Register Eligibility Recommendation
1	31MK0095	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
		Historic	Ceramic Scatter - 19th to 20th century		
2	31MK0096	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
3	31MK0097	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
4	31MK0228	Prehistoric	Lithic Scatter – Early to Late Archaic	Unassessed	Unassessed
5	31MK0229	Prehistoric	Lithic Scatter – Early Archaic	Unassessed	Unassessed
6	31MK0230	Prehistoric	Lithic Scatter	Unassessed	Unassessed
7	31MK0548	Prehistoric	Lithic Scatter – Early to Late Archaic	Lack of Integrity	Not Eligible
		Historic	Domestic Artifact Scatter – 20th century		
8	31MK0549	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
		Historic	Domestic Artifact Scatter – 18th to 19th century	Lack of Integrity	Not Eligible
9	31MK0550	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
10	31MK0551	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
11	31MK0552	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible

Table 4. Summary of Previously Recorded Sites.

#	Site Number	Component(s)	Description	Assessment	National Register Eligibility Recommendation
12	31MK0553	Historic	Domestic Artifact Scatter – 18th to 19th century	Unassessed	Unassessed
13	31MK0815	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
14	31MK0816	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
		Historic	Domestic Artifact Scatter		
15	31MK0818	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
16	31MK0819	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
17	31MK0822	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
18	31MK0824	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
19	31MK0825	Prehistoric	Lithic Scatter – Late Woodland	Lack of Integrity	Not Eligible
20	31MK0826	Prehistoric	Lithic Scatter – Middle Woodland	Lack of Integrity	Not Eligible
		Historic	Ceramic Scatter – 19th to 20th century		
21	31MK0827	Prehistoric	Lithic Scatter – Late Archaic to Middle Woodland	Lack of Integrity	Not Eligible
22	31MK0828	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
23	31MK0829	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
24	31MK0830	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
25	31MK0831	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
26	31MK0832	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
27	31MK0833	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
28	31MK0834	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
29	31MK0835	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
30	31MK0836	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
31	31MK0842	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
32	31MK0844	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
		Historic	Ceramic Scatter – 19th to 20th century		
33	31MK0845	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
34	31MK0850	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
35	31MK0852	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
36	31MK0853	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
37	31MK0854	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
38	31MK0855	Prehistoric	Lithic Scatter – Early Archaic	Lack of Integrity	Not Eligible
39	31MK0856	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
40	31MK0857	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
41	31MK0858	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
42	31MK0859	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
43	31MK1086	Historic	Former House Site – late 19th century	Lack of Integrity	Not Eligible
44	31MK1087	Historic	Former House Site – 20th century	Lack of Integrity	Not Eligible
45	31MK1088	Historic	Possible Brick Manufacturing – 19th century	Unassessed	Unassessed
46	31MK1143	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
47	31MK1144	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
		Historic	Former House Site – 20th century		
48	31MK1145	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
49	31MK1148	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
50	31MK1149	Prehistoric	Lithic Scatter	Lack of Integrity	Not Eligible
51	31MK1150	Prehistoric	Lithic Scatter – Middle Woodland	Lack of Integrity	Not Eligible
52	31MK1151	Historic	Domestic and Architectural Artifact Scatter	Lack of Integrity	Not Eligible
53	31MK1152	Historic	Domestic and Architectural Artifact Scatter	Lack of Integrity	Not Eligible
54	31MK1153	Historic	Architectural Artifact Scatter	Lack of Integrity	Not Eligible
55	31MK1154	Historic	Extant abandoned house with no associated subsurface cultural material	Lack of Integrity	Not Eligible

Most (n=47 or 85 percent) of the 55 sites have prehistoric lithic scatter components.⁵ Of these, 44 (94 percent) were found to lack integrity and were not eligible for the National Register; the other three (6 percent) were unassessed for National Register eligibility. A review of the site locations on current aerial imagery shows that the locations for two (31MK0228 and 31MK0229) of the three unassessed lithic scatters have not been destroyed by past infrastructure construction activities at the Charlotte-

⁵ Of the 47 sites with prehistoric lithic scatter components, seven have evidence of historic occupation and/or land use.

Douglas International Airport. Note: 31MK0228 was assessed as being ineligible for the National Register by Tolonen and Clifford in 1998; however, in 2018 the NCHPO identified 31MK0228 as being unassessed for the National Register. For this study, 31MK0228 will be identified as being unassessed for the National Register.

Fifteen (27 percent) of the 55 sites are identified as having evidence of historic occupation and/or land use.⁶ Thirteen of the 15 historic sites have been assessed as being not eligible for the National Register; the other two (31MK0553 and 31MK1088) are unassessed. Site 31MK0553 has been destroyed by infrastructure construction; Site 31MK1088 is in an area planned for future land-use and is currently being assessed for National Register eligibility in a concurrent investigation for two proposed borrow areas.

A summary of the 55 sites follows. Information was been extracted from technical reports that date from 1978 to 2020 on file at the NC OSA. These reports provide survey outcomes resulting from varying methodologies and reporting standards.

Survey of the Proposed Relocation of NC 160 south of the Charlotte-Douglas Airport (Mathis 1978)

In 1978, a survey of the proposed right-of-way for NC 160 south of the Charlotte-Douglas Airport recorded three prehistoric lithic scatters (31MK0095, 31MK0096, and 31MK0097). These sites were determined to be extensively disturbed and ineligible for the National Register.

31MK0095 is a prehistoric lithic scatter of quartz flakes.⁷

31MK0096 is a prehistoric lithic scatter of quartz flakes and a biface.

31MK0097 is a prehistoric lithic scatter of quartz flakes.

Survey of the Coffey Creek Drainage (Fischer 1978)

An archaeological survey of the Coffey Creek drainage was conducted by Fischer in 1978. The report discusses three sites (31MK0228, 31MK0229, and 31MK0230) that were not evaluated for the National Register. Sites 31MK0228 and 31MK0229 are within the West Boulevard Relocation Study Area.

31MK0228 contained Early, Middle, and Late Archaic projectile point/knives (PPKs) identified as Palmer, Guilford, and Savannah River.

31MK0229 yielded an Early Archaic Kirk PPK.

31MK0230 is a prehistoric lithic scatter with no diagnostic artifacts.

Documentary Research of the Sugar Creek Basin (McLean and Sellon 1978)

In 1978, a survey of the Sugar Creek Basin revisited the three sites described above (31MK0228, 31MK0229, and 31MK0230). These sites were not evaluated for the National Register by McLean and Sellon in 1978. Sites 31MK0228 and 31MK0229 are within the West Boulevard Relocation Study Area.

31MK0228 Revisit found that the site was under cultivation.

31MK0229 Revisit found that the site had been destroyed by construction.

31MK0230 Revisit found that its significance was unknown.

Reconnaissance Survey of the Proposed Charlotte Outer Loop from I-77 to NC 27 (Hargrove 1990)

In 1989, Archaeological Research Consultants (ARC) conducted a reconnaissance survey of proposed corridors for a 13-mile section of the Charlotte Outer Loop (Western Section) from I-77 to NC 27. The survey recorded 25 archaeological sites and revisited previously recorded site 31MK0095.

Most of the sites are prehistoric lithic scatters with a few diagnostic artifacts that date to the Early, Middle, and Late Archaic periods. Five are historic (two gold mines, one eighteenth-century house site, an antebellum plantation, and a late-nineteenth to early-twentieth-century house site). Only one site (31MK0553) was recommended for additional work; a review of aerial imagery in 2020 found that this location has been destroyed by construction associated with airport infrastructure.

Of the 25 sites recorded or revisited by ARC in 1989, one (31MK0549) is within the West Boulevard Relocation Study Area Register and seven (31MK0095 and 31MK0548-31MK0553) are within a mile of the West Boulevard Relocation Study Area.

⁶ Of the 16 sites with evidence of historic occupation and/or land use, seven have prehistoric lithic scatter components.

⁷ 31MK0095 is revisited in 1989 and recommended as being not eligible for the National Register (Hargrove 1990).

- 31MK0095 Revisit was in a fallow field partly covered with clover and wildflowers. The site was situated on a small upland knoll overlooking Coffey Creek at the headwaters of Steele Creek. The site was recorded by Mathis in 1978 as a prehistoric lithic scatter. The 1989 revisit identified an historic component (Hargrove 1990). Hargrove's artifact recovery included prehistoric lithic artifacts (a biface, secondary flakes, and thinning flakes) and historic ceramic artifacts (salt-glazed stoneware and blue feather-edged whiteware). It was determined to have been extensively disturbed by agricultural activities and sheet erosion and was recommended as being not eligible for the National Register.
- 31MK0548 yielded a prehistoric Early Archaic Palmer PPK, the base of a possible Middle Archaic Stanly PPK, and two Late Archaic Savannah River PPKs. The collection from this site also included secondary flakes, biface thinning flakes, and a possible hammerstone. All artifacts were recovered from a surface inspection of a recently plowed field with 100-percent surface visibility. The site was determined to have been extensively disturbed by agricultural activities, erosion, and possible road construction and was recommended as being not eligible for the National Register.
- 31MK0549 is a multi-component prehistoric and historic site on a broad upland ridge. The prehistoric component was identified by the recovery of biface thinning flakes and a secondary flake. The historic component was represented by the presence of feather-edged pearlware, hand-painted pearlware, green lead-glazed earthenware, black transfer-printed ironstone willowware, undecorated ironstone, and a small brick fragment. The historic artifacts suggested a late-eighteenth- to early-nineteenth-century homesite. All artifacts were recovered from a surface inspection of a recently bulldozed field with 100-percent surface visibility. The site location is within the West Boulevard Relocation Study Area; however, it was determined to have been destroyed by bulldozing activities and was recommended as being not eligible for the National Register.
- 31MK0550 is a prehistoric lithic scatter of biface thinning flakes, secondary flakes, and a thinning flake. The site is situated on a small knoll on a ridgetop between tributaries of Coffey Creek. All artifacts were recovered from a surface inspection of a recently bulldozed field with 100-percent surface visibility. The site was determined to have been destroyed by bulldozing activities and was recommended as being not eligible for the National Register.
- 31MK0551 is a prehistoric lithic scatter of quartz flakes on a ridgetop. All artifacts were recovered from a surface inspection of a recently cleared lot with 70-percent surface visibility. The site was determined to have been destroyed by land-clearing activities and was recommended as being not eligible for the National Register.
- 31MK0552 is a prehistoric lithic scatter of secondary and thinning flakes on a ridgetop overlooking the headwaters of Beaverdam Creek. All artifacts were recovered from a surface inspection of a fallow field with 80- to 100-percent surface visibility. The site was determined to have been destroyed by plowing and soil erosion and was recommended as being not eligible for the National Register.
- 31MK0553 is an historic house site (known as the "Brown House" and "Bloomingdale Farm") that dates to the second half of the eighteenth century. The house and kitchen were leveled by a tornado in 1929, and the house foundations and chimney stones had been salvaged. The site was recommended for further work because it appeared to have the potential to yield information about eighteenth- and nineteenth-century farm life in Mecklenburg County, especially when combined with the documents and oral history of the Brown family, whose Scotch-Irish settlers built the house soon after arriving in the Steele Creek area. The recommendation was for test excavations to determine site preservation and National Register significance.

Survey of the Proposed Charlotte-Douglas International Airport Expansion (Tolonen and Clifford 1998)

Between 1995 and 1997 an archaeological survey of 3,000 acres for the proposed expansion of the airport was conducted by Anthony Tolonen with KEMRON Environmental Services in Atlanta, Georgia, and Laura Clifford with Environment and Archaeology LLC in Verona, Kentucky. Their work identified 49 archaeological sites (31MK0811-31MK0859) and revisited two previously recorded sites (31MK0228 and 31MK0548). Thirty-two of the 51 sites recorded or revisited by Tolonen and Clifford are within a mile of the West Boulevard Study Area and are described below.

Tolonen and Clifford describe the results of their work as follows:

The cultural resources identified during the Phase I survey were typically small, special activity loci, short-term occupation loci that lacked culturally or temporally diagnostic artifact types, or small scatters of historic debris. Two large prehistoric cultural resources were identified: the Wynn Site (31MK0811) and the Ertel Site (31MK0814). These sites possessed large numbers of debitage, flake tools, chipped stone varieties including diagnostic chipped stone types, and fire-cracked rock. The assemblage analyses of these two sites suggests that the sites formed as metachronous aggregates representing short-term reuse of the site areas throughout prehistory. They did not appear to represent village locations. However, additional work at these sites is recommended to address their potential eligibility for inclusion in the National Register of Historic Places. The remaining sites have been recommended as ineligible for the National Register and no further work is proposed.

A summary of the 32 sites within a mile of the West Boulevard Study Area follows.⁸

- 31MK0228 Revisit found four quartzite flakes at a site recorded by Fisher in 1978. The artifacts were recovered from the surface reconnaissance of a plowed field. Shovel testing in the vicinity of the flakes found no buried materials, features, or cultural horizons. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The site was recommended as being not eligible for the National Register.
- 31MK0548 Revisit of a site recorded by Hargrove in 1990 found a surface scatter of clear bottle glass, a shotgun shell, and refined white earthenware. These materials were determined to be recent and were recommended as not requiring additional investigation. The survey also found a prehistoric lithic scatter of quartzite and chert flakes from a small area that “did not contain diagnostic material or associated features.” It was determined that 31MK0548 did not have the potential to contribute significant information to the knowledge of North Carolina’s prehistory and was recommended as being not eligible for the National Register.
- 31MK0815 is situated on an upland ridgetop between the headwaters of Little Paw Creek - Danga Lake. The ground surface was level, and the site had been impacted by agricultural, residential, and road construction. The site was bisected by Mountainview Road, and the southern half of the site was destroyed by demolition of a private residence. The northern half of the site was impacted by construction of a second residence. At the time of the survey, both areas of the site were covered in residential lawn. It was determined that the site was unlikely to contribute to our knowledge of the prehistory of the area and was recommended as being not eligible for the National Register.
- 31MK0816 was situated on an upland ridgetop overlooking the headwaters of Little Paw Creek. The site had been impacted primarily by agricultural activities. At the time of the survey, the site was sparsely covered with tall weedy vegetation and recently tilled soil. The survey recovered four prehistoric artifacts and five historic artifacts. The prehistoric artifacts were debitage. This site also contained a light scatter of historic artifacts (milk glass, white earthenware, cobalt, shell-edged pearlware; hand-painted cobalt annularware; and one plain, undecorated (note: ceramic type not identified); one green bottle glass, one smokey flat glass, and three red tiles). It was determined that the site was unlikely to contribute to our knowledge of the history or prehistory of the area and was recommended as being not eligible for the National Register.
- 31MK0818 is a prehistoric lithic scatter of debitage and biface bases. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. It was determined that the site was unlikely to contribute to our knowledge of the prehistory of the area and was recommended as being not eligible for the National Register.
- 31MK0819 (also known as “Airport 9”) is a prehistoric lithic scatter of debitage found on an upland ridgetop overlooking the headwaters of Coffey Creek. The site was in a dense secondary forest. All artifacts were recovered from shovel testing. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0822 is a sparse prehistoric lithic scatter. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0824 is a prehistoric lithic isolated find of an incomplete PPK. The site is situated on an upland ridgetop near the headwaters of Beaverdam Creek. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0825 is a prehistoric lithic scatter situated on an upland ridgetop near the headwaters of Beaverdam Creek. Artifact recovery included cores, a biface, debitage, and a Late Woodland Savannah River PPK. All artifacts were recovered from a surface inspection of a recently cleared lot with 70-percent surface visibility. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0826 is a multicomponent site situated on an upland ridgetop near the headwaters of Beaverdam Creek. The prehistoric component is a lithic scatter of a possible groundstone tool, a core, a biface, debitage, and a Middle Woodland Swannanoa-type PPK. The historic component was comprised of late-nineteenth to twentieth-century ceramic fragments: undecorated refined earthenware and Bristol-glazed annularware stoneware. All artifacts were recovered from a surface inspection of a recently cleared lot with 70-percent surface visibility. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0827 is a prehistoric lithic scatter of a core, biface, debitage, Late Archaic Savannah River PPK, Middle Woodland Swannanoa-type PPK, and fire-cracked rock from an upland ridgetop near the headwaters of Beaverdam Creek. All artifacts were recovered from a surface inspection of a recently cleared lot with 70-percent surface visibility. The site

⁸ The Tolonen and Clifford 1998 report provided various levels of detail for site location and environmental setting and for the site inspection level of effort. The site summary in the report has been extracted from available information provided in the Tolonen and Clifford 1998 report.

- was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0828 is a sparse prehistoric lithic scatter. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0829 is a sparse prehistoric lithic scatter. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0830 is a sparse prehistoric lithic scatter. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0831 is a sparse prehistoric lithic scatter. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0832 is a sparse prehistoric lithic scatter. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0833 is a sparse prehistoric lithic scatter. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0834 is a sparse prehistoric lithic scatter. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0835 is a sparse prehistoric lithic scatter. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0836 is a sparse prehistoric lithic scatter. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0842 is an isolated find of prehistoric debitage from an upland ridgetop near the headwaters of Beaverdam Creek. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0844 is a multicomponent site situated on an upland ridgetop near the headwaters of Beaverdam Creek. The prehistoric component is a lithic scatter of bifaces and debitage. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The historic component was comprised of late-nineteenth to twentieth-century undecorated refined earthenware. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0845 is a prehistoric lithic scatter of debitage and biface bases. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The site was recommended as being not eligible for the National Register.
- 31MK0850 is a prehistoric lithic scatter of debitage and biface bases. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The site was recommended as being not eligible for the National Register.
- 31MK0852 is a sparse prehistoric lithic scatter of debitage and biface bases. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The site was recommended as being not eligible for the National Register.
- 31MK0853 is a sparse prehistoric lithic scatter of debitage and biface bases. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. Because this site was unlikely to contribute to our knowledge of North Carolina's prehistory, it was recommended as being not eligible for the National Register.
- 31MK0854 is a sparse prehistoric lithic scatter situated on an upland ridgetop near the headwaters of Beaverdam Creek. Artifact recovery was debitage. The lack of diagnostic artifacts made a date assignment to a specific prehistoric component impossible. The site was determined to have been destroyed by agricultural activities and was recommended as being not eligible for the National Register.
- 31MK0855 is a prehistoric lithic isolated find of a possible Early Archaic St. Charles PPK recovered along an unpaved road. The investigation determined that the artifact had been redeposited and the site was recommended as being not eligible for the National Register.
- 31MK0856 is a prehistoric lithic isolated find of debitage. Because this site was unlikely to contribute to our knowledge of North Carolina's prehistory, it was recommended as being not eligible for the National Register.

31MK0857 is a prehistoric lithic scatter of debitage found along an old logging road that had been extensively disturbed. Because this site was unlikely to contribute to our knowledge of North Carolina's prehistory, it was recommended as being not eligible for the National Register.

31MK0858 is a prehistoric lithic scatter of debitage. Because this site was unlikely to contribute to our knowledge of North Carolina's prehistory, it was recommended as being not eligible for the National Register.

31MK0859 is a prehistoric lithic scatter of debitage and biface bases. Because this site was unlikely to contribute to our knowledge of North Carolina's prehistory, it was recommended as being not eligible for the National Register.

Archaeological Investigations of a Possible Cemetery and Documentation of Other Sites on Property Owned by the Charlotte-Douglas International Airport (Robinson 2008)

In 2007, Ken Robinson with the Wake Forest University (WFU) Archaeology Laboratory investigated property owned by the Charlotte-Douglas International Airport to search for a possible unmarked slave cemetery, to document two historic dwelling sites, and to inspect the property to locate other sites, historic road traces, and landscape features. This work documented three archaeological sites (31MK1086, 31MK1087, and 31MK1088) that are within a mile of the West Boulevard Study Area. No evidence of the possible unmarked slave cemetery was found. Two sites (31MK1086 and 31MK1087) were recommended as being not eligible for the National Register. The third (31MK1088) was unassessed for National Register eligibility.

31MK1086 is also identified as "Dwelling Site 1." It is the site of a late-nineteenth-century dwelling that is depicted on the 1905 USGS *Charlotte Quadrangle* map and the 1910 USDA *Mecklenburg County NC Soil Survey Map*. Two structures are depicted at this location on both maps. The site inspection found that the house had been demolished with the floor joists and foundation piers left intact. Cultural material identified, but not recovered, included window glass, nails, clothing buttons and buckles, a brass trunk fragment, construction materials, door lock part, shoe and belt leather, and historic ceramics (alkaline-glazed stoneware and undecorated whiteware). Given the age of the site and extensive disturbance from demolition, it was determined that intensive investigations were not warranted because it was unlikely that they would add information to our understanding of twentieth-century Mecklenburg County. It was recommended as being not eligible for the National Register.

31MK1087 is also identified as "Dwelling Site 2." It is on a slightly sloping ridgetop that is wooded and overgrown with briars. A former house site is on a level ridge that extends several hundred feet. The primary feature of the dwelling is a two-sided brick fireplace and chimney on a stone foundation. The former house was estimated to be approximately 25 ft by 35 ft and had been supported by brick and stone piers. It was speculated that the structure had been a one-story residence. A review of the 1905 USGS *Charlotte Quadrangle* map and the 1910 USDA *Mecklenburg County NC Soil Survey Map* found that a structure is not depicted at this location. Cultural material identified, but not recovered, included window glass, nails, and building construction materials. Given the age of the site, it was determined that intensive investigations were not warranted because it was unlikely that they would add information to our understanding of twentieth-century Mecklenburg County. It was recommended as being not eligible for the National Register.

31MK1088 was defined as a "brick feature" that was an exposed rectangular-shaped pile of brick along a tributary of Coffey Creek. It was suggested that this site was used for brick-manufacturing during the construction of the Steele Creek Presbyterian Church (MK1377) that was listed on the National Register in 1991 (Mattson and Huffman 1991). Other interpretations include a mill site or residence. It was recommended that this site remain undisturbed until it could be archaeologically investigated. The site is currently being assessed for eligibility to the National Register in concurrent investigation of two proposed borrow areas.

Archaeological Survey of the Proposed South Crossfield Taxiway at the Charlotte-Douglas International Airport (Joy 2018)

In 2018, Legacy surveyed approximately 176 acres that are proposed for ground-disturbing activities associated with the construction of the South Crossfield Taxiway. This work identified three sites (31MK1143, 31MK1144, and 31MK1145) that are within a mile of the West Boulevard Study Area. They were determined lack potential to contribute to our understanding of the prehistory and history of the area and were recommended as being not eligible for the National Register.

31MK1143 is a prehistoric lithic isolated find of debitage recovered from a wooded ridgetop above Coffey Creek. The artifact was produced during tool production and maintenance. There was no evidence of an occupation midden, stratigraphic deposits, or features.

31MK1144 has a prehistoric and historic component. The prehistoric component is a sparse lithic scatter of an interior flake and a uniface. Shovel testing found no evidence of an occupation midden, stratigraphic deposits, or features. The historic component is associated with the Herron House. Artifacts date to the early- and mid-twentieth century (whiteware and

manganese-dioxide-tinted glass). Historic artifacts recovered from the site are Architectural-related (nails, brick, window glass, and asbestos roofing/siding), Kitchen-related (ceramics and bottle glass), and Heating-related (cinders and coal). Diagnostic ceramic artifacts include Bristol-glazed stoneware with Albany slip-glazed interior, alkaline-glazed stoneware, Rockingham-glazed yellowware, and undecorated whiteware. The historic component represents an early- to mid-twentieth-century farmstead with above-ground remains and an associated artifact scatter.

31MK1145 has a prehistoric and historic component. The prehistoric component is a sparse lithic scatter of debitage and a uniface. Shovel testing found no evidence of an occupation midden, stratigraphic deposits, or features. The historic component is associated with the McGinn House. Historic artifacts recovered from the site are indicative of Architectural-related activities (brick and window glass), Kitchen-related activities (whiteware ceramic plate and cup, a manganese-dioxide-tinted bottle fragment, and a condiment bottle), and Personal-activities (bisque doll leg, pharmaceutical and patent medicine bottles, nail polish bottle, and shoe polish bottle). The historic component represents an early- to mid-twentieth-century farmstead with above-ground remains and an associated artifact scatter.

Cultural Resource Investigations for a Proposed Drainage Area at the Charlotte-Douglas International Airport (Joy 2020)

In January 2020, Legacy recorded seven archaeological sites (31MK1148-31MK1154) in area proposed for ground-disturbing activities associated with a drainage area on Charlotte-Douglas International Airport property. All the sites consisted of small and/or disturbed artifact assemblages that lack potential to contribute meaningful knowledge to the history or prehistory of the area. These sites were recommended as being not eligible for the National Register.

31MK1148 consists of four prehistoric quartz lithic artifacts (debitage and a biface) that were recovered from deflated soil.

31MK1149 is comprised of three prehistoric quartz lithic debitage artifacts that were recovered from deflated soil.

31MK1150 is an isolated find of one prehistoric sherd recovered from deflated soil. This artifact (eroded quartz-tempered sherd) probably dates to the Middle Woodland period.

31MK1151 is comprised of three historic/modern artifacts that were recovered from testing around the abandoned shed. These materials are a 3-inch long ferrous wire nail and machine-made colorless bottle glass fragments.

31MK1152 consists of two historic/modern artifacts (machine-made amber-tinted cylindrical bottle with threaded finish and a 4-inch long ferrous wire nail).

31MK1153 consists of one modern brick fragment recovered from shovel testing around a pile of brick and a plumbing fixture.

31MK1154 is assigned to an abandoned house. No cultural material was recovered from testing around the structure.

Expected Archaeological Potential for the Study Area

Overall, the expected archaeological potential for the West Boulevard Relocation Study Area was low. Most of soils in the study area are identified as eroded (79.23 percent Cecil sandy clay loam = CeB2 and CeD2) or had slopes that ranged from 15-25 percent (8.32 percent Pacolet sandy loam = PaE). There are smaller areas of wetlands (0.3 percent), urban land (1 percent), and poorly drained soils along streams and drainage ways (1.15 percent). What remains as possible high- to moderate-probability areas are mapped with Helena (HeB) sandy loam with 2-8 percent slopes (10 percent). Additionally, much of the land on each side of roadways in the relocation corridors has been developed.

Previous research within a mile of the study recorded 55 sites; of which 89 percent (n=49) lacked integrity were recommended as being not eligible for the National Register. The high percentage of sites that lacked integrity, indicates that the likelihood of finding intact archaeological remains in the study area is low.

METHODOLOGY

The methodology used for the investigation is described below. It covers Background Research, Archaeological Investigations, Lab Methods, National Register of Historic Places Evaluation, and Technical Documentation.

Background Research

Background research to locate information about previously surveyed areas and previously recorded archaeological sites within the study area and within a mile of the study area was conducted prior to the initiation of fieldwork at the NC OSA in Raleigh. This included retrieving GIS shapefiles for previously recorded archaeological sites within the study area and within a mile of the study area and importing them into ARCGIS 10.5 to create site location maps and reviewing archived reports and site forms relevant to previous archaeological research in the study area.

Online research to collect information about historic occupation and environmental factors that may have contributed to historic or prehistoric settlement of the study area included a review of the NCHPO Architectural Database, the Mecklenburg County GIS

Polaris 3G, and NETR and Google Earth historical images. Historic and current USGS topographical maps were accessed from the ARCGIS USGS Historical Topographic Map Explorer, current USGS topographical maps were accessed from USGS TopoView, and the 1905 USGS *Charlotte Quadrangle* topographic map was downloaded from the Perry-Castañeda Library Map Collection at the University of Texas. Other historic maps that show the study area and were reviewed include the 1888 Thomas Jefferson Orr *Map of Mecklenburg County* at the Robinson-Spangler Carolina Room at the Charlotte-Mecklenburg Public Library, and the 1911 C. A. and J. B. Spratt *Map of Mecklenburg County* and the 1910 USDA *Soil Survey Map of Mecklenburg County* archived at the University of North Carolina Library.

Archaeological Investigations

Guidance from the NCHPO regarding fieldwork methodology on 29 May 2018 for the West Boulevard Relocation Study Area recommended the following:

The entire study area does not need to be comprehensively surveyed using shovel testing. A determination of higher/moderate/lower probability areas for the presence of archaeological sites can exclude areas with slopes exceeding 15 percent, water-logged or swampy areas, and areas classified as having eroded soils. Low probability areas can be inspected with pedestrian survey. The survey can be limited to the area of direct impacts associated with the proposed road relocation corridors.

Following the NCHPO guidance, the archaeological field investigations in the West Boulevard Relocation Study Area included a surface reconnaissance in areas with at least 50-percent ground-surface visibility, subsurface investigations with systematic transect shovel testing and some judgmentally placed shovel tests, and site assessment investigations with radial shovel testing. No test unit excavation was deemed appropriate for sites discovered or revisited during the investigation.

Surface Reconnaissance. Surface reconnaissance consisted of systematically examining exposed ground surfaces to determine if artifacts or surface traces indicative of an archaeological site were present. Experience has found that a surface survey is effective only in situations where 50 percent or more of the ground surface is visible and where there is no potential for buried archaeological resources. It is not effective in areas where less than 50 percent of the surface is exposed or in places where alluvial or colluvial soil deposition is likely to be present; these areas were inspected with subsurface testing.

Subsurface Investigations. Subsurface testing consisted of systematically excavating shovel tests at 30-m intervals. Each shovel test measured about 38 centimeter (cm) in diameter and was excavated to sterile subsoil, bedrock, or the watertable. All removed soil was screened through ¼-inch mesh hardware cloth to ensure uniform artifact recovery. Each shovel test was described in terms of depth, stratigraphy, and artifact recovery; the texture and color of all soils in each shovel test was recorded.

Site Assessment. Archaeological sites were defined as at least one artifact or cultural feature greater than 50 years of age. Site assessments included the following:

- establish a datum
- designate a numbering system for shovel tests and stratigraphic levels keyed to drawings, written records, and photographs
- use standard-sized excavation units
- hand-screen excavated soils through ¼-inch mesh
- excavate in levels that conform to natural soil strata
- use standardized field forms to document stratigraphy and artifact recovery
- describe soils using Munsell Soil color charts
- maintain a photographic record of the survey areas and site investigations

When a site was encountered, standard procedures were followed to gather preliminary data on cultural affiliation, site size and boundaries, integrity, and significance in terms of eligibility for the National Register. These procedures began with radial shovel tests excavated at 15-m intervals in cardinal directions from the positive shovel tests to establish the extent of the deposits, to identify potential subsurface activity areas, and to recover cultural artifacts and temporal data useful in producing an evidence-based evaluation of the site's National Register eligibility. Site testing was limited to the boundaries of the study area.

A sketch map of each site discovered was produced in the field. The location of shovel tests excavated at every site was placed on the site sketch map. Sites were photographed, and notes were taken concerning the site location and condition. Site locations were recorded with a global positioning system (GPS) receiver with sub-meter accuracy. Sites that were less than 30 m x 30 m were recorded as point data; sites greater than 30 m x 30 m were recorded as polygons.

Throughout the investigation, the Field Director maintained detailed notes on the survey methodology, sites identified during the survey, testing methodology, artifact recovery, and relevant environmental factors.

Lab Methods

The scope of work for this project anticipated recovering archaeological materials during the Legacy-ESI 2020 survey. The following is the proposed Lab Work Plan that was not implemented because no cultural material was recovered during the Legacy-ESI 2020 survey and cultural material recovered by New South in 2019 was not conveyed to Legacy.

Once the fieldwork is completed, the archaeological materials will be returned to Legacy's Durham facility for processing. They will first be organized by provenience and then verified against the field tracking form and the CSV (comma separated values) database generated from downloaded survey field data. Artifacts will be washed with a soft brush. The artifacts will set on trays to dry; then separated by provenience with their original field provenience card. Once dry, the artifacts will be rough sorted by artifact category and then examined closely for identification to determine the date or dates of occupation present and the range of activities carried out at each site. All artifacts will be weighed in grams and counted. All artifact information will be entered into a *Microsoft Access* database.

The laboratory methodology will include the following: artifact analysis, intra-site pattern analysis, and inter-site pattern analysis. Prehistoric lithic debitage artifacts will be identified by flake type (bifacial thinning flakes, cortical flakes, interior flakes, linear flakes, notching flakes, and shatter). Primary flakes will be identified by the presence of cortex on most of the dorsal surface; secondary flakes exhibit cortex on less than half the dorsal surface; and tertiary flakes will have no evidence cortex.

Historic artifacts will be identified using Stanley South's (1977) system for organizing historic period artifacts in eight functional groups (Kitchen, Architectural, Furniture, Arms and Ammunition, Clothing, Personal, and Tobacco, and Activities). By sorting artifacts from a site into each group and then counting them by group, the data can be compared to similar data generated from other sites.

Artifacts will be labeled with the accession number assigned by the NC OSA, packed in 4-mil-thick, clear, archival-quality, acid-free polyethylene storage bags with an acid-free provenience card, and boxed in acid-free archival storage containers. Legacy will temporarily curate all artifacts and records resulting from the fieldwork pending final curation and will prepare all materials for curation following the 2017 NC OSA *Archaeological Investigations Standards and Guidelines for Background Research, Field Methodologies, Technical Reports, and Curation*.

Technical Documentation

The technical documentation of the work details the background research, field investigations, artifact analysis, and site interpretation. The report includes a description of the archaeological sites located or revisited during the survey and provides recommendations for site significance according to National Register criteria of evaluation. The summary for each site is documented with maps and photographs.

Project study area GIS shapefiles were imported into ARCGIS 10.5 and were projected in NAD 1983 (2011) State Plane North Carolina FIPS 3200 (US feet). GIS mapping was used in the field to determine the extent of the study area and in the lab to generate site plans. Site coordinates in WGS 1984 recorded in the field with GPS with sub-meter accuracy. The coordinates were imported into the GIS program and were projected to NAD 1983 State Plane (US feet). Universal Transverse Mercator (UTM) coordinates and site elevations were exported from the GIS program for the report and site forms. Site location data was generated using NAD 1983, UTM 17N (meters) coordinates.

The documentation complies with the Secretary of the Interior's *Standards for Archaeological Documentation* and meets the requirements of the Secretary of Interior's *Standards and Guidelines for Archaeology and Historic Preservation* (48 FR 44716). It conforms to the NC OSA 2017 *Archaeological Investigations Standards and Guidelines for Background Research, Field Methodologies, Technical Reports, and Curation* and is formatted in accordance with the 2018 Society for American Antiquities (SAA) *Publication Policy and Style Guide*.

National Register of Historic Places Evaluation

All archaeological sites (cultural resources) identified or revisited during the survey were assessed for National Register eligibility. Cultural resources were evaluated using criteria for National Register eligibility specified by the Department of Interior (36 CFR Part 60). A recommendation on the significance of cultural resources was based on the National Register-eligibility criteria described in 36 CFR 60.4 and *National Register Bulletin 36* (Little et al. 2000) as follows:

Sites, objects, districts, structures, and buildings are determined as worthy of inclusion on the National Register if “The quality of significance in American history, architecture, engineering and culture is present” and if they “possess integrity of location, design, setting, materials, workmanship, feeling, association and

- Criterion a: are associated with events that have made a significant contribution to the broad patterns of our history; or
- Criterion b: are associated with the lives of persons significant in our past; or
- Criterion c: embody the distinctive characteristics of a type, period, or method of construction that represents the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction; or
- Criterion d: have yielded, or may be likely to yield, information important in prehistory or history.”

Most archaeological sites that are deemed significant and thus eligible for inclusion on the National Register fall under Criterion d because of the information that can be retrieved from analysis of archaeological materials. To assess the potential of a resource for contributing new or collaborative information to the theoretical and substantive knowledge of archaeology, its significance must be determined (Townsend et al. 1993). Consequently, researchers must consider how each site does or does not address the questions within the research design and within the framework of other regional research questions.

Archaeological sites recorded or revisited during the survey for the proposed West Boulevard Relocation were evaluated for National Register using the following attributes.

- Site Integrity – Does the site contain intact cultural deposits?
- Preservation – Does the site contain material suited to in-depth analysis and/or absolute dating, such as preserved features, botanical and/or faunal remains, or human skeletal remains?
- Uniqueness – Is the information contained in the site redundant in comparison to that available from similar sites, or do the remains provide a unique or insightful perspective on research concerns of regional importance?
- Relevance to Current and Future Research – Would additional work at this site contribute to our knowledge of the past? Would preservation of this site protect valuable information for future studies?

WEST BOULEVARD RELOCATION SURVEY

This chapter of the report presents work conducted for the proposed relocation of West Boulevard. It includes a discussion of the previously recorded archaeological sites within the study area and the results of two archaeological field investigations (New South 2019 and Legacy-ESI 2020). Information about the New South 2019 survey has been extracted from the *Affected Environment*, Final Charlotte-Douglas International Airport Environmental Impact Statement (VHB 2019) and North Carolina Archaeological Site Forms for 31MK0228, 31MK0229, 31MK1146, and 31MK1147 prepared by New South.

Previously Recorded Archaeological Sites

A search of the NC OSA database found that there were nine previously recorded sites in the Study Area (Table 5 and Figure 10).

Site	Component	Time Period	Site Type	National Register Assessment (Researcher Year)	National Register Recommendation
31MK0228	Prehistoric	Early, Middle, and Late Archaic	Lithic Scatter	Not Eligible (Tolonen and Clifford 1998)	No Further Work
31MK0229	Prehistoric	Early Archaic	Lithic Scatter	Unassessed (Fischer 1978, McLean and Sellon 1978)	Unassessed
31MK0548	Prehistoric	Early, Middle, and Late Archaic	Lithic Scatter	Not Eligible (Hargrove 1990, Tolonen and Clifford 1998)	No Further Work
31MK0854	Prehistoric	Non-diagnostic	Lithic Scatter	Not Eligible (Tolonen and Clifford 1998)	No Further Work
31MK0855	Prehistoric	Possible Early Archaic	Lithic Scatter	Not Eligible (Tolonen and Clifford 1998)	No Further Work
31MK0856	Prehistoric	Non-diagnostic	Lithic Scatter	Not Eligible (Tolonen and Clifford 1998)	No Further Work

Table 5. Previously Recorded Archaeological Sites.					
Site	Component	Time Period	Site Type	National Register Assessment (Researcher Year)	National Register Recommendation
31MK0857	Prehistoric	Non-diagnostic	Lithic Scatter	Not Eligible (Tolonen and Clifford 1998)	No Further Work
31MK0858	Prehistoric	Non-diagnostic	Lithic Scatter	Not Eligible (Tolonen and Clifford 1998)	No Further Work
31MK0859	Prehistoric	Non-diagnostic	Lithic Scatter	Not Eligible (Tolonen and Clifford 1998)	No Further Work

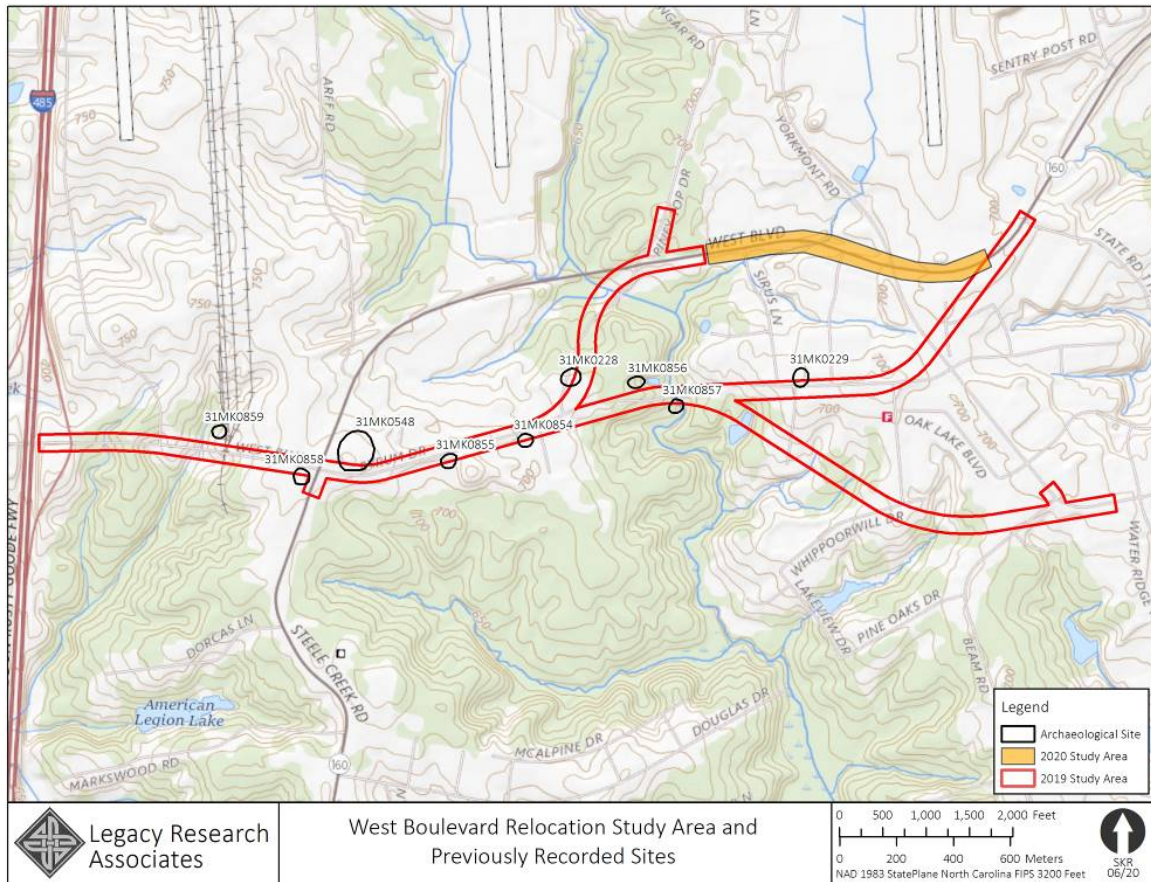


Figure 10. Previously Recorded Sites in the Study Area. Basemap: 2016 USGS *Charlotte West Quadrangle*, 1:24,000 scale.

Sites 31MK0228 and 31MK0229 were recorded by Fischer in 1978 during the Coffey Creek Drainage Survey and revisited by McLean and Sellon in 1978 during a survey of the Sugar Creek Basin. Both surveys identified the sites as having evidence of Archaic occupations and/or land use but did not assess the sites for National Register significance.

Site 31MK0228 was revisited in Tolonen and Clifford in 1998. They recovered non-diagnostic prehistoric lithic artifacts (quartzite flakes) during a surface reconnaissance; shovel testing found no buried materials, features, or cultural horizons. The site was recommended as being not eligible for the National Register.

Site 31MK0229 was not revisited by Tolonen and Clifford in 1998 and was identified in the NC OSA database as being unassessed for National Register eligibility.

Site 31MK0548 was recorded during a reconnaissance survey of the proposed Charlotte Outer Loop (Hargrove 1990). This site had diagnostic artifacts from the Archaic period that were recovered from a surface inspection of a recently plowed field. It was determined to have been extensively disturbed by agriculture, erosion, and possible road construction. Hargrove recommended that 31MK0548 was not eligible for the National Register. Site 31MK0548 was revisited in Tolonen and Clifford in 1998. They found a surface scatter of “recent” material (glass and ceramic) and a small surface scatter of prehistoric lithic (quartzite and chert) flakes that were recommended as being not eligible for the National Register.

Sites 31MK0854 to 31MK0859 are six prehistoric lithic scatters that were also recorded by Tolonen and Clifford in 1998. Five sites (31MK0854, 31MK0856, 31MK0857, 31MK0858, and 31MK0859) were non-diagnostic lithic scatters (debitage and biface fragments) that were recovered from disturbed context and one site (31MK0855) that consisted of a possible Early Archaic St. Charles PPK recovered on a logging road and was suspected as being a “redeposited” artifact. All six sites were recommended as being not eligible for the National Register.

Archaeological Field Investigations

The Study Area was surveyed for archaeological resources in March 2019 by New South and April 2020 by Legacy-ESI. Figure 11 shows the New South survey area outlined in red. Their work covered 97.86 acres of the 116.57-acre study area. The remainder of the study area (18.72 acres) was surveyed with assistance from Legacy’s subcontractor, ESI, in April 2020. Figure 11 also shows the location of the systematic shovel tests and site locations. A description of the two surveys follows.

New South Survey 2019

In March 2019, New South conducted a survey of the West Boulevard Relocation Study Area. The principal investigator was Shawn Patch, MA, Registered Professional Archaeologist.⁹

During the survey, 511 subsurface tests were plotted 30 m to either side of the centerline of the three proposed relocation alignments and on both sides a 950-m section of existing West Boulevard between Steele Creek Road and I-485 (Figure 12). Of these, 249 (48.7 percent) grid points were not excavated due to steep slopes, extensive ground disturbance, or wetlands. Systematic shovel testing was conducted at the remaining 262 (51.3 percent) grid points. Of these, four or 1.5 percent yielded cultural material. The remaining 258 shovel tests (or 98.5 percent of the excavated shovel tests) did not yield cultural material. Figure 12 shows the location of the shovel tests, identifies the unexcavated grid points and the excavated shovel tests, and indicates positive and negative artifact recovery.

Excavated tests typically exposed 10-20 cm of clay or silty clay subsoil. New South determined that since the normal pedons recorded for soils in the relocation survey corridors all have sandy loam A-horizons, their absence in the observed profiles indicated that the survey corridor has been eroded and/or disturbed. Visible evidence of disturbances included borrow pits, runoff management devices (e.g., catch basins), cut banks, natural gas lines, and fiber-optic lines. These were attributed to road construction, industrial park construction, mid-1990s house demolition, and buried utilities. Overall, much of the study area had been severely modified by modern development. Subsurface testing consistently demonstrated that the road relocation corridors were heavily eroded and/or disturbed, which indicated that the potential for intact subsurface artifact deposits or features was low.

During the March 2019 survey, New South revisited two previously recorded sites (31MK0228 and 31MK0229) that the NCHPO identified as unassessed for the National Register and recorded two new sites (31MK1146 and 31MK1147) (Table 6). All four sites are small and/or disturbed artifact assemblages that lack potential to contribute meaningful knowledge to the history or prehistory of the area. These sites are recommended as being not eligible for the National Register. A summary of the site investigations follows.

Table 6. Summary of the March 2019 New South Survey.					
Site	Component	Time Period	Description	National Register Assessment	National Register Recommendation
31MK0228	Prehistoric	Early, Middle, and Late	Lithic Scatter	Not Eligible	No Further Work
31MK0229	Prehistoric	Early Archaic	Lithic Scatter	Not Eligible	No Further Work
31MK1146	Historic	20 th Century	Homesite	Not Eligible	No Further Work
31MK1147	Historic	20 th Century	Homesite	Not Eligible	No Further Work

⁹ New South provided Legacy with NC Archaeological Site Forms for 31MK0228, 31MK0229, 31MK1146, and 31MK1147; however, their field notes, field maps, shovel test forms, field photography, artifacts, and GPS data were not available for reference.

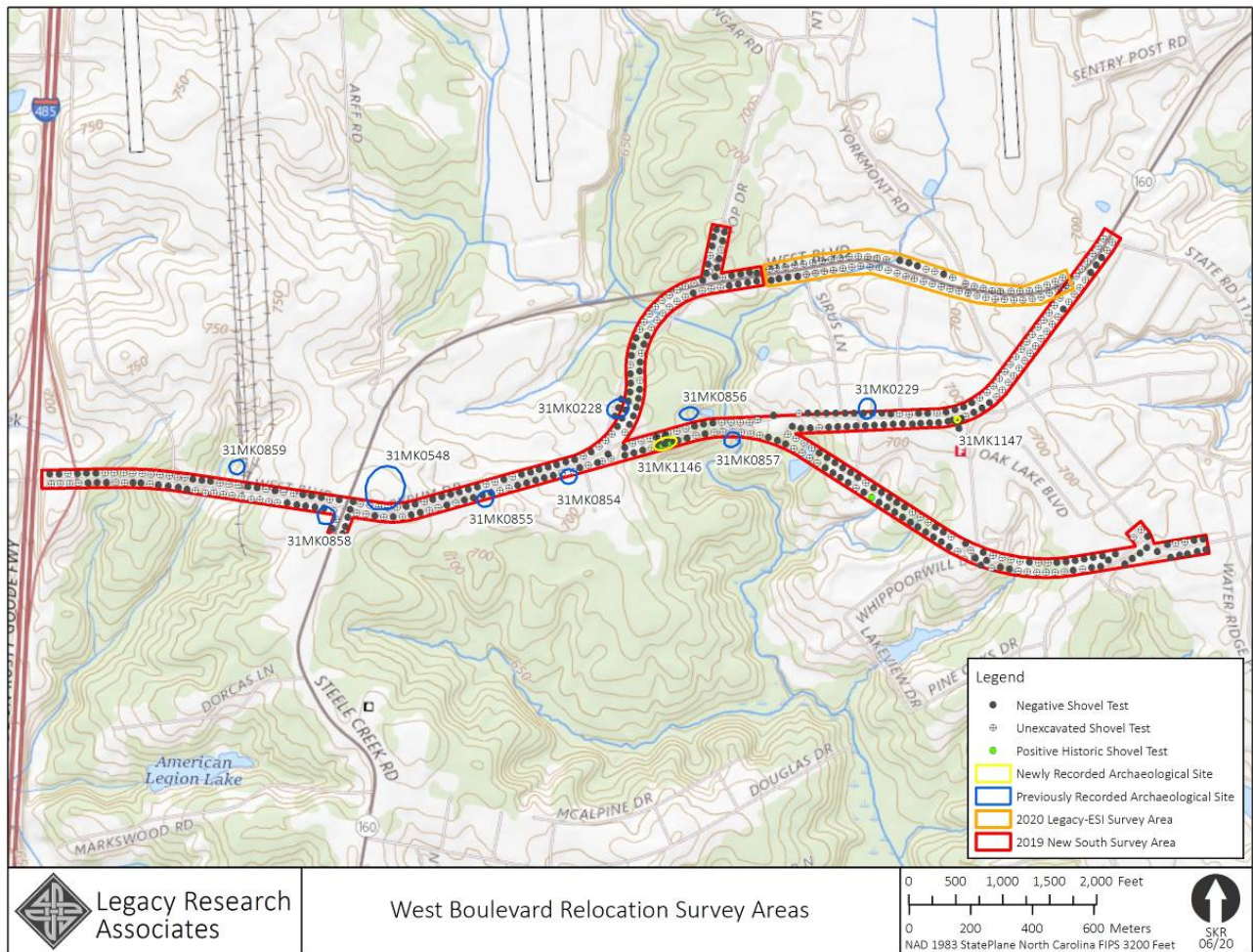


Figure 11. West Boulevard Relocation Study Area showing 2019 and 2020 shovel testing grid and the location of the two revisited previously recorded sites (31MK0228 and 31MK0229) and the two new sites (31MK1146 and 31MK1147). Basemap: 2016 USGS Charlotte West Quadrangle, 1:24,000 scale.

Site 31MK0228 Revisit

Site 31MK0228 was recorded in 1978 during the survey of the Coffey Creek Drainage (Fischer 1978). It was identified by the presence of Early, Middle, and Late Archaic PPKs (Palmer, Guilford, and Savannah River). The site was revisited later that year during research of the Sugar Creek Basin (McLean and Sellon 1978). At that time, the site was reported as being under cultivation and its significance was unknown. Twenty years later, the site was revisited by during a survey for the proposed expansion of the Charlotte-Douglas International Airport (Tolonen and Clifford 1998). The site was identified by four quartzite flakes recovered during a surface reconnaissance of a plowed field. Shovel testing in the vicinity of the flakes found no buried materials, features, or cultural horizons. The site was recommended as being not eligible for the National Register (Tolonen and Clifford 1998:99). During the NCHPO review for the current project, 31MK0228 was identified as being unassessed for the National Register.

In 2019, New South excavated systematic and judgmental shovel tests in the vicinity of the NC OSA plotted location for 31MK0228 (Figure 12). Site testing consisted of three systematic shovel tests (#253, #254, and #255) and three judgmental shovel tests that were 15-m west of the three systematic tests and outside the study area. All six tests encountered culturally sterile reddish brown (5YR 5/3) clay subsoil (Figure 13). Soils documented at 31MK0228 are consistent with the NRCS soil description for this location (Cecil eroded sandy clay loam – CeD2). A visual inspection of the site location found that it was covered with scrub vegetation, juvenile hardwoods, and grass (Figure 14). A powerline corridor and gravel road crossed through the NC OSA mapped location of 31MK0228 that appeared to have been extensively modified/disturbed.

Evidence of site disturbance, the absence of any identifiable topsoil, and the lack of cultural material indicated that 31MK0288 had no integrity and was too disturbed to convey any associations with significant individuals or historical events. Site 31MK0288 was recommended as being not eligible for the National Register. No further work was recommended for this site.

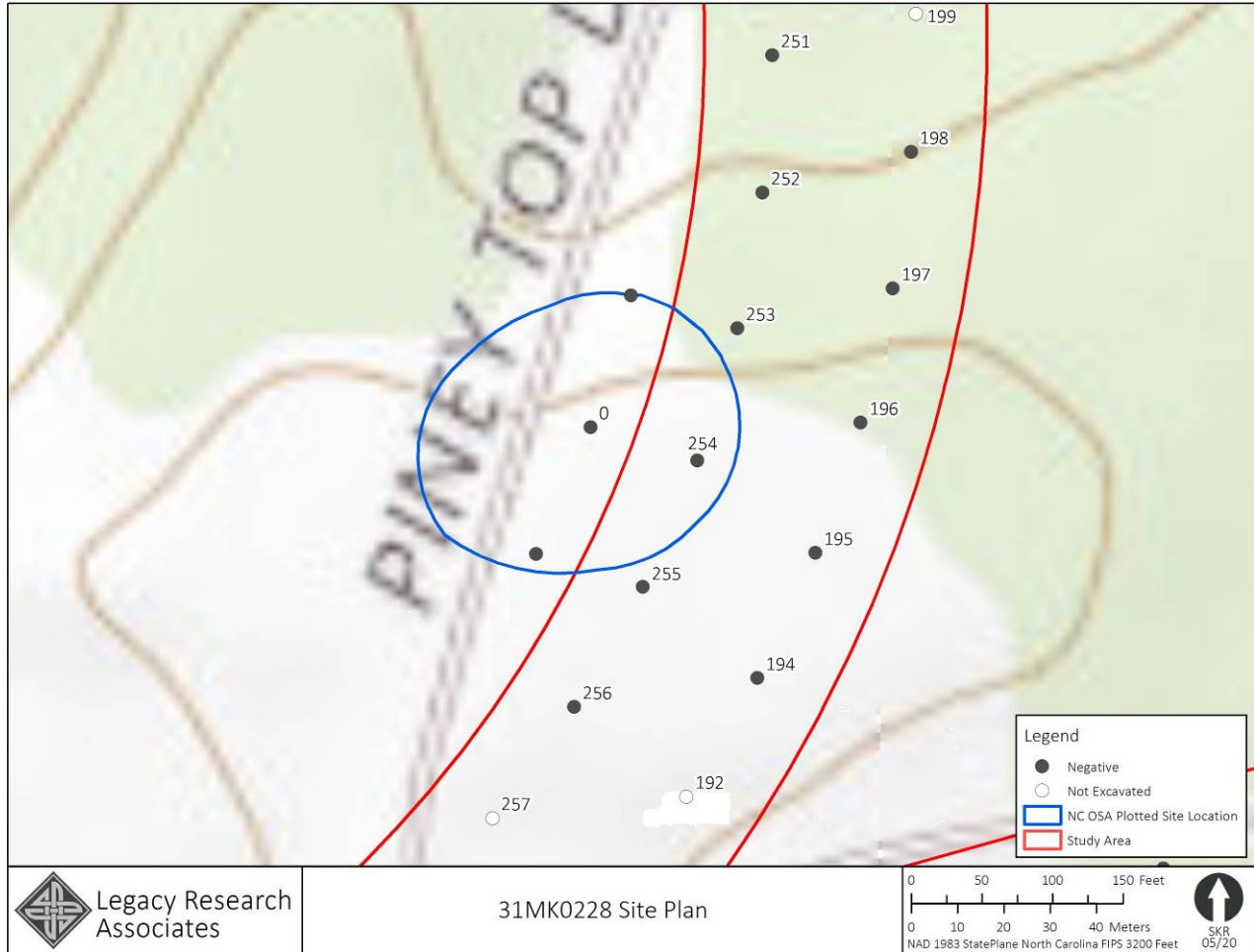


Figure 12. 31MK0228, site plan. Basemap: 2016 USGS *Charlotte West Quadrangle*, 1:24,000 scale.



Figure 13. 31MK0228, representative shovel test profile (New South Fieldwork Photograph 12 March 2019).¹⁰



Figure 14. 31MK0228, general setting (New South Fieldwork Photograph 12 March 2019).

¹⁰ Photographs from the 2019 New South survey were extracted from the *Affected Environment* in the Final Charlotte-Douglas International Airport Environmental Impact Statement dated 22 May 2019.

Site 31MK0229 Revisit

Site 31MK0229 was also recorded in 1978 during the survey of the Coffey Creek Drainage (Fischer 1978). It was identified by the presence of an Early Archaic Kirk PPK. The site was revisited later that year during research of the Sugar Creek Basin (McLean and Sellon 1978). They reported that the site had been destroyed by construction. The 9 May 2018 NCHPO assessment that identified 31MK0229 as being unassessed for the National Register.

In March 2019, the NC OSA plotted location of 31MK0229 was revisited by New South. The survey crew excavated two shovel tests (163 and 164) along the top of the rise and uncovered 15 cm of reddish brown (5YR 5/3) clay subsoil (Figure 15). Soils documented at 31MK0229 are consistent with the NRCS soil description for this location (Cecil eroded sandy clay loam – CeB2). Soil profiles exposed by shovel testing confirmed visual observation of disturbance by grading, construction, and utility excavation activity. No additional testing outside the study area was deemed warranted.

The survey crew also found that commercial development had impacted this location and grading for an industrial park located northwest of the site removed six to ten feet of soil (Figure 16 and Figure 17). Fiber-optic and gas lines were also present along the edge of Byrum Drive.

New South determined that the intensity of the disturbances removed any potential for this site to contain intact buried features or significant artifact deposits. Site 31MK0229 lacked integrity. The site was recommended as being not eligible for the National Register. No further work was recommended for this site.

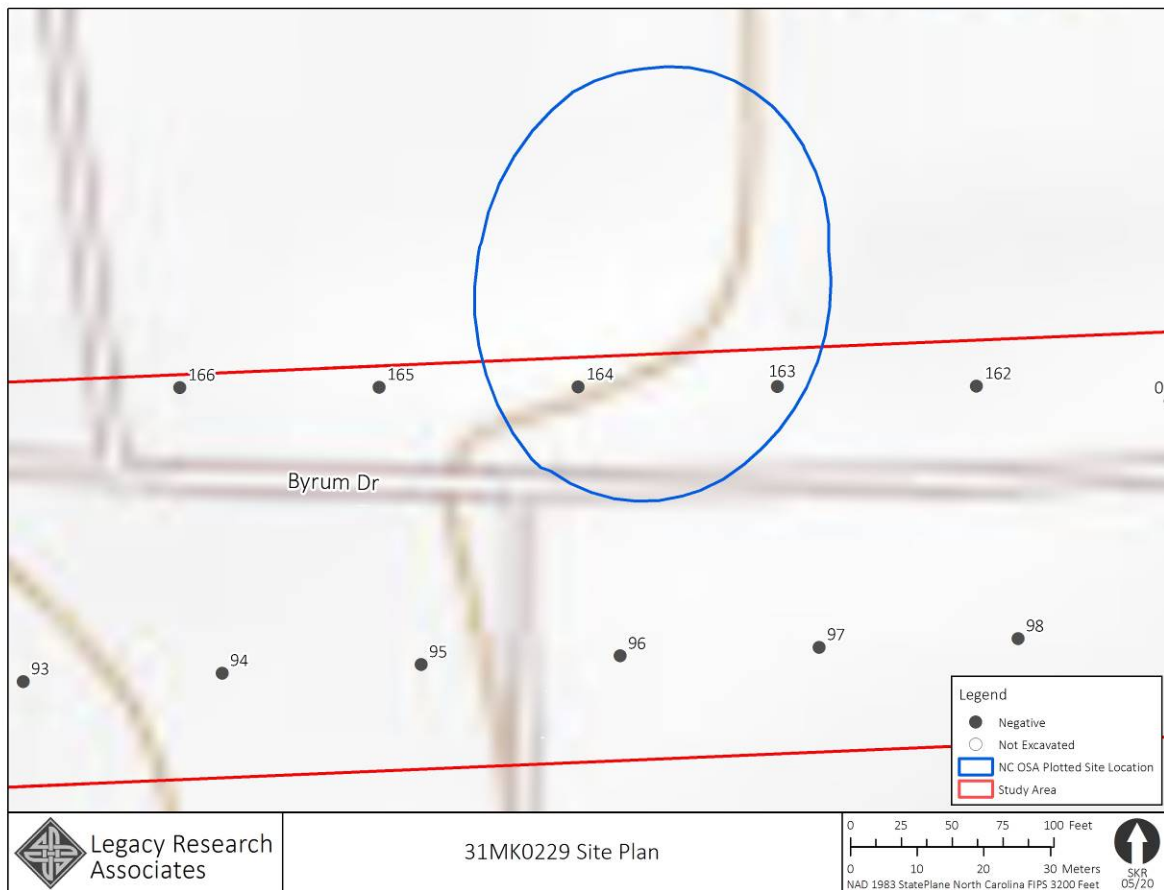


Figure 15. 31MK0229, site plan. Basemap: 2016 USGS *Charlotte West Quadrangle*, 1:24,000 scale.



Figure 16. 31MK0229, general setting showing Byrum Drive on the right (New South Fieldwork Photograph 12 March 2019).



Figure 17. 31MK0229, general setting (New South Fieldwork Photograph 12 March 2019).

Site 31MK1146

During the systematic shovel testing, New South identified an historic artifact scatter and ornamental vegetation on the south side of Byrum Drive. A summary of New South's site description follows.

The site is located on a south-facing bluff covered by mature oaks, small cedars, periwinkle, and ornamental yucca and bulbs (Figure 18). An aluminum chain-link fence separated the survey transect from Byrum Drive. Three mature oak trees were aligned three feet north of the fence line. The 1980 and 1993 USGS *Charlotte West Quadrangle* topographic maps placed an occupied structure near the site location. The earliest USGS *Charlotte West Quadrangle* map showing this structure dates to 1948. Aside from the fence line, no other above-ground features (e.g., foundations or house pads) were identified in the site vicinity, and it was assumed that the structure was demolished for road construction.



Figure 18. 31MK1146, general setting (New South Fieldwork Photograph 12 March 2019).

Survey shovel tests 5 and 6 yielded seven historic/modern artifacts (Figure 19). Subsurface testing at 15-m intervals (n=7) around the two positive shovel tests did not locate any additional artifacts, and site boundaries were estimated at 45 m x 15 m. The nine excavated tests exposed 15 cm of strong brown (7.5YR 5/8) clay subsoil (Figure 20). Soils documented at 31MK1146 are consistent with the NRCS soil description for this location (Cecil eroded sandy clay loam – CeB2). Shovel test 6 also uncovered a 20-cm thick gravel roadbed beneath 5 cm of strong brown (7.5YR 5/8) clay. This test elevation was below the grade of the modern roadbed, and the overburden was attributed to road construction slope wash. As the slope wash and subsoil were identical in color and texture, the artifacts collected from 31MK1146 could not be associated with a primary context.

New South determined that the artifact collection from 31MK1146 supported map evidence of a twentieth-century house location. The collection included four twentieth-century asphalt shingle fragments, one milk glass fragment, a brick fragment, and a mortar fragment (Table 7). Given the site's proximity to the re-aligned road, it was determined that the house was likely demolished during road construction occurring between 1981 and 1996. This activity and subsequent slope wash also affected the artifact deposit integrity. The few pieces of building debris recovered during the evaluation and the absence of identifiable activity areas were found to be evidence that the site has little research potential. New South found that 31MK1146 does not meet any National Register eligibility criteria, and recommended it as being not eligible for the National Register. No further work was advised for the site.

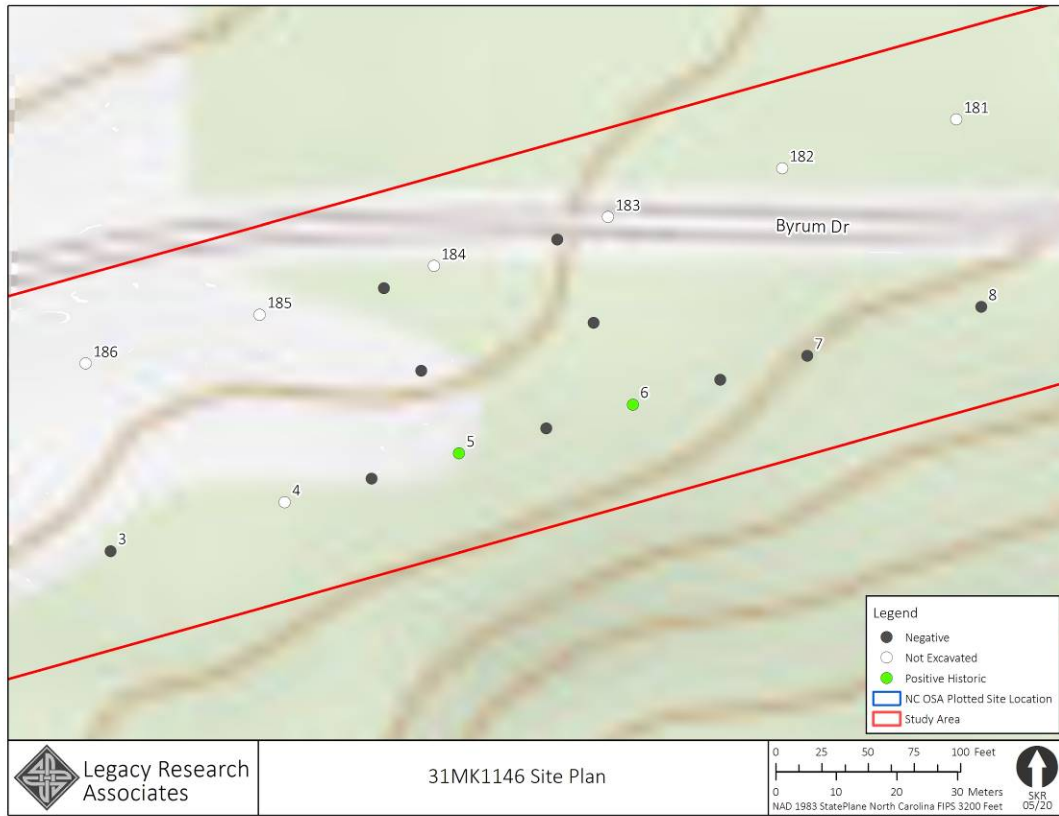


Figure 19. 31MK1146, site plan. Basemap: 2016 USGS *Charlotte West Quadrangle*, 1:24,000 scale.



Figure 20. 31MK1146, representative shovel test profile (New South Fieldwork Photograph 12 March 2019).

Table 7. Site 31MK1146 Artifact Inventory.				
Provenience	Depth below surface (cm)	Artifact	Count	Weight (grams)
ST 5	0-10	Asphalt roofing fragment (discarded)	4	1.2
		Milk glass fragment	1	2.1
ST 6	0-20	Concrete chunk	1	42.2
		Machine-made brick fragment	1	22.4
Total			7	67.9

Site 31MK1147

Site 31MK1147 was a surface scatter of historic artifacts (15 m x 15 m) located at the Byrum Road and Belle Oaks Drive intersection (Figure 21). New South described the site as follows.

The site is located 15 m southwest of the intersection, near a pair of large oak trees (Figure 23). A grass lawn extended across the area, and 50 percent of the surrounding ground surface was exposed during the site visit. The presence of a concrete driveway indicated that the surface scatter was associated with a former house site. A comparison of 1960 aerial photography and the 1948 USGS *Charlotte West Quadrangle* topographic map indicated this structure and Belle Oaks Drive were both constructed in the 1950s.¹¹ Aerial photography also showed that the structure was demolished between 1978 and 1998.

Two brick fragments and a porcelain insulator were collected from the ground surface near shovel test 104 (see Figure 21 and Table 8). Visual inspection of the surrounding area identified a concrete slab (0.5 m x 1.5 m) between the trees and the driveway. Site testing at 15-m intervals (n=4) consistently encountered yellowish red (5YR 5/8) clay subsoil at the ground surface (Figure 23). Soils documented at 31MK1147 are consistent with the NRCS soil description for this location (Cecil eroded sandy clay loam – CeB2). This stratum did not yield any artifacts, and the absence of any topsoil suggested that the site was heavily disturbed and/or eroded.

Table 8. Site 31MK1147 Artifact Inventory.				
Provenience	Depth below surface (cm)	Artifact	Count	Weight (grams)
ST 104	Surface	Porcelain insulator	1	8.4
		Machine made brick fragment	2	24.0
Total			3	32.4

New South determined that 31MK1147 does not meet National Register eligibility criteria. Aside from the driveway, evidence of 31MK1147 activity is limited and lacks integrity. The site has no potential to provide research contributions since the house was demolished and the artifact scatter was limited to surface finds with no context. The site was recommended as being not eligible for the National Register. No further work was recommended for 31MK1147.

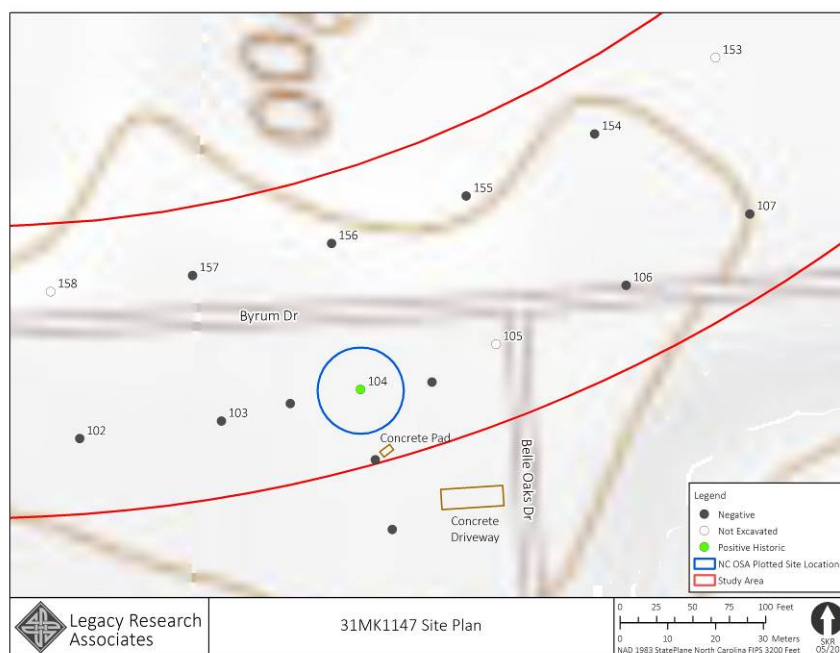


Figure 21. 31MK1147, site plan. Basemap: 2016 USGS *Charlotte West Quadrangle*, 1:24,000 scale.

¹¹ New South research of historic aerial imagery was conducted through NETR Historic Aerials.



Figure 22. 31MK1147, general setting showing Byrum Drive in the background (New South Fieldwork Photograph 12 March 2019).



Figure 23. 31MK1147, representative shovel test profile (New South Fieldwork Photograph 12 March 2019).

Legacy – ESI Survey 2020

An 950-m section along existing West Boulevard between the east end of Corridor A and Yorkmont Road that as added to the study area in 2020 was surveyed on 29 April 2020 by Legacy’s subcontractor, ESI (Figure 24). The work was conducted by Melissa McKay, MS, RPA, and Rebecca Sponseller. A visual inspection of the survey area found that most of it was extensively disturbed by past construction or was wetlands.

Two densely wooded areas (Area 1 and Area 2) were identified as having a high to moderate probability for the presence of archaeological sites. These two areas were relatively flat and had soils that were classified as Helena (HeB) sandy loam with 2 to 8 percent slopes. The USDA Soil Survey describes Helena soils as having a 20-cm-deep grayish brown plowzone overlying about 10 cm of light yellowish brown sandy loam eluvial horizon and yellowish brown clay subsoil.

Area 1 is southwest of the West Boulevard and Sirius Lane intersection. It extends 110 m along West Boulevard and covers 0.63 acres. Four shovel tests that were excavated along Transect 2 found deflated soils (Table 9). Soils documented Area 1 are inconsistent with the NRCS soil description for this location (Helena sandy loam – HeB). No cultural material was recovered.

Figure 25 is a view of the woodland area and Figure 26 is representative of the eroded soils documented during shovel testing.

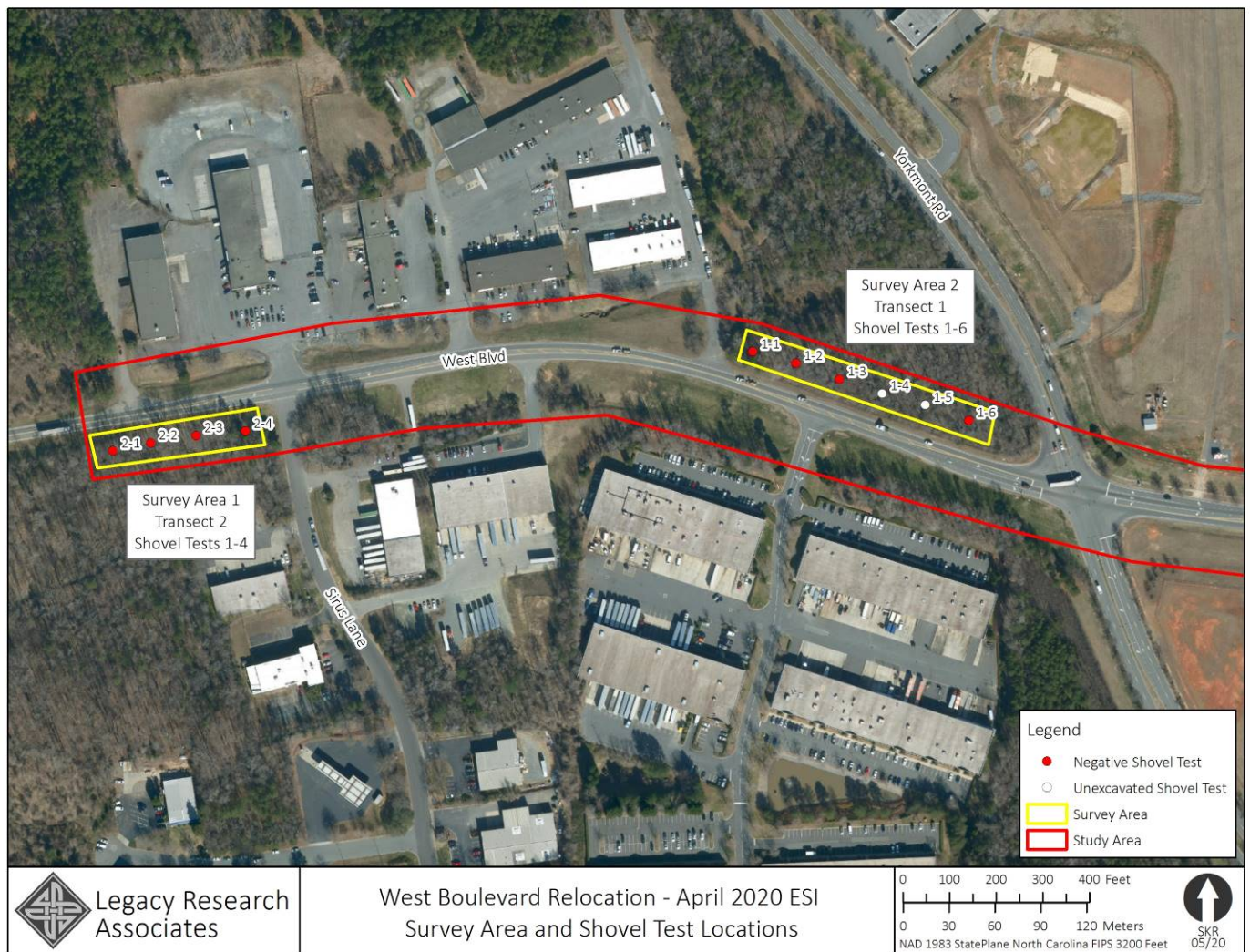


Figure 24. April 2020 Legacy-ESI Survey Area. Basemap: 2019 Aerial Image, NC CGIA.

Table 9. West Boulevard Relocation Area 1 (Transect 2) Shovel Test Summary.				
Provenience	Depth below surface (cm)	Soil Color	Description	Artifact Recovery
T2 ST 1	0-10	Dark brown (10YR 3/4) mottled with red (2.5YR 4/8)	Clay loam	None
	10-30	Dark reddish brown (5YR 3/3)	Sandy clay	
	30-40	Dark reddish brown (5YR 2.5/2)	Sandy clay	
T2 ST 2	0-10	Dark brown (10YR 3/3)	Clay loam	
	10-15	Red (10YR 4/8)	Clay loam	
	15-25	Yellowish brown (10YR 5/4) mottled with red (2.5YR 4/8)	Sandy clay	
T2 ST 3	0-20	Very dark grayish brown (10YR 3/2) mottled with strong brown (7.5YR 4/6)	Sandy clay loam	
	20-30	Red (5YR 4/6)	Clay	
T2 ST 4	0-10	Dark brown (7.5YR 3/3)	Sandy clay loam	
	10-20	Yellowish red (5YR 5/8)	Clay	

Area 2 is in the northeast quadrant of the West Boulevard and Yorkmont Road intersection. It extends 172 m along West Boulevard and covers 0.88 acres. The area was densely wooded with no ground-surface visibility (Figure 27). Six potential shovel test locations were identified along Transect 1 (Table 10). Two (T1 ST 4 and T1 ST 5) were not excavated due to the presence of standing water. In the other four shovel tests, hydric soils were encountered at 20-30 cm bs. Soils documented Area 2 are inconsistent with the NRCS soil description for this location (Helena sandy loam – HeB).

No cultural material was recovered from testing in Area 2. Figure 27 to Figure 30 document the conditions in the Area 2 and Figure 31 is representative of the hydric soils encountered during shovel testing in Area 2.

Table 10. West Boulevard Relocation Area 2 (Transect 1) Shovel Test Summary.				
Provenience	Depth below surface (cm)	Soil Color	Description	Artifact Recovery
T1 ST 1	0-20	Dark grayish brown (10YR 4/6) mottled with light yellowish brown (2.5YR 5/2)	Clay Loam	None
	20-30	Grayish brown (2.5YR 5/2)	Hydric Sandy Clay	
T1 ST 2	0-20	Brown (7.5YR 4/4)	Clay Loam	
	20-30	Dark yellowish brown (10YR 4/4) mottled with strong brown (7.5YR 5/6) and black (10YR 2/1)	Clay Loam	
	30-40	Light yellowish brown (10YR 6/4) mottled with black (10YR 2/1)	Hydric Sandy Clay	
T1 ST 3	0-20	Dark brown (7.5YR 3/4) mottled with light olive brown (2.5YR 5/4)	Sandy Clay Loam	
	20-30	Light olive brown (5YR 5/4)	Hydric Clay	
T1 ST 4	Not Excavated – Standing Water			
T1 ST 5	Not Excavated – Standing Water			
T1 ST 6	0-10	Very dark grayish brown (10YR 3/2)	Clay loam	
	10-30	Yellowish red (5YR 4/6) mottled with dark gray (10YR 4/1)	Sandy clay	



Figure 25. Transect 2 Shovel Test 4 location view west (ESI Fieldwork Photograph 29 April 2020).



Figure 26. Transect 2 Shovel Test 4 shovel test profile (ESI Fieldwork Photograph 29 April 2020).



Figure 27. Transect 1 Shovel Test 2 location view northwest (ESI Fieldwork Photograph 29 April 2020).



Figure 28. Transect 1 Shovel Test 3 location view north-northwest (ESI Fieldwork Photograph 29 April 2020).



Figure 29. Transect 1 Shovel Test 3 location showing erosion view southeast (ESI Fieldwork Photograph 29 April 2020).



Figure 30. Transect 1 Shovel Test 4 location view north (ESI Fieldwork Photograph 29 April 2020).



Figure 31. Transect 1 Shovel Test 6 shovel test profile showing hydric soil (ESI Fieldwork Photograph 29 April 2020).

SIGNIFICANCE EVALUATION AND RECOMMENDATIONS

The National Register assessment and recommendations for the two previously recorded sites (31MK0228 and 31MK0229) and the two new sites (31MK1146 and 31MK1147) located in the West Boulevard Relocation Study Area follows (Table 11).

Table 11. Summary of Archaeological Sites and National Register Eligibility Recommendations.				
Site	Component	Site Description	National Register Assessment	National Register Recommendation
31MK0228	Prehistoric	Archaic Lithic Scatter	Not Eligible	No Further Work
31MK0229	Prehistoric	Unknown Lithic Scatter	Not Eligible	No Further Work
31MK1146	Historic	20 th Century Homesite	Not Eligible	No Further Work
31MK1147	Historic	20 th Century Homesite	Not Eligible	No Further Work

31MK0228 – Earlier investigations at this site identified the presence of Early, Middle, and Late Archaic diagnostic PPKs (Fisher 1978). Later that year, the site was revisited and reported as being under cultivation (McLean and Sellon 1978). It was not assessed for the National Register at that time. Twenty years later the site was revisited (Tolonen and Clifford 1998). That survey recovered four quartzite flakes during a surface reconnaissance of a plowed field. Shovel testing in the vicinity of the flakes found no buried materials, features, or cultural horizons. The site was recommended as being not eligible for the National Register (Tolonen and Clifford 1998:99). During the NCHPO review for the current project, 31MK0228 was identified as being unassessed for the National Register.

Testing by New South at the NC OSA plotted location for 31MK0228 in 2019 resulted in finding no cultural material, no intact soil horizons, and no cultural features. All tests encountered culturally sterile reddish brown clay subsoil. A powerline corridor and gravel road crossed through the site location. The area appeared to have been extensively modified/disturbed.

New South determined that the strong evidence of site disturbance, the absence of any identifiable topsoil, and the lack of cultural material indicated that 31MK0288 has no integrity and is too disturbed to convey any associations with significant individuals or historical events. Site 31MK0288 was recommended as being not eligible for the National Register. No further work was recommended for this site.

31MK0229 – Like 31MK0228 discussed above, this site was recorded in 1978 during the survey of the Coffey Creek Drainage (Fischer 1978). It was identified by the presence of an Early Archaic Kirk PPK. The site was revisited later that year during research of the Sugar Creek Basin (McLean and Sellon 1978). They reported that the site had been destroyed by construction. The 9 May 2018 NCHPO assessment identified 31MK0229 as being unassessed for the National Register.

In March 2019, the NC OSA plotted location of 31MK0229 was revisited by New South. Shovel testing documented clay subsoil at the surface that confirmed the visual observation of disturbance by grading, construction, and utility excavation activity. The survey crew observed that commercial development had impacted the site location and grading for an industrial park located northwest of the site removed six to ten feet of soil. Fiber-optic and gas lines were also present along the edge of Byrum Drive. New South determined that the intensity of the disturbances removed any potential for the site to contain intact buried features or significant artifact deposits. They further stated that 31MK0229 “lacks integrity and cannot be associated with any significant historic individuals or events.” The site was recommended as being not eligible for the National Register. No further work was recommended for this site.

In addition to revisiting two previously recorded sites (31MK0228 and 31MK0229) described above, New South recorded two new sites (31MK1146 and 31MK1147) in the West Boulevard Relocation Study Area. These two sites are discussed below.

31MK1146 is an historic artifact scatter and ornamental vegetation. The site is located on a south-facing bluff covered by mature oaks, small cedars, periwinkle, and ornamental yucca and bulbs. Shovel testing exposed 15 cm of strong brown clay subsoil at the surface. One test (6) uncovered a 20-cm thick gravel roadbed beneath 5 cm of strong brown clay. This test elevation was below the grade of the modern roadbed, and the overburden was attributed to road construction slope wash.

Survey shovel tests 5 and 6 yielded seven historic/modern artifacts that are identified as a milk glass fragment, concrete chunk, machine-made brick fragments, and four asphalt roofing shingles. The cultural material supports the historic map evidence of a twentieth-century house location. It was assumed that given the site’s proximity to the re-aligned road, the house was likely demolished during road construction occurring between 1981 and 1996. This activity and subsequent slope wash also affected the artifact deposit integrity. The few pieces of building debris recovered during the evaluation and the absence of identifiable activity areas were evidence that the site has little research potential. Thus, New South recommended 31MK1146 as being not eligible for the National Register under Criterion D. The site does not provide any strong associations with significant individuals or historical events, which indicates the site is not eligible under National Register Criteria A and B. The house demolition also removed any potential Criterion C eligibility. As the site does not meet any National Register eligibility criteria, 31MK1146 was recommended as being not eligible for the National Register and no further work was advised for the site.

31MK1147 consists of an historic artifact scatter (two brick fragments and a porcelain insulator) that was recovered by the New South field crew near the intersection of Byrum Road and Belle Oaks Drive. The site is located about 15 m southwest of the intersection near two large oak trees. A grass lawn extended across the area, and 50 percent of the surrounding ground surface was exposed during the site visit. The presence of a concrete driveway indicated that the surface scatter was likely associated with a former house site. A comparison of 1960 aerial photography and the 1949 USGS *Charlotte West Quadrangle* topographic map indicated that this structure and Belle Oaks Drive were constructed in the 1950s. Aerial photography also showed that the structure was demolished between 1978 and 1998. Visual inspection of the surrounding area identified a 0.5-m x 1.5-m reinforced concrete slab between the trees and the driveway. Site testing at 15-m intervals consistently encountered yellowish red (5YR 5/8) clay subsoil at the ground surface. This stratum did not yield any artifacts, and the absence of any topsoil suggested that the site was heavily disturbed and/or eroded.

New South determined that 31MK1147 does not meet National Register eligibility criteria. Aside from the driveway, evidence of site activity is limited and lacks integrity. The site has no potential to provide research contributions since the house was demolished and the artifact scatter was limited to surface finds with no context. Thus, 31MK1147 does not meet National Register Criterion D eligibility criteria. The site cannot convey any associations with any significant individuals or events, and therefore is recommended not eligible under Criterion A or B. The site was also recommended as being not eligible under Criterion C since it does not possess an ability to convey associations with distinctive architectural or engineering patterns. The site was therefore recommended as being not eligible for the National Register under all four criteria. No further work was recommended for 31MK1147.

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APPENDIX A – ARTIFACT INVENTORY

Site 31MK1146 Artifact Inventory.				
Provenience	Depth below surface (cm)	Artifact	Count	Weight (grams)
ST 5	0-10	Asphalt roofing fragment (discarded)	4	1.2
		Milk glass fragment	1	2.1
ST 6	0-20	Concrete chunk	1	42.2
		Machine-made brick fragment	1	22.4
Total			7	67.9

Site 31MK1147 Artifact Inventory.				
Provenience	Depth below surface (cm)	Artifact	Count	Weight (grams)
ST 104	Surface	Porcelain insulator	1	8.4
		Machine made brick fragment	2	24.0
Total			3	32.4



**North Carolina Department of Natural and Cultural Resources
State Historic Preservation Office**

Ramona M. Bartos, Administrator

Governor Roy Cooper
Secretary Susi H. Hamilton

Office of Archives and History
Deputy Secretary Kevin Cherry

October 12, 2020

Deborah Joy
Legacy Research Associates

djoy@legacy-research.com

Re: Future Airport Development and New Runway & Terminal, Charlotte Douglas International
Airport, Mecklenburg County, ER 16-1791

Dear Ms. Joy:

Thank you for your letter of September 1, 2020, transmitting the archaeological survey report associated with the above-referenced project. We have reviewed the information provided and offer the following comments:

According to the Phase I archaeological report submitted by Legacy Research Associates (Legacy), approximately 116 acres were investigated by both New South Associates as well as Legacy's subcontractor, Environmental Services, Inc. As a result of this work, two unassessed archaeological sites were revisited and evaluated, 31MK228 and 31MK229, and two newly identified sites were recorded, 31MK1146 and 31MK1147.

All four sites consist of small and/or disturbed artifact assemblages (prehistoric lithic scatters and twentieth century domestic artifact scatters) that do not retain sufficient site integrity to contribute new information about the history or prehistory of the area. These sites are recommended as being not eligible for the National Register of Historic Places and no further archaeological work is recommended for the West Boulevard Relocation as currently planned.

We concur with their findings and recommendations and accept the report as final.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, contact Renee Gledhill-Earley, environmental review coordinator, at 919-814-6579 or environmental.review@ncdcr.gov. In all future communication concerning this project, please cite the above referenced tracking number.

Sincerely,



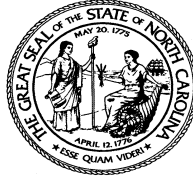
for Ramona Bartos, Deputy
State Historic Preservation Officer

cc: Tim Alexander
FAA, Memphis Airport District Office

Timothy.L.Alexander@faa.gov

Sarah Potter
Landrum and Brown

SPotter@landrum-brown.com



**North Carolina Department of Natural and Cultural Resources
State Historic Preservation Office**

Ramona M. Bartos, Administrator

Governor Roy Cooper
Secretary Susi H. Hamilton

Office of Archives and History
Deputy Secretary Kevin Cherry

October 26, 2020

Tommy L. Dupree, Manager
Memphis Airports District Office
US Federal Aviation Administration
2600 Thousand Oaks Blvd.
Memphis, TN 38118

tommy.dupree@faa.gov

RE: Phase I Report, Historic Resources, Charlotte Douglas International Airport,
Mecklenburg County, ER 20-0438

Dear Mr. Dupree:

Thank you for providing the above-referenced report for our review and comment. The report covers a wide variety of resource types for which we offer the following comments.

We concur with the findings regarding previous eligibility recommendations for the reasons cited in the report as follows, noting where some resources are addressed in an existing MOA:

- William Grier House (MK1364 and MK3993), eligible, although deterioration is having an impact on eligibility, covered by an MOA
- Steele Creek Presbyterian Church (MK1377), NRHP-listed, remains eligible
- John Douglas House (MK1361), DOE, remains eligible, covered by an MOA
- Spratt-Grier Farm (MK1875), DOE, remains eligible, covered by an MOA
- Oakden Motel (MK2150), DOE, remains eligible
- WPA Douglas Airport Hanger (MK2399), DOE, remains eligible
- Steele Creek Manse (MK1378), DOE, remains eligible, covered by an MOA
- Byrum Croft House (MK2841), DOE, remains eligible, covered by an MOA
- Two Guys Auto Repair Body Shop (MK3197), DOE, remains eligible

We concur with the findings regarding resources that should be considered for eligibility in the future as follows:

3100 Yorkmont Road (MK3770)
3140 Yorkmont Road (MK3762)
FAA Control Tower (MK3798)
CLT Box Building (MK38160)
4824 and 4828 Parkway Plaza Boulevard (MK4115 and MK4123)

We concur with the finding that the Charlotte Speedway Ticket Office/Barn (MK3933), is not eligible for the National Register due to lost integrity of setting, feeling, and association.

For the remaining resources, however, we do not have enough information to concur regarding eligibility. All the following resources need to be placed in their historical and architectural context and compared to other, similar resources to make a defensible determination of eligibility. When possible, it is most effective to compare the resource in question to a similar resource that is already listed or determined eligible. To help provide adequate context for the resources, we offer the following comments:

Fred Webber House (MK3192): To determine eligibility for a resource under Criterion B, the resource must be associated with the person's productive life. Was this house where he lived when he founded K&W? Does the founding of K&W make him a person of importance? Is there a K&W building that would better represent his productive life? In short, is he significant in history and is this the best building to convey that significance?

Big Spring Methodist Church (MK3822): How does this compare to other 1950s, Colonial Revival churches in Mecklenburg County?

5455-5461 Wilkinson Boulevard (MK4047): How does this complex compare to other, mid-twentieth century shopping buildings in Charlotte or Mecklenburg county? How is its design innovative relative to other shopping strips?

Old Terminal Building (MK2189): Are there other 1950s air terminals in Mecklenburg County and/or how does this one compare to other terminals at the other airports active during the 1950s (Smith Reynolds Airport in Winston-Salem and Raleigh Durham International Airport come to mind as comparison properties)?

Thomas W. Ferebee Building (MK3825): To evaluate the eligibility of the military-related buildings in the APE, including the Officer's and NCO clubs, Morris Field Baptist Church, the Stonewall Jackson Homes, etc., the report must discuss the architectural and historical significance of Morris Field and of the military in Charlotte and Mecklenburg County and compare these military resources to other military resources in the county, if any exist. Additionally, please address how it meets Criteria Consideration B regarding moved buildings.

9311 Markswood Road (MK3836): How does it compare to other log kit houses from the 1980s? Why would it not be considered for eligibility when it reaches fifty-years in age? Show other, better examples to make this case.

Officers Club (MK3823): To evaluate the eligibility of the military-related buildings, the report must discuss the architectural and historical significance of Morris Field and of the military in Charlotte and Mecklenburg County and compare these military resources to other military resources in the county, if any exist.

Stonewall Jackson Homes (MK4126): To evaluate the eligibility of the military-related buildings, the report must discuss the architectural and historical significance of Morris Field and of the military in Charlotte and Mecklenburg County and compare these military resources to other military resources in the county, if any exist. Or, if this development was not related to the military, place it in context with other post-war or war-era apartments. Furthermore, the architectural integrity of this complex is questionable and

should be addressed. How do the replacement windows and doors and introduction of vinyl siding affect architectural integrity?

Cold War Era Fallout Shelter (MK3853): The report itself notes what is needed to make a defensible eligibility case. The report needs a context of Cold War-era civilian preparedness and it needs to address the context of existing similar shelters.

West Mecklenburg High School (MK4023): How does this campus compare to other large, county high schools? And do changes over time to the design mean that the complex is not eligible for the Register because of a lack of architectural integrity, regardless of historic context?

Steeleberry Drive Neighborhood (MK4407): Many, many, many post-war subdivisions were built in Charlotte; how does Steeleberry compare to the others that are already determined eligible or listed?

Harvest Center (MK4004): To evaluate the eligibility of the military-related buildings, the report must discuss the architectural and historical significance of Morris Field and of the military in Charlotte and Mecklenburg County and compare these military resources to other military resources in the county, if any exist.

Ashley Circle (MK4127): To evaluate the eligibility of the military-related buildings, the report must discuss the architectural and historical significance of Morris Field and of the military in Charlotte and Mecklenburg County and compare these military resources to other military resources in the county, if any exist. Or, if this development was not related to the military, place it in context with other post-war or war-era apartments. Additionally, the windows appear to be replacements and it appears that vinyl siding has been applied over the gable ends. The doors may also be replacements. The report should confirm if the materials are original or early and evaluate the complex's architectural integrity.


Additionally, all resources recommended as eligible for the National Register must include a map of the proposed boundary and a justification of the boundary.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, contact Renee Gledhill-Earley, environmental review coordinator, at 919-814-6579 or environmental.review@ncdcr.gov. In all future communication concerning this project, please cite the above referenced tracking number.

Sincerely,



 Ramona Bartos, Deputy
State Historic Preservation Officer

cc: Tim Alexander, FAA

timothylalexander@faa.gov

FAA's Determination of Effect



U.S. Department
of Transportation
**Federal Aviation
Administration**

Memphis Airports District Office
2600 Thousand Oaks Blvd., Suite 2250
Memphis, TN 38118

Phone (901) 322-8180

February 23, 2021

Ms. Renee Glenhill-Early
North Carolina Department of Natural and Cultural Resources
State Historic Preservation Office
4617 Mail Service Center
Raleigh, NC 27699-4617

**RE: Capacity Enhancement Project EA, CLT, Charlotte, Mecklenburg County, ER
20-0438**

Dear Ms. Glenhill-Early:

As part of the Federal Aviation Administration's (FAA's) Section 106 review and pursuant to 36 CFR § 800.4, the FAA has undertaken identification efforts for the Capacity Enhancement Project EA. Based on the results of these efforts the FAA has determined a finding of adverse effect is appropriate for this undertaking.

Tribal Consultation: The FAA initiated Section 106 consultation with the Catawba Native American Tribe to determine their interest as a consulting party. The FAA has not received a response, to date, but will document any reply we receive regarding their participation in the consultation.

Area of Potential Effects: The Area of Potential Effects (APE) for this undertaking was defined as the area in which the proposed project may cause an alteration in the character or use of historic resources. Concurrence with the APE by the North Carolina Department of Natural and Cultural Resources (NC DNCR) was received via letter dated January 21, 2021.

Identification Efforts of Historic Properties in the APE: The FAA, through a series of resource surveys, identified historic resources located within the APE. The resources identified within the APE are included in **Table 1** along with a description and eligibility determination. The surveys identified twenty two (22) aboveground resources within the APE. Seventeen (17) were determined not eligible, one (1) is no longer extant, two (2) were recommended for further study upon reaching 50 years of age, one (1) resource, WPA Douglas Airport Hangar, was previously determined eligible, and one (1) resource, Old Terminal, is recommended as eligible as part of this effort. The study with the eligibility recommendation entitled, *Addendum Historic Resources Report, Charlotte Douglas International Airport, Old Terminal Building (Mk2189), Mecklenburg County, North Carolina, ER# 20-0438*, is enclosed for your review.

Finding of Effect: A determination of effects for NRHP eligible properties follows.

MK2399/3761 WPA Douglas Airport Hangar: This property is located to the northeast of the airfield on Airport property within the established APE for this undertaking. It is recommended eligible for National Register listing under Criterion A for its association with New Deal work programs in North Carolina during the Great Depression. The proposed undertaking would not physically alter the property or change the character of the property's use or physical features with the property's setting that contribute to its historic significance. In addition, the view-shed of this resource would not be altered as it is currently located on the airfield next to airport facilities, active taxiways, and two active runways. The WPA Douglas Airport Hangar would not experience a 1.5 dB increase within the 65 DNL of either the 2028 or the 2033 noise contours, which is FAA's threshold for a significant noise impact. Therefore, the proposed undertaking would not significantly change the property's setting or diminish the integrity of the property's significant features. In addition, the 2028 and 2033 Proposed Action (Proposed Undertaking) would not cause significant air pollutant emissions or water pollutants that could affect this structures. Therefore, the FAA finds No Adverse Effect from the proposed undertaking on the WPA Douglas Airport Hangar.

MK2189 Old Terminal: The Old Terminal building is located in the south midfield on Airport property within the established APE for this undertaking. The property has been recommended eligible for National Register listing under Criterion A for its association with the development of air travel in Charlotte in the mid-20th century and Criterion C as one of the best remaining examples of a mid-century air terminal, embodying the distinctive characteristics of a modernist air terminal from the early days of commercial air travel. The proposed undertaking would physically remove/destroy the property. Therefore, the FAA finds an Adverse Effect from the proposed undertaking on the Old Terminal.

Summary: Based on the results of the studies and an assessment of effects to historic properties, the FAA has determined that this proposed undertaking will have an adverse effect on one historic property; MK2189 Old Terminal. Please review this finding and the enclosed documentation in accordance with 36 CFR Part 800.8.

If you have any questions regarding the EA or this request, please contact Tim Alexander at (901) 322-8188 or by email at timothy.l.alexander@faa.gov.

Sincerely,

Duane L. Johnson, PE
Assistant Manager, Memphis Airports District Office

Enclosures

cc: Amber Leathers, City of Charlotte Aviation Department

Table 1, Resources Identified and Eligibility

Resource Number	Resource Name	NRHP Eligibility Recommendation
MK1368	Herron House	No Longer Extant
MK2189	Old Terminal Building	Eligible Under Criterion A and C ¹
MK2399/3761	WPA Douglas Airport Hangar	Eligible Under Criterion A
MK3725	c. 1980 Prefab Warehouse	Not Eligible
MK3771	Main Airport Terminal	Not Eligible
MK3798	FAA Control Tower	Potentially Eligible In 2029
MK3799	Parking Toll Plaza	Not Eligible
MK3800	c. 1980 Commercial Building	Not Eligible
MK3801	c. 1956 Warehouse	Not Eligible
MK3802	c. 1970 Warehouse	Not Eligible
MK3803	c. 1980 Truck Terminal	Not Eligible
MK3813	c. 1980 Warehouse	Not Eligible
MK3814	c. 1980 Warehouse	Not Eligible
MK3815	c. 1980 Warehouse	Not Eligible
MK3816	c. 1980 Truck Terminal	Potentially Eligible In 2031
MK3819	1970 Prefab Warehouse	Not Eligible
MK3820	c. 1980 Warehouse	Not Eligible
MK3821	c. 1955 Office	Not Eligible
MK3822	c. 1950 Church	Not Eligible
MK3825	Thomas W. Ferebee Building	Not Eligible ²
MK4075	c. 1970 Commercial Building	Not Eligible
MK4076	c. 1970 Commercial Building	Not Eligible

Source: Environmental & Archaeology, LLC

Notes:

1. Recommendation study included as attachment to this letter.
2. Email from Renee Gledhill-Early, NC SHPO to Stephen Saucier at Carolinas Aviation

DRAFT

**ADDENDUM HISTORIC RESOURCES REPORT
CHARLOTTE DOUGLAS INTERNATIONAL AIRPORT
OLD TERMINAL BUILDING (MK2189)
MECKLENBURG COUNTY, NORTH CAROLINA
ER# 20-0438**

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February 2021

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INTRODUCTION

Environment & Archaeology, LLC performed an Historic Resources Survey (HRS) between February 3, 2020 and March 4, 2020 within the vicinity of the Charlotte Douglas International Airport (CLT) in Charlotte, Mecklenburg County, North Carolina (Figure 1). The survey had three primary purposes: (1) identify all properties within the Area of Potential Effect (APE) that were 50 years of age or greater; (2) identify additional properties within the APE that would be 50 years of age or greater as of 2033; and (3) provide recommendations as to each property's eligibility for listing in the National Register of Historic Places (NRHP). Properties less than 50 years of age would be assessed for their potential to be eligible for the NRHP once they reached 50 years of age.

The following report provides the additional information requested for the Old Terminal Building (MK2189) in the North Carolina Historic Preservation Office (NCHPO) October 26, 2020 response letter to the Historic Resources Report Charlotte Douglas International Airport Historical Resources Summary Project, Mecklenburg County, North Carolina (Warminski et al. 2020) (Figure 2).

Project History

A total of 599 properties were identified during the February 3, 2020 through March 4, 2020 field survey. A report documenting the results of the survey (Warminski et al. 2020) was prepared in June 2020 and submitted to the NCHPO.

Within the Warminski et al. 2020 report, 65 properties were listed or recommended as eligible for inclusion in the NRHP, 138 properties were recommended as potentially eligible for the NRHP, and 6 properties were recommended as potentially eligible for the NRHP when they "age in" by 2033. A total of 242 properties was recommended as not eligible for the NRHP and 106 properties were no longer extant. No further work was recommended for these 348 properties. Access to 42 properties could not be obtained at the time of the survey and were recommended to be evaluated in the future.

Following the completion of the survey and submission of the report, consultation between the Federal Aviation Administration (FAA) and North Carolina Historic Preservation Office (NCHPO) resulted in a revision to the APE (Figure 3).

Based on the revised APE, only 22 properties in the original study area remain subject to Section 106 review for the project (Table 1). Of these properties, two (WPA Douglas Airport Hangar [MK2399/3761] and Thomas W. Ferebee Building [MK3825]) were recommended as eligible for inclusion in the NRHP, one was recommended as potentially eligible (Old Terminal Building [MK2189]), two were recommended for further study upon reaching 50 years of age (Air Traffic Control Tower [MK3798] and c. 1980 Truck Terminal [MK3816]). One building is no longer extant (Herron House [MK1368]), and the remaining 16 properties were recommended as not eligible for inclusion in the NRHP (Warminski et al 2020). The NCHPO subsequently determined that the Thomas W. Ferebee building (MK3825) was not recommended as eligible for inclusion in the NRHP (Renee Gledhill-Earley personal communication 2021).

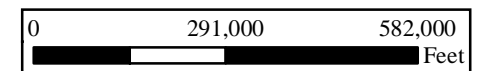
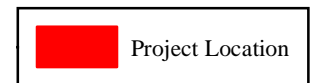
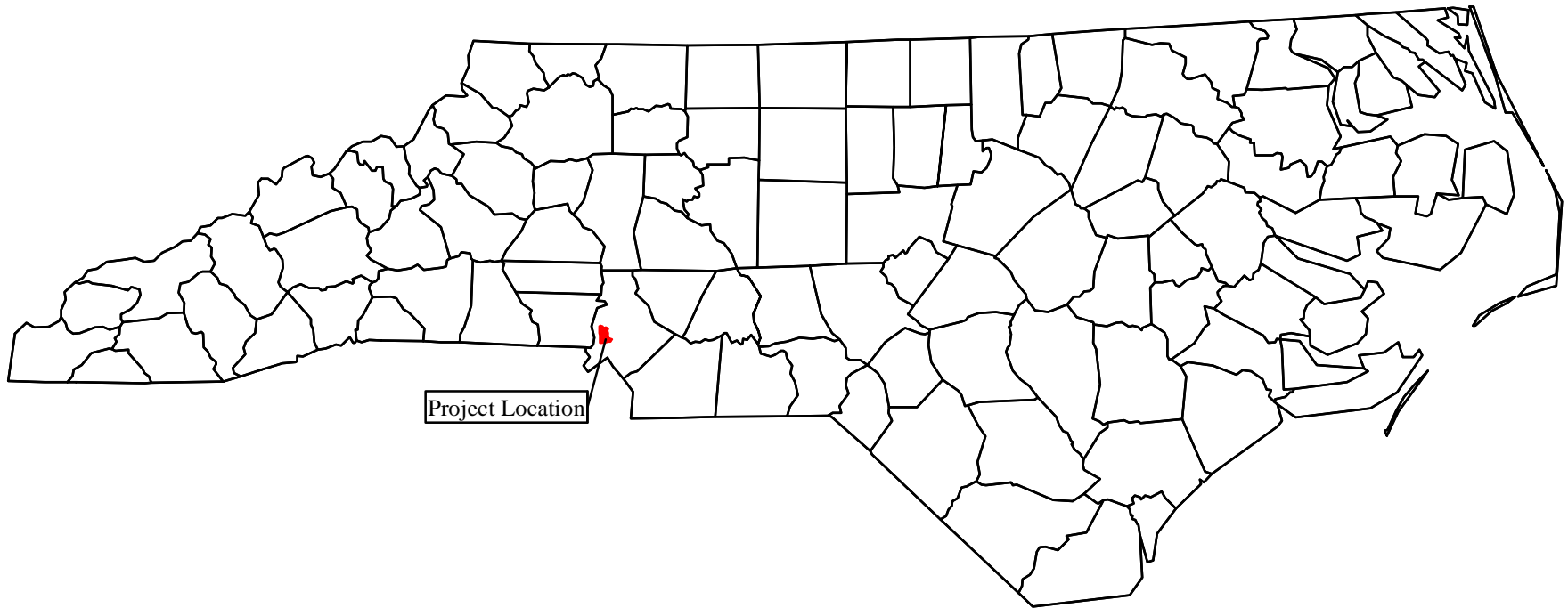


Figure 1

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

State Map of North Carolina with Project Location

Environment & Archaeology
LLC

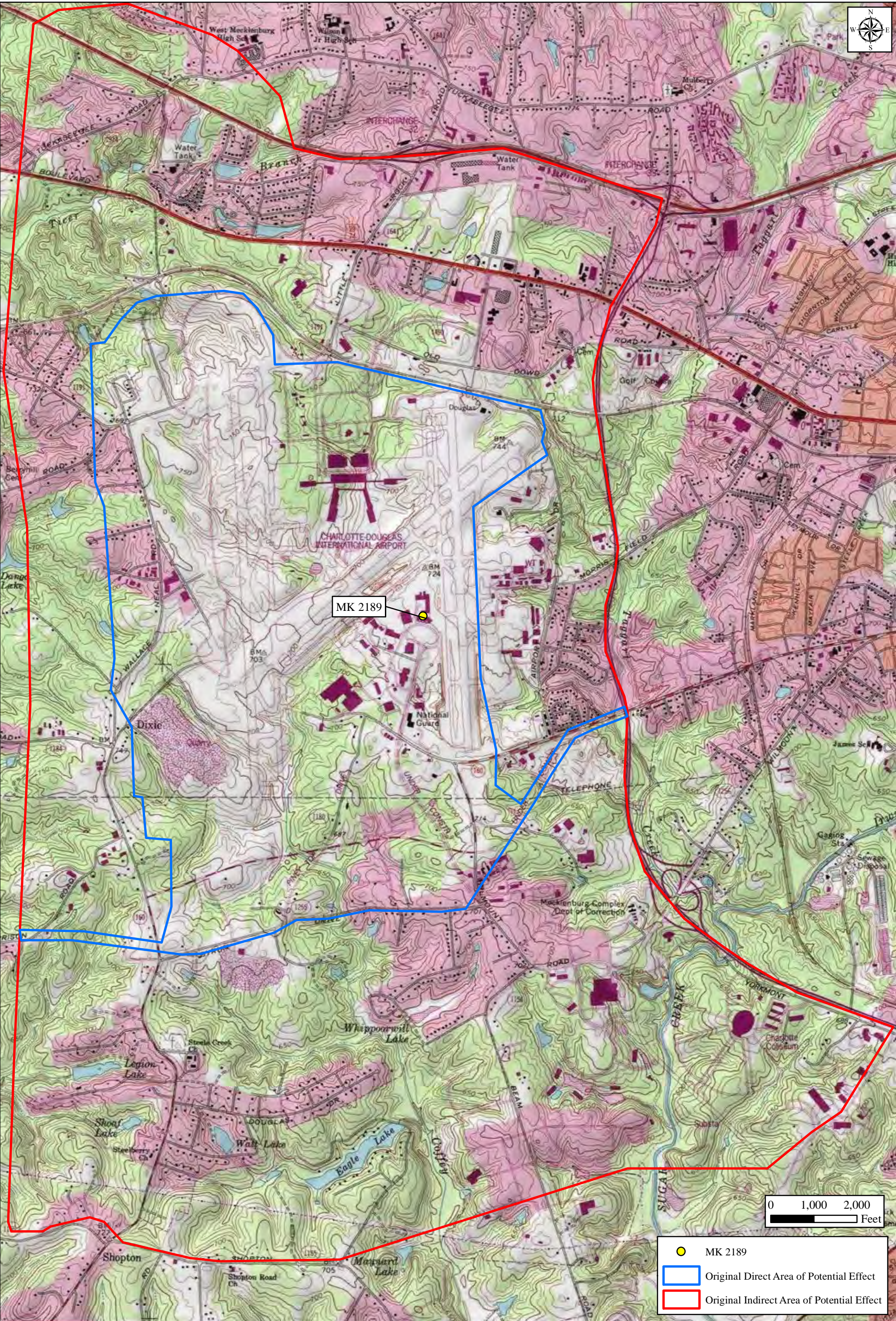


Figure 2	Charlotte Douglas International Airport Historical Resources Summary Project Mecklenburg County, North Carolina	7.5' USGS Charlotte West and Mountain Island Lake (1993/1996), NC Quadrangles <i>Environment & Archaeology</i> LLC
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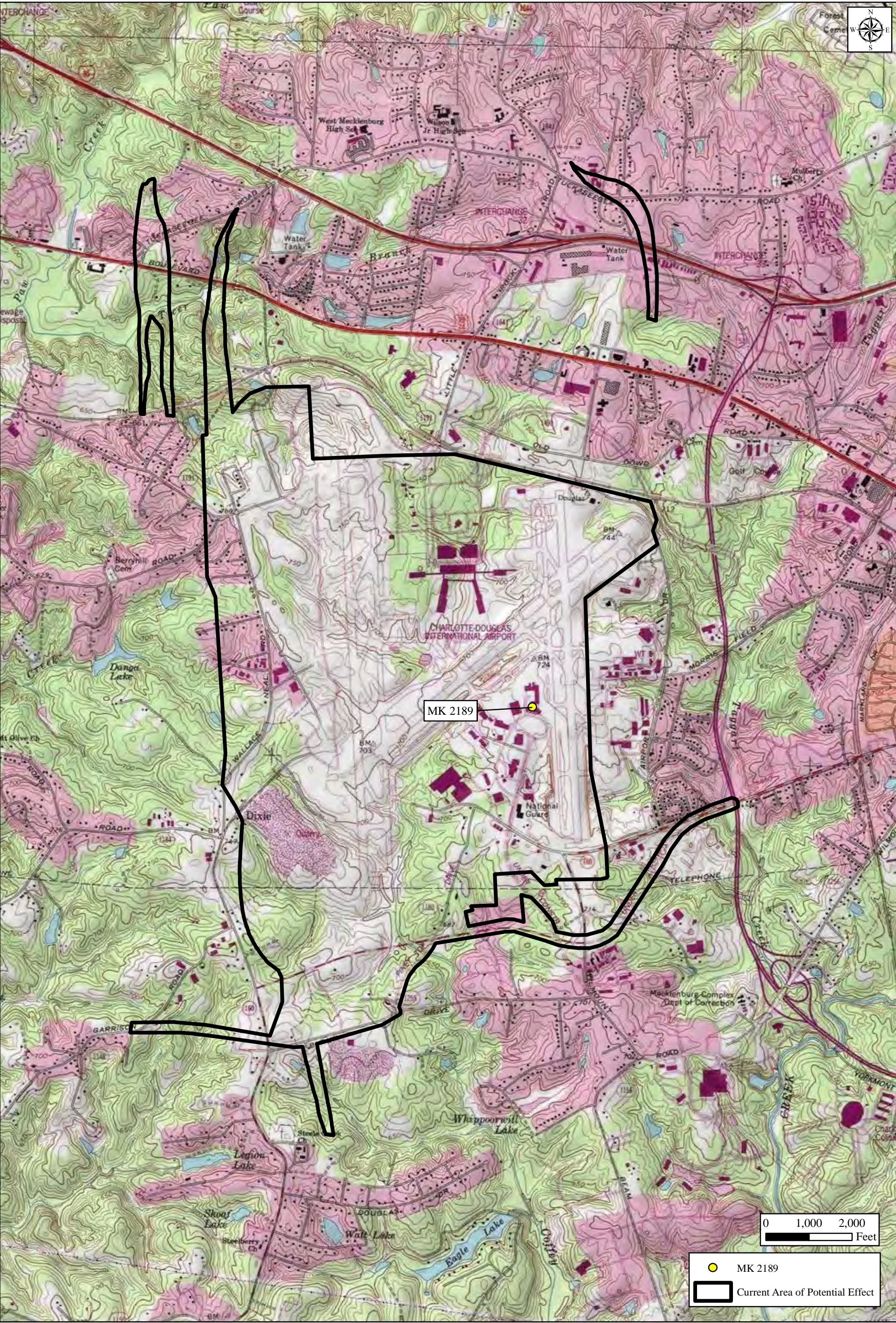


Figure 3	Charlotte Douglas International Airport Historical Resources Summary Project Mecklenburg County, North Carolina	7.5' USGS Charlotte West and Mountain Island Lake (1993/1996), NC Quadrangles <i>Environment & Archaeology</i> LLC
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Table 1. Surveyed Historic Buildings within Current APE (from Warminski et al. 2020)

Resource Number	Resource Name	NRHP Eligibility Recommendation
MK1368	Herron House	No Longer Extant
MK2189	Old Terminal Building	Potentially Eligible Under Criterion A
MK2399/3761	WPA Douglas Airport Hangar	Eligible Under Criterion A
MK3725	c. 1980 Prefab Warehouse	Not Eligible
MK3771	Main Airport Terminal	Not Eligible
MK3798	FAA Control Tower	Potentially Eligible In 2029
MK3799	Parking Toll Plaza	Not Eligible
MK3800	c. 1980 Commercial Building	Not Eligible
MK3801	c. 1956 Warehouse	Not Eligible
MK3802	c. 1970 Warehouse	Not Eligible
MK3803	c. 1980 Truck Terminal	Not Eligible
MK3813	c. 1980 Warehouse	Not Eligible
MK3814	c. 1980 Warehouse	Not Eligible
MK3815	c. 1980 Warehouse	Not Eligible
MK3816	c. 1980 Truck Terminal	Potentially Eligible In 2031
MK3819	1970 Prefab Warehouse	Not Eligible
MK3820	c. 1980 Warehouse	Not Eligible
MK3821	c. 1955 Office	Not Eligible
MK3822	c. 1950 Church	Not Eligible
MK3825	Thomas W. Ferebee Building	Eligible Under Criterion A*
MK4075	c. 1970 Commercial Building	Not Eligible
MK4076	c. 1970 Commercial Building	Not Eligible

*Determined to be not eligible for inclusion in the NRHP (Renee Gledhill-Earley personal communication 2021).

SHPO Consultation to Date

Comments regarding the historic resources report were prepared by the NCHPO on October 26, 2020. For the Old Terminal Building (MK2189) NCHPO inquired:

Are there other 1950s air terminals in Mecklenburg County and/or how does this compare to other terminals at other airports active during the 1950s (Smith Reynolds Airport in Winston-Salem and Raleigh Durham International Airport come to mind as comparison properties)?

A conference call was held on December 23, 2020 between Renee Gladhill-Earley and Sarah David of the NCHPO and Courtney Stoll and Christina Kelly of *Environment & Archaeology, LLC*. To further evaluate MK2189 (Old Terminal Building), NCHPO requested that the terminal building be evaluated in the context of other large airport terminal buildings in the state that are approximately of the same age. Sarah David provided reports that the NCHPO had in their files for some of these buildings (Smith Reynolds Terminal and Piedmont Office Building). They indicated that further research would be needed to determine if there are other terminal buildings in the state that are comparable and to gather additional information regarding the history of the Old Terminal Building to fully evaluate the NRHP eligibility of the resource. This report presents the findings of the additional context and research.

HISTORIC CONTEXT

Morris Field

Conceived in 1939, the small Charlotte Municipal Airport (later named for Charlotte Mayor Ben Douglas) was a part of the New Deal Works Project Administration (WPA) program. It was taken over by the Army several months prior to the attack on Pearl Harbor. The outbreak of World War II caused the Army Air Force to take control of the airport in 1941 as well as a larger tract to the southwest. It was renamed Charlotte Army Airbase, and the first base commander was Colonel C. W. Howard (Howard 2006). North Carolina Governor James Broughton and New York City Mayor Fiorello La Guardia were among the speakers at the airport dedication which attracted 10,000 attendees.

The post was renamed Morris Field in 1942 after Major William C. Morris. Major Morris, a Henderson North Carolina native, was a World War I veteran flier and instructor who originally served in the Canadian Air Force and then transferred to the United State Air Corps in 1917 when the United States joined the war. During World War II, Charlotte and the surrounding area supplied combat-ready troops, supporting personnel, and vital materials for the war effort (Howard 2006). The city was home to a huge Quartermaster Corps Depot and at the South Carolina state line to the south, a large artillery plant was constructed.

From its humble beginnings, as a small two runway airfield, the federal government spent six (6) million dollars to develop Morris Field/CLT into a pilot training base where pilots were trained to fly fighter aircraft. Airplane repair and gunnery training also occurred. The base expanded to encompass several hundred acres, a control tower, and over 100 buildings including the Army constructed barracks to house 1,800 enlisted men, a chapel, theater, library, hospital, armory, clubs, and mess halls. The base was home to the 29th Air Group, 40th Material Group, 56th Pursuit Squadron, and 62nd Pursuit Squadron (Howard 2006). African American service members had their own facilities (Morse Code January 2, 1943:4). The base newspaper was the Morris Code whose banner included “Keep ‘em Flying” in morse code beneath the newspaper name (Howard 2006). It published editorials, biographies of incoming commanders, original cartoons, poems, and calendars of a variety of social and athletic events.

Wartime restrictions meant the base was protected by fencing; news stories about the facility needed permission of the base intelligence officer prior to publication. Windows were covered with black-out cloth so that crews could work around the clock, repairing approximately 100 planes per month. The sound of planes overhead and soldiers everywhere became routine to the citizens of Charlotte and Mecklenburg County, even providing a sense of security (Howard 2006).

Shortly before the war ended, the War Assets Corporation in May 1946, conveyed Morris Field back to the City of Charlotte and it was renamed Charlotte Municipal Airport. Some barracks and other structures were converted to apartments to relieve the postwar housing shortage in the area. However, housing shortages in the area were not new. A September 1943 edition of Morse Code included a “Wanted: Apartments” section in the newspaper noting that “more than a score of officers with families” who had reported to the base were having difficulty finding lodging

(Morse Code September 22, 1943:4). A Housing Committee at the Morris Field Women's Club was tasked with assisting newcomers on finding available house and apartment listings. In 1948, an Air National Guard base was established north of the former Morris Field/CLT to provide continental air defense. Present day Charlotte Douglas International Airport is currently home to the Charlotte Air National Guard Base and its host unit, the 145th Airlift Wing of the North Carolina Air National Guard (Air National Guard 2021).

The Charlotte airport saw important improvements beginning in the 1950s, including a runway extended to accommodate jet aircraft, and a new, Modernist passenger terminal, which opened in 1954 (MK2189). Although growth and development at the airport resulted in the demolition of buildings associated with Morris Field, others are still extant and have been repurposed for new uses. These include the former Morris Field/CLT Officers Club-Civil Air Patrol Headquarters (MK3823), Thomas W. Ferebee Building (MK3825), and Harvest Center (MK4004).

Post-War Civil Aviation in North Carolina

Seven other Army Airfields were located within North Carolina: Seymour Johnson Field in Goldboro (now Seymour Johnson Air Force Base), Blumenthal Field (now Wilmington International Airport), Laurinburg-Maxton Army Base in Maxton (currently inactive), Pope Field (now Fort Bragg) in Fayetteville, Lindy Field [now Piedmont Triad International Airport] in Greensboro, Smith Reynolds in Winston-Salem, and Raleigh-Durham.

The Charlotte airport saw important improvements beginning in the 1950s, including a runway extended to accommodate jet aircraft, and a new, Modernist passenger terminal, which opened in 1954. The 1950s and 1960s are known as the "Golden Age of Flying." The flight itself was almost a vacation with glamorous air hostesses, gourmet meals, lots of leg room, and plenty of drinks. Smoking was even permitted except when the plane was on the ground. Passengers dressed up and posed for photos before boarding. Airlines provided postcards to passengers so they could share their flight experience with those at home. This also provided an activity to keep passengers occupied during their flight. Magazines and newspapers were available; however, in flight movies did not gain popularity until the mid-1960s.

Flights were very expensive, for example a \$138 flight from Chicago to Phoenix adjusted for inflation would be \$1,168 today (Brownlee 2013). These prices meant that the average person could pay up to five percent of their yearly salary for a plane ticket. Ticket cost alone was prohibitive enough for most minorities, but airline operators also trained their operators to identify African Americans and put them on certain flights but not others. Early commercial planes were loud, vibrated fiercely, got bumped around by turbulence, and were often grounded due to weather. The arrival of commercial jets after 1952, alleviated some of the earlier issues. Pressurized airplane cabins were relatively new in the 1950s and a cross country flight required multiple layovers.

Charlotte Douglas International Airport:

As previously noted, a runway extension and new passenger terminal were opened at Charlotte Airport in 1954. It was at this time that the airport was renamed Douglas Municipal Airport. Delta Airlines began scheduled passenger service in 1956. Other airlines operating in Charlotte

included Eastern, Piedmont, Capital (later United), and Southern. Nonstop flights went as far as Newark, Pittsburgh, Columbus, Louisville, Birmingham, and Jacksonville.

In the 1960s, renovations were undertaken to expand the facility and the Boeing 720 jet began flying from the airport in 1962. The renovations included a unit terminal in 1967 for Eastern Airlines containing eight dedicated gates, each with its own departure lounge, snack bar, and baggage claim space. A new concourse was constructed in 1969. Delta, Eastern, and Piedmont moved their operations into this location and United operated from the eastern pier. By 1975, up to 97 flights departed daily to 32 destinations on seven airlines (Departed Flights 2021).

Following airline deregulation, passenger numbers doubled between 1978 and 1980 which meant changes at the airport to accommodate the increase. A new 10,000-foot runway and control tower opened in 1979. A new passenger terminal, designed by Odell Associates, opened in 1982 and the airport was renamed Charlotte Douglas International Airport. A new concourse was constructed and others expanded. Piedmont created a hub and in 1987 started non-stop service to London. The old terminal building was converted into office space and the old control tower was removed in the late 1990s.

Smith Reynolds Airport

Other regional airports of the time included Smith Reynolds in Winston-Salem, Piedmont Triad International near Greensboro, and Raleigh Durham, in Raleigh-Durham. Winston-Salem's leading citizens subsidized the creation of a landing field, originally called Miller Municipal, in anticipation of Charles Lindbergh's planned stop there on the tour following his solo trans-Atlantic flight. However, Miller Municipal's inadequate physical plant impeded other companies' use of the airport. For example, Eastern Airlines began commercial service in 1935, but discontinued flights after eight months due to insufficient facilities. That situation began to change, however, as Winston-Salem native Tom Davis incorporated his airline business as Piedmont Aviation on July 2, 1940. Davis obtained funding from the Z. Smith Reynolds Foundation to allow for the construction of a new terminal and other airport improvements in 1941, and Eastern Airlines reestablished service there following the facility upgrade. Over the years, the terminal has been modified and upgraded.

Soon after the airport's expansion, Piedmont secured a Civil Aeronautics Administration contract to provide flight instruction to civilian pilots. The airport subsequently served as a military aircraft repair center and training facility for over one thousand pilots during World War II. When the airport needed to expand its runway system, the New Deal's Works Progress Administration and the Winston-Salem Foundation provided funding and labor. After the war, the company grew rapidly, making Smith Reynolds the state's busiest airport for five years during the 1960s. However, competition from the Greensboro Regional Airport led to the decline of Smith Reynolds, a situation made worse by Piedmont electing to make Charlotte Douglas International Airport its hub. Piedmont merged with USAir in 1987 and flew its last flights two years later. USAir closed its Smith Reynolds operations in 1993 and the last scheduled commercial passenger plane departed from the airport in 2000. It still maintains a high volume of general aviation and private business aircraft activity.

Piedmont Triad International Airport

Maynard Field, near Greensboro, was dedicated in 1919 and served as one of the first commercial airports in the south. Named for North Carolina pilot Lt. Belvin Maynard, the airport had two intersecting runways and hangar space. In May 1927, Greensboro and Guilford County purchased property in Friendship and named it Lindley Field. Charles Lindberg and his “Spirit of St. Louis” plane stopped there on October 14, 1927. The airfield served as a stop for mail service starting in 1928 by Pitcairn Aviation, Incorporated, the government’s airmail carrier. Pitcairn Aviation became Eastern Air Transport and began passenger service from the airfield. Following several near misses, Lindley Field was closed by the U.S. Department of Commerce. In 1937 it reopened with new paved runways and a passenger terminal.

During the 1940s, the U.S. Army Corps assumed control of the airport for the duration of World War II. It served as a training site for fighter and bomber pilots, a refueling station, and a hub for the Overseas Replacement Depot. Following the War, the airport authority purchased 900 acres around the airport, runways were lengthened and in 1958, a new 34,000 square-foot terminal opened. The terminal featuring aircraft gates, a restaurant, bank, operations office and counters for tickets and rental cars. Eastern, Piedmont, and Capital Airlines served the airport.

In the 1970s the airport was renamed the Greensboro-Highpoint Airport and later the Greensboro-Highpoint-Winston-Salem Regional Airport. It offered passenger service from Delta Air Lines, Piedmont Airline, United Airlines, and Eastern Air Lines. As part of the growth of the airport, facilities shifted to the northwestern portions of the property and the original terminal was phased out and eventually demolished. A new terminal opened in 1982 and the runway was extended to a length of 10,000 feet making it the longest runway in the state. The facility was renamed Piedmont Triad International Airport in 1987.

Raleigh-Durham International Airport

Raleigh Municipal Airport was founded in 1929. Eastern Airlines President Captain Eddie Rickenbacker took out a full-page ad in area newspapers to encourage the counties of Wake and Durham and the cities of Durham and Raleigh to build an airport together. While under construction, in 1942, the airport was taken over by the federal government to use during World War II. Designated Raleigh-Durham Army Airfield it opened for operations on May 1, 1943 with three runways. It served as a training facility for the Army Air Corps until January 1, 1948.

Eastern Airlines received permission to use the airfield in 1943 and began service to New York and Miami. In 1946, the airport was returned to the cities and counties by the federal government. Capital Airlines and Piedmont Airlines arrived by 1948 and by 1957 there were 36 daily departures. Delta Air Lines began operation at Raleigh-Durham in 1970 followed by Allegheny Airlines in 1979 and by 1985 Trans World Airlines, American Airlines, Ozark, People Express, New York Air, and Pan Am flew in and out of the airport. Eastern Airlines began Boeing 727 jet service in 1965.

Today Raleigh-Durham is one of the fastest growing major airports in the U.S. Expansions and upgrades occurred throughout the early 2000s. A second terminal was added. The modernization of Terminal 1 included the demolition of the original 1955 terminal. Other changes include underground pedestrian walkways and additional airport parking.

EVALUATION PROCESS

The historical significance and integrity of this historic property was evaluated using the criteria outlined in the National Register bulletin, How to Apply the National Register Criteria for Evaluation (National Park Service [NPS] 1997). The NRHP recognizes four categories of significance of a resource possessing integrity of location, design, setting, materials, workmanship, feeling and associations and:

- A. That are associated with events that have made a significant contribution to the broad patterns of our history,
- B. That are associated with the lives of persons significant in our past,
- C. That embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that represent a significant and distinguishable entity whose individual components may lack individual distinction, and
- D. That have yielded, or may be likely to yield, information important in prehistory or history.

For a property to qualify for the NRHP it must meet one of the NRHP criteria for evaluation by being associated with an important historic context, such as transportation, military history, or architecture, and retaining historic integrity of those features necessary to convey its significance.

Integrity is the ability of a property to convey its significance. Within the concept of integrity, the NRHP criteria recognize seven aspects or qualities that, in various combinations, define integrity (NPS 1997:44-48).

Location is the place where the historic property was constructed or the place where the historic event occurred. The actual location of a historic property, complemented by its setting, is particularly important in recapturing the sense of historic events and persons. Except in rare cases, the relationship between a property and its historic associations is destroyed if the property is moved. Resources therefore retain integrity of location if they remain on their original sites.

Design is the combination of elements that create the form, plan, space, structure, and style of a property (ibid.). It includes such elements as organization of space, proportion, scale, technology, ornamentation, and materials (ibid.). form, shape, proportions and solids to voids. Resources retain integrity of design if their original form, scale, footprint and pattern of piercing remain evident.

Setting is the physical environment of a historic property. It refers to the character of the place where they property played its historical role. The physical features that constitute the setting of a historic property can include topography, vegetation, simple manmade features, and relationships between buildings and other features or open space. Resources retain integrity of setting if they retain these qualities.

Materials are the physical elements that were combined or deposited during a particular period of time and in a particular pattern or configuration to form a historic property. Indigenous materials

are often the focus of regional building traditions and thereby help define an area's sense of time and place. Resources possess integrity of materials if they retain key exterior materials dating from the period of their historic significance.

Workmanship is the physical evidence of the crafts of a particular culture or people during any period in history or prehistory. It is evidence of artisans' labor and skill in constructing or altering a building or structure. Examples in historic buildings include tooling, carving, painting, graining, turning and joinery. Resources retain integrity of materials if they retain these features.

Feeling is a property's expression of the aesthetic or historic sense of a particular period of time. It results from the presence of physical features that, taken together, convey the property's historic character. If resources possess sufficient integrity of design, workmanship, materials, location and setting to be identified as products of their particular time and place in history, they will convey the feeling of that place and time.

Association is the direct link between an important historic event or person and a historic property. Like feeling, it requires the presence of physical features that convey a property's historic character. A resource retains association if it is the place where the event or activity occurred and is sufficiently intact to convey that relationship to an observer.

The steps in assessing integrity are:

- define the essential physical features,
- determine whether they are visible,
- compare the property with similar resources in the area, and
- determine, based on the significance and essential physical features, which aspects of the property are particularly vital to the property and if they are present.

Ultimately, the question of integrity is answered by whether the property retains the identity for which it is significant (NPS 1997.:45-48).

OLD TERMINAL BUILDING (MK2189)

Charlotte has known three major airports in its history. The first one opened in 1936; the second, in 1954, was named after former Charlotte mayor Ben Elbert Douglas Sr. When it opened, it was hailed as an unprecedented technological achievement never before seen in the region because it could handle as many as 13 to 14 planes at once. It also was considered the Carolinas' largest and busiest airport. The terminal was designed by Walter Hook, son of noted local architect Charles Christian Hook (1870-1938), who practiced "a suave and monumental Beaux-Arts Classicism" (Michael 2009).

The original terminal for the Charlotte Douglas Airport was built ca. 1951 and opened in 1954 (Figure 5 and Figure 6). The two-story, steel-frame building has a flat roof and brick exterior (Photo 1 and Photo 2). Its footprint is a wide, shallow, U-shape. The front façade is blind apart from the glass central section. A flat canopy extends over the sidewalks. The interior has a full-height lobby with a double staircase of terrazzo. Railings are aluminum. The second-floor balcony overlooks the lower waiting room. There is a fallout shelter in the building. After a new terminal was completed, the building was used as a cargo terminal. The building has seen little alteration and has a high degree of integrity under all its aspects.

The Old Terminal Building reflects the influence of the Mid-Century Modern style and much of its original historic fabric remains intact. The Modernist-inspired, covered breezeway and lobby are seemingly unchanged since their construction 67 years ago, as are the decorative brick façade and the fixed, multi-light, tinted glazing. Likewise, the tile floors are intact and the massive free-hanging stair, built of glazed concrete, chrome and terrazzo tile, is still intact. The former storefronts have been covered over but presumably are intact underneath. Some alterations have been made to the terminal: The old control tower has been removed and concourses that previously led to boarding gates have been demolished.



Photo 1. Old Terminal Building (CK2189) ca. 1954.



Figure 4

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 2189 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC



Figure 5

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with MK 2189 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

MK2189, Old Terminal Building, 4700 Yorkmont Road, V.W., February 2020



MK2189_OldTerminalBuilding_02 20_V.W._01

Front View



MK2189_OldTerminalBuilding_02 20_V.W._02

Right View



MK2189_OldTerminalBuilding_02 20_V.W._03

Back View



MK2189_OldTerminalBuilding_02 20_V.W._04

Left View

Comparison to Other Regional Airport Terminals in North Carolina

Terminals were constructed at Raleigh-Durham, Piedmont Triad International, and Smith Reynolds around the same time as the Old Terminal Building at Charlotte International Airport. These resources were chosen as comparative resources to evaluate the NRHP eligibility of MK2189 (Figure 7).

Raleigh-Durham Terminal 1 (later Terminal B), built in 1955, originally consisted of a two-story section surmounted by a control tower and flanked by a one and one-half story wing (Photo 3). The terminal was expanded and reshaped over the years, adding new baggage claim space and a three-story addition for airport offices in the 1960s and 1970s. An addition (Terminal A) was added in the 1980s and a new concourse was added overhead in the 1990s (Photo 4). The original terminal, referred to as the “Blue Box” was eventually demolished as part of updates in 2014. These updates included a new roof, skin, finishes and the addition of an exterior canopy system that provided both shelter for passengers and an updated architectural look for the building which reopened as Terminal 1 (Photo 5).

The terminal at Piedmont Triad was a two-story glass paneled structure with a single enclosed pier (Photo 6). The original terminal location was along Airline Road in the southwestern portion of the airline property. Airport expansions in the 1980s shifted operations to the northwest areas of the airport and phased out the original terminal as the new facility opened in 1982. The building appears on aerial images into the early 1990s but is gone by 1999. It is currently vacant land.

The Smith Reynolds Airport Terminal originally consisted of a two-story central section surmounted by a control tower and flanked by one- and two-story wings (Photo 7). Smith Reynolds’ terminal was designed by Chicago architect Howard Lovewell Cheney, who headed his own firm for most of his career. Cheney also was employed by the Public Buildings Branch of the Treasury Department from 1934 to 1942, designing New Deal-funded public buildings, and by the University of Illinois in later years. In addition, Cheney designed Federal buildings in Peoria, Gary and New Orleans as well as buildings at the 1939 World’s Fair. He also was supervising architect for the Chicago Tribune Tower.

A full-height glass curtain wall in the waiting room overlooked the runway and metal railings surrounded rooftop viewing areas. The terminal has been extensively altered through additions, stucco wall treatment and window replacement (Photo 8). The interior was completely remodeled in 1984 when large open spaces were partitioned to create offices. A mural depicting city buildings, businesses and aviation formerly hung in the dining room but was moved to its current location after the dining room closed in 1987.

Steel posts, beams and trusses comprise the flat-roofed hangar’s structural system and ribbed metal posts sheathe its exterior walls. The north elevation contains a continuous series of suspended hangar doors, three of which are nearly two stories tall. The door openings and two bands of translucent metal casement windows, one above the one-story rear wing and another just below the roofline, bring light to the interior. Glass curtain walls enclose the hyphen that connects the hangar to the T-shaped, flat-roofed, Modernist office building, where curtain walls consist of long horizontal bands of plate-glass windows above enameled metal panels. Blind brick panels punctuate one end of each elevation. The first floor originally contained executive offices and meeting rooms and the basement held the accounting department and storage, break, and copy rooms.

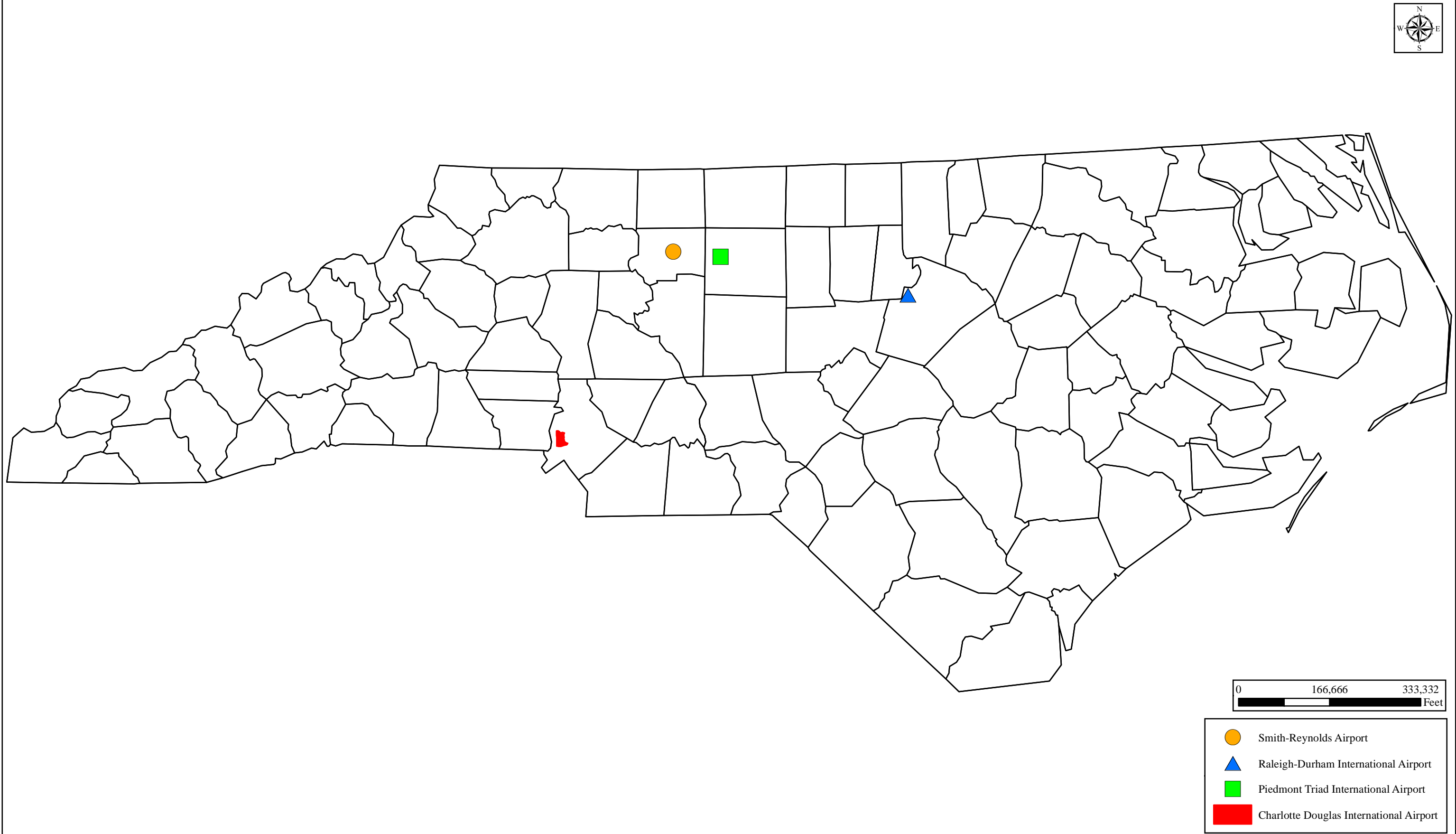


Figure 6

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

State Map of North Carolina with the Charlotte Douglas International Airport, Raleigh-Durham International Airport,
Smith-Reynolds Airport, and Piedmont Triad International Airport Identified

Environment & Archaeology
LLC



Photo 3. Raleigh-Durham Airport ca. 1955



Photo 4. Raleigh-Durham pre-2014 demolition.



Photo 5. Raleigh-Durham 2014



Photo 6. Piedmont Triad Terminal ca. 1960.



Photo 7. Smith-Reynold Terminal 1952.



Photo 8. Smith Reynolds Terminal 2020.

NRHP DISCUSSION:

Criterion A: The 1950s passenger terminals at Charlotte Douglas International Airport, Raleigh-Durham International Airport, Piedmont Triad International Airport, and Smith Reynolds Airport are not recommended as a multiple property nomination to the NRHP under Criterion A. Of these properties, only the terminals at Charlotte Douglas International (MK2189) and Smith Reynolds remain. Smith Reynolds does not appear to retain sufficient integrity to convey its significance.

The Old Terminal Building (MK2189) is recommended eligible for NRHP listing under Criterion A for its association with the development of air travel in Charlotte in the mid-20th century. Constructed in 1939, Charlotte Douglas International Airport transformed from a military installation to an international airport serving over 50 million passengers, making it the 11th busiest airport (ranked on passenger traffic) in the U.S (Airport Council International 2017). World War II Army Airfields located within North Carolina, Charlotte Douglas International Airport along with Bluethenthal Field (now Wilmington International Airport), Lindley Field [now Piedmont Triad International Airport] near Greensboro, Smith Reynolds in Winston-Salem, and Raleigh-Durham transitioned to commercial enterprises in the post war years of the mid-20th century. It was during this time that flights started to become more accessible to average Americans, airlines established hubs at the facilities and the stage was set for the federal deregulation of the airline industry in 1979 ushering in a free-market industry resulting in reduced fares and more flights.

Criterion B: The Old Terminal Building is not recommended as eligible under Criterion B for its association with architect Walter Hook. Along with his father, C.C. Hook, their firm Hook and Hook were the architects for the Union County Courthouse in Monroe, North Carolina and United States Post Office in Gastonia, North Carolina. Mr. Hook was best known for his work designing hospitals and medical facilities. He was the architect for the Mercy Hospital, Carolinas Medical Center, and Presbyterian Hospital in Charlotte and the Veteran's Administration in Salisbury. The terminal building is not the best example of his work to convey his significance.

Criterion C: The Old Terminal Building is also recommended as eligible under Criterion C as an example of a forward-looking, Modernist air terminal of the mid-20th century. Despite the alterations, it retains sufficient integrity that an airline passenger of the 1950s or 1960s, transported to the present day, would almost certainly recognize the airport despite these changes. Other airports such as Raleigh-Durham, Piedmont Triad International and Smith Reynolds were constructed during the same time periods. The original terminal at Raleigh-Durham and Piedmont Triad are no longer extant and Smith Reynolds does not retain sufficient integrity under Criterion C. As one of the best remaining examples of a mid-century air terminal, the Old Terminal Building embodies the distinctive characteristics of a modernist air terminal from the early days of commercial air travel and conveys its significance under Criterion C.

Environment & Archaeology, LLC recommends that the Old Terminal Building (MK2189) is eligible for inclusion in the NRHP under Criteria A and C.

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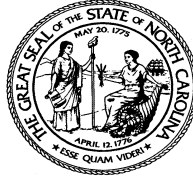


Figure 5

Charlotte Douglas International Airport
Historical Resources Summary Project
Mecklenburg County, North Carolina

Aerial with NRHP Boundary of MK 2189 Identified
Aerial Provided by ArcGIS Online
Environment & Archaeology
LLC

SHPO Concurrence on Effect Finding



**North Carolina Department of Natural and Cultural Resources
State Historic Preservation Office**

Ramona M. Bartos, Administrator

Governor Roy Cooper

Secretary D. Reid Wilson

April 8, 2021

Tim Alexander
FAA – Memphis Airports District Office
2600 Thousand Oaks Boulevard, Suite 2250
Memphis, TN 38118

timothyalexander@faa.gov

RE: Expansion of Charlotte Douglas International Airport (CLT), Charlotte, Mecklenburg County,
ER 20-0438

Dear Mr. Alexander:

We are in receipt of the February 25, 2021, letter from Environment & Archaeology transmitting the "Addendum to the Historic Structures Survey Report" for the above-referenced undertaking. After reviewing the report, we concur with its finding that the Charlotte Douglas International Airport Old Terminal Building (MK 2189) is eligible for the National Register under Criterion A for its association with the development of air travel in Charlotte and Criterion C for its Modernist design.

However, the report must include a specific boundary map, boundary description to supplement the map, and a boundary justification. Figure 5 on page 14 may be intended to illustrate the eligible boundary, but the title block needs to clearly indicate that the boundary drawn is the National Register-eligible boundary. Please provide such a map.

Although we are able to concur with the report's findings, its integrity discussion is weak. The report describes the seven aspects of integrity, but it does not describe how the Old Terminal Building specifically retains or does not retain those aspects of integrity. Instead of or in addition to defining each aspect on pages 10 and 11, the report would benefit from adding notes about how the Old Terminal meets each aspect.

In general, the description of the Terminal describes very few alterations, but the report does mention that the control tower and concourse have been removed. These two losses need to be addressed more sufficiently. The report fails to make a strong case that those removals have not had a negative effect on the building's integrity of design, feeling, and association. The argument for the retention of integrity in the face of the loss of the control tower and concourses would create a truly defensible case for eligibility.

While able to concur with the eligibility judgement, the reviewer had to tease out each aspect of integrity from the body of the text, images, and aerial photos, and create a reasonable argument that the loss of the control tower and concourse did not affect the resource's integrity of design, association, and feeling to a degree did not preclude National Register eligibility. This may create a problem for future readers of the report.

In short, future reports should address the seven aspects of integrity specifically and individually, even if that seems redundant or repetitive: not every reader will be able or willing to pull integrity out of the text.

By this letter, we also reaffirm our determination that the Thomas W. Ferebee Building (MK3825) is not eligible for listing in the National Register of Historic Places due to its being moved from its original location and changes to its original layout.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, contact Renee Gledhill-Earley, environmental review coordinator, at 919-814-6579 or environmental.review@ncdcr.gov. In all future communication concerning this project, please cite the above referenced tracking number.

Sincerely,



for Ramona Bartos, Deputy
State Historic Preservation Officer

cc: Christina Kelly

ckelly@environment-archaeology.com



**North Carolina Department of Natural and Cultural Resources
State Historic Preservation Office**

Ramona M. Bartos, Administrator

Governor Roy Cooper

Secretary D. Reid Wilson

May 17, 2021

MEMORANDUM

TO: Crystal Best
N.C. Department of Administration
State Clearinghouse
crystal.best@doa.nc.gov

FROM: Renee Gledhill-Earley
Environmental Review Coordinator
RGE for Ramona M. Bartos

SUBJECT: Environmental Assessment for Charlotte Douglas International Airport (CLT)
Capacity Enhancement Projects, Charlotte, Mecklenburg County, ER 16-1791

We have received notification from the State Clearinghouse of the above-referenced proposed undertaking and offer the following comments.

There are no known archaeological sites within the proposed project area. Based on our knowledge of the area, it is unlikely that any archaeological resources that may be eligible for inclusion in the National Register of Historic Places will be affected by the project. We, therefore, recommend that no archaeological investigation be conducted in connection with this project.

The North Carolina State Historic Preservation Office is prepared to enter into a Memorandum of Agreement between the Federal Aviation Administration and the City of Charlotte to mitigate the adverse effect of the undertaking on the Old Terminal Building. We look forward to the consultation with FAA, the City of Charlotte, and other interested parties.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, contact Renee Gledhill-Earley, environmental review coordinator, at 919-814-6579 or environmental.review@ncdcr.gov. In all future communication concerning this project, please cite the above referenced tracking number.

cc: Tim Alexander, FAA
timothy.l.alexander@faa.gov

Memorandum of Agreement

**MEMORANDUM OF AGREEMENT
BETWEEN
THE FEDERAL AVIATION ADMINISTRATION,
NORTH CAROLINA STATE HISTORIC PRESERVATION OFFICE,
AND THE CITY OF CHARLOTTE, NORTH CAROLINA
REGARDING
THE PROPOSED CAPACITY ENHANCEMENT PROJECTS AND THE DEMOLITION
OF THE OLD TERMINAL BUILDING AT CHARLOTTE DOUGLAS INTERNATIONAL
AIRPORT
CHARLOTTE, MECKLENBURG COUNTY, NORTH CAROLINA**

WHEREAS, the Federal Aviation Administration (hereinafter “FAA”), the lead Federal agency for this Undertaking, is considering the City of Charlotte, North Carolina’s (hereinafter “City”) requests (1) to approve the proposed revisions to the Airport Layout Plan for the Charlotte Douglas International Airport pursuant to 49 U.S.C. §§ 40103 and 47107; and (2) relating to eligibility for funding pursuant to 49 U.S.C. §§ 47106 and 47107 and/or 49 U.S.C. § 40117; and

WHEREAS, the FAA defined the project’s Area of Potential Effects (hereinafter “APE”) as defined in 36 CFR 800.16(d), for direct effects and indirect effects (the APE is illustrated in the Environmental Assessment, Department of Transportation Act Section 4(f), 49 U.S.C. §303, and National Historic Preservation Act (hereinafter “NHPA”) Section 106, 54 U.S.C. § 306108 evaluations) and is attached as Appendix A; and

WHEREAS, the City by and through its Aviation Department, plans to construct the capacity enhancement projects, including a new fourth parallel runway and associated exits and taxiways and the expansion of the terminal (Concourse B and C building and ramp) that necessitate the removal of the Old Terminal Building (hereinafter “the Undertaking”); and

WHEREAS, the FAA, in consultation with the North Carolina State Historic Preservation Office (hereinafter “SHPO”) has evaluated the potential eligibility of the Old Terminal Building for listing in the National Register of Historic Places (hereinafter “NRHP”); and

WHEREAS, the FAA has determined and the SHPO has concurred that the Old Terminal Building is eligible for listing in the NRHP under Criterion A and C, for its association with the development of air travel in Charlotte in the mid-20th century and in Architecture as one of the best remaining examples of a mid-century air terminal, embodying the distinctive characteristics of a modernist air terminal from the early days of commercial air travel; and

WHEREAS, the FAA has determined that the Undertaking will have an adverse effect on the Old Terminal Building and has consulted with the SHPO pursuant to 36 CFR part 800 of the regulations implementing Section 106 of the NHPA; and

WHEREAS, the FAA provided opportunity for the Catawba Indian Nation to consult on the Undertaking’s potential to affect properties with religious and cultural significance in letters dated February 19, 2021. The Catawba Indian Nation declined the invitation to

participate as a consulting party regarding the effects of the Undertaking on historic properties in a letter dated March 29, 2021; and

WHEREAS, the FAA has consulted with the Old Mecklenburg Genealogical Society and the Historic Preservation Foundation of North Carolina concerning the effects of the Undertaking, and invited them to concur in this Memorandum of Agreement (hereinafter "MOA"); and

WHEREAS, in accordance with 36 CFR § 800.6(a)(1), FAA has notified the Advisory Council on Historic Preservation (hereinafter "ACHP") of its adverse effect determination with specified documentation, and the ACHP declined to participate in the consultation on June 14, 2021 pursuant to 36 CFR § 800.6(a)(1)(iii); and

WHEREAS, the draft Environmental Assessment, including the Section 4(f) and Section 106 evaluations, was made available to the public for review and comment on April 16, 2021, and two public workshops and public hearings were held on May 17, 2021 and May 18, 2021, to discuss the Undertaking's impacts;

NOW, THEREFORE, the FAA, the SHPO, and the City of Charlotte, by and through the City of Charlotte Aviation Department, the parties to this MOA, shall carry out the Undertaking in accordance with the following stipulations to take into account the effect of the Undertaking on historic properties:

STIPULATIONS

If the FAA issues a Record of Decision approving the proposed Undertaking as described in the Environmental Assessment, Section 4(f), and Section 106 evaluations, the FAA, in coordination with the SHPO and the City of Charlotte Aviation Department, will ensure that the following mitigation measures are carried out:

1. ARCHITECTURAL AND HISTORICAL DOCUMENTATION

A. A Level II Historic America Building Survey (HABS) recordation of the Old Terminal Building property will be completed. The Level II documentation will contain architectural and historical narrative, measured drawings, and digital photographs in an archivally-stable format. The photographic record will include at a minimum the general environment, front facade, front and rear perspective views, typical windows, and exterior and interior. The submission will also include a CD of all printed images with the digital copy. The Level II HABS recordation will be submitted to SHPO for approval. SHPO will provide written acceptance of the Level II HABS recordation within 30 calendar days of their receipt.

B. The City of Charlotte Aviation Department may proceed with demolition of the Old Terminal Building once the archival photography of the exterior and interior has been submitted and approved by the SHPO.

C. SHPO will arrange for deposit of the Level II HABS recordation to the State Archives of North Carolina. A second set of images and final report shall be submitted to the City of Charlotte Aviation Department.

D. If items which may contain historical significance or if additional historic properties or unanticipated effects on the historic property are discovered (36 CFR § 800.6(c)(6)), the City shall notify the SHPO of the discovery and consult with the FAA and SHPO pursuant to 36 CFR Part 800.

2. EFFECTIVE DATE AND DURATION

A. This MOA will be effective on the date the last Signatory signs the agreement.

B. This MOA will expire if its terms are not carried out within five years from the date of its execution. Prior to expiration of the MOA, the Signatories may consult to reconsider its terms and amend it in accordance with Stipulation 3 below.

3. AMENDMENTS

A. Any party to this MOA may propose to the other parties that the MOA be amended, whereupon the parties will consult in accordance with 36 CFR Part 800.6(c)(7) to consider such an amendment. The amendment will be effective on the date the last Signatory signs the revised MOA.

4. TERMINATION

A. If any signatory to this MOA determines that its terms will not, or cannot be carried out, that party shall immediately consult with the other parties to attempt to develop an amendment per Stipulation 3, above. If within 30 calendar days (or another time period agreed to by all signatories) an amendment cannot be agreed upon, any signatory may terminate the MOA upon written notification to the other signatories.

B. Once the MOA is terminated and prior to work continuing on the Undertaking, FAA must either (1) execute an MOA with different terms pursuant to 36 CFR § 800.6 or (2) take into account and respond to the comments of the ACHP under 36 CFR § 800.7. FAA shall notify the signatories as to the course of action it will pursue. The FAA will undertake its obligations pursuant to applicable statutes, regulations, and agency guidance.

Execution of this MOA by FAA, SHPO, and the City, by and through its Aviation Department, and implementation of its terms evidences that the FAA has taken into account the effects of this Undertaking on historic properties and afforded the ACHP an opportunity to comment. This MOA is not intended to provide a private right of action by any individual and/or group.

This MOA may be executed in any number of counterparts, each of which when executed and delivered shall constitute a duplicate original, but all counterparts together shall constitute a single agreement.


Signatories:

Federal Aviation Administration

By: **TOMMY L DUPREE** Digitally signed by TOMMY L DUPREE
Date: 2021.07.15 16:37:04 -05'00' Date: _____

Tommy L. Dupree
Manager, Memphis Airport District Office

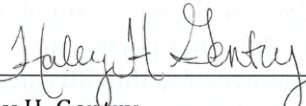
North Carolina State Historic Preservation Office

By:  Date: 8/3/2021

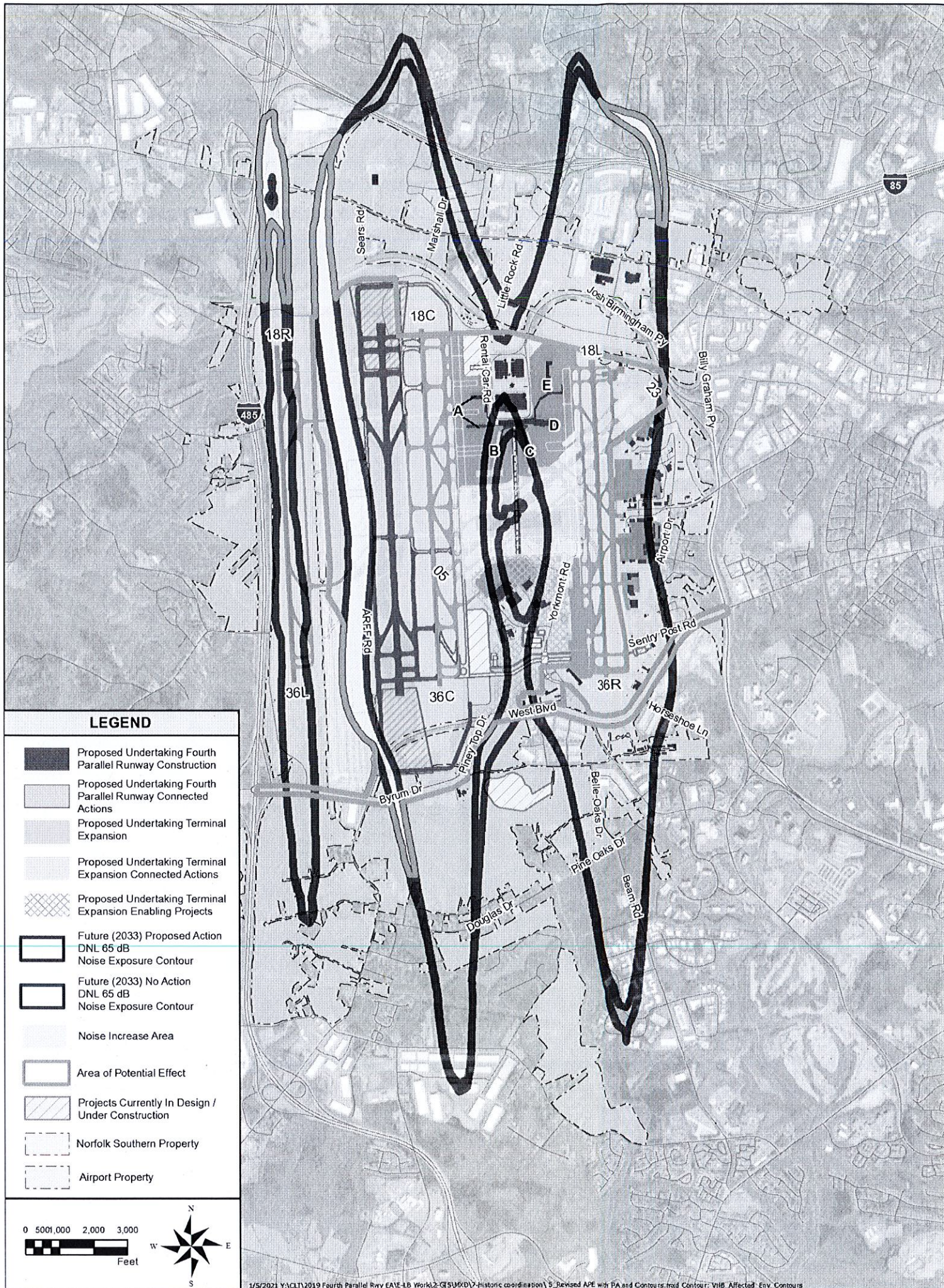
Ramona M. Bartos, Deputy State Historic Preservation Officer

Invited Signatory:

City of Charlotte, North Carolina (by and through City of Charlotte Aviation Department)

By:  Date: 08/12/2021

Haley H. Gentry
Aviation Director, Aviation Department
Approved as to form:



ADDENDUM #1
TO
MEMORANDUM OF AGREEMENT
BETWEEN THE
FEDERAL AVIATION ADMINISTRATION,
NORTH CAROLINA STATE HISTORIC PRESERVATION OFFICE,
AND
THE CITY OF CHARLOTTE, NORTH CAROLINA
REGARDING
THE PROPOSED CAPACITY ENHANCEMENT PROJECTS AND DEMOLITION OF
THE OLD TERMINAL BUILDING
AT
CHARLOTTE DOUGLAS INTERNATIONAL AIRPORT
CHARLOTTE, MECKLENBURG COUNTY, NORTH CAROLINA

The Memorandum of Agreement (MOA) for the proposed undertaking was fully executed by all signatories on August 12, 2021. The MOA was based on the City of Charlotte's selection of Alternative 1 as the preferred alternative. Since that time, the City of Charlotte has identified Alternative 2 as its preferred alternative.

Alternative 2, depicted in the attached drawing, is similar to Alternative 1. However, the runway and associated infrastructure would be positioned 100 feet further east. The shift was made to allow the proposed new runway to take advantage of a new change to Federal Aviation Administration (FAA) Order 7110.65Z for Closely Spaced Parallel Operations (CSPO). When commissioned, the new runway will primarily operate as a departure runway. Increased use as an arrival runway shall require additional environmental evaluation.

In comparing the two alternatives, the FAA considered the potential for new impacts relevant to Section 106 of the National Historic Preservation Act. Given the similarities between the two alternatives, the Area of Potential Effects (APE) for ground disturbing activities associated with Alternative 2 were considered to be consistent with Alternative 1. For above ground resources, a revision to the APE was considered due to audible impacts from aircraft noise. Given the location of the Alternative 2 runway being 100 feet further the east, the APE based on significant noise levels will also be further east. However, the change in the APE will not place any new structures within the APE. As such, no additional resources need to be identified and assessed. Consequently, the selection of Alternative 2 as the preferred alternative, does not result in any material changes to the APE, the resources previously assessed, or effects on historic resources. Furthermore, the selection of Alternative 2 has no impact on the stipulations and terms within the MOA.

Therefore, the signatories to the MOA and the City of Charlotte as an invited signatory agree to this Addendum to the MOA executed on August 12, 2021.

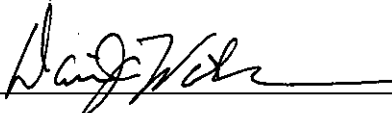
Signatories:

Federal Aviation Administration

By: **TOMMY L DUPREE** Digitally signed by TOMMY L DUPREE
Date: 2021.12.02 20:13:33 -06'00' Date:

Tommy L. Dupree
Manager, Memphis Airport District Office

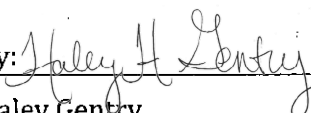
North Carolina State Historic Preservation Office

By:  Date: 12/8/21

Darin J. Waters, Ph.D.,
State Historic Preservation Officer

Invited Signatory:

City of Charlotte, North Carolina (by and through City of Charlotte Aviation Department)

By:  Date: 1/5/2022

Haley Gentry
Aviation Director, Aviation Department
Approved as to for

